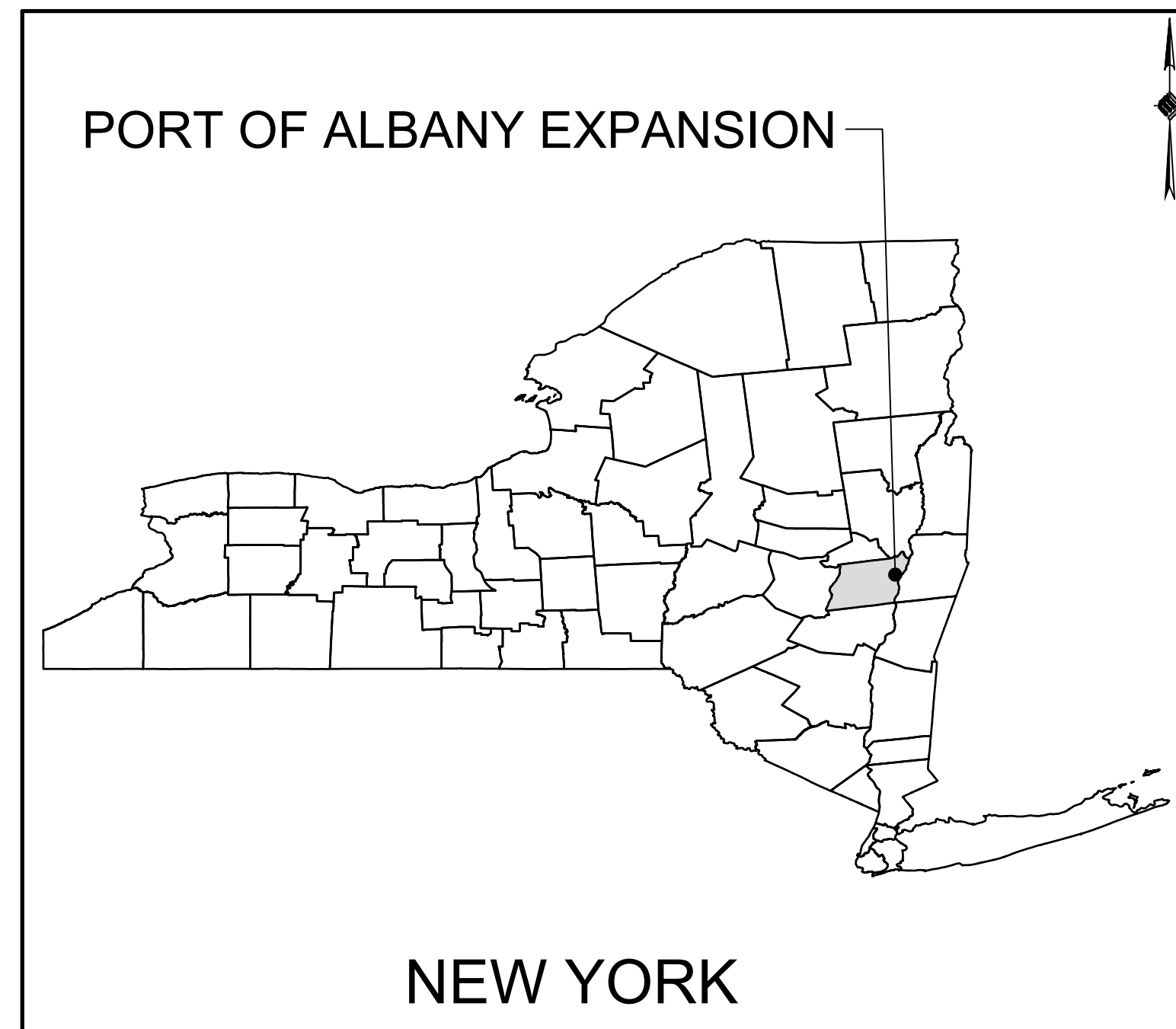


ALBANY PORT DISTRICT COMMISSION

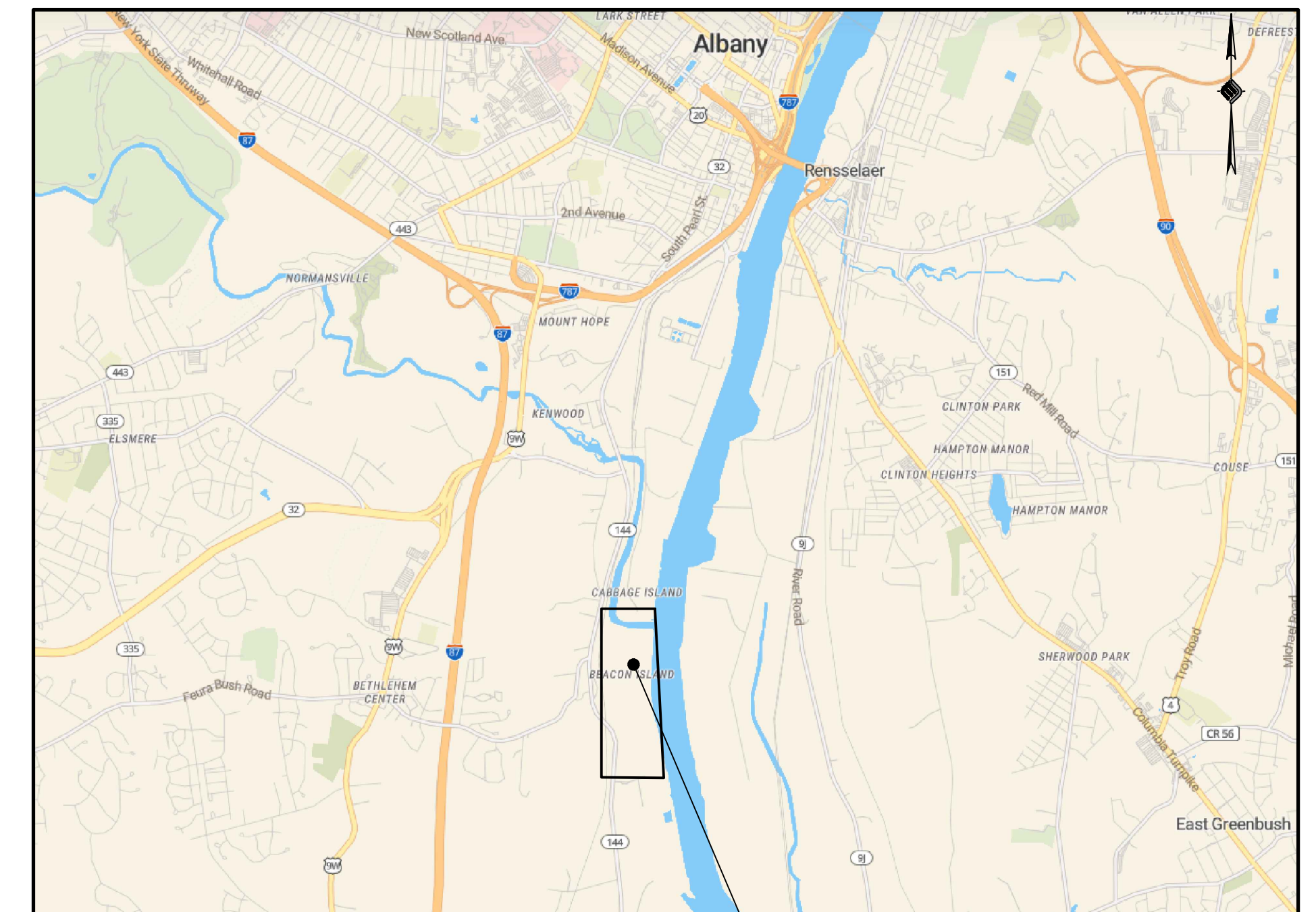
PORT OF ALBANY EXPANSION SITE



LOCATION MAP

GMP BRIDGE PLANS JUNE 8, 2022

TOWN OF BETHLEHEM
ALBANY COUNTY
NEW YORK



VICINITY MAP

PREPARED FOR:



ALBANY PORT DISTRICT COMMISSION
106 SMITH BOULEVARD
ALBANY, NEW YORK
(518) 463-8763
WWW.ALBANY.GOV

PREPARED BY:



MCFARLAND JOHNSON PROJECT # 18641.00

| | | |
|---------|----------------|--|
| SEALED | LAURA A. SANDA | |
| PE_NO | 082966 | |
| PE_DATE | JUNE 8, 2022 | |

IT IS A VIOLATION OF THE LAW FOR ANY PERSON, UNLESS ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.



McFarland Johnson
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 SUITE 402
 SARATOGA SPRINGS, NEW YORK 12866
 P: 518-580-9380 F: 518-580-9383
 SaratogaROM@mjinc.com

PROJECT MILESTONE
GMP PLANS

| NO. | DATE | DESCRIPTION |
|-----|----------|-----------------------|
| 1 | 06/08/22 | GMP BID SET |
| 2 | 10/28/22 | GMP BID SET REVISIONS |

CLIENT: **ALBANY PORT DISTRICT COMMISSION**
 ALBANY, NEW YORK
 PROJECT: **PORT OF ALBANY SITE INFRASTRUCTURE IMPROVEMENTS**

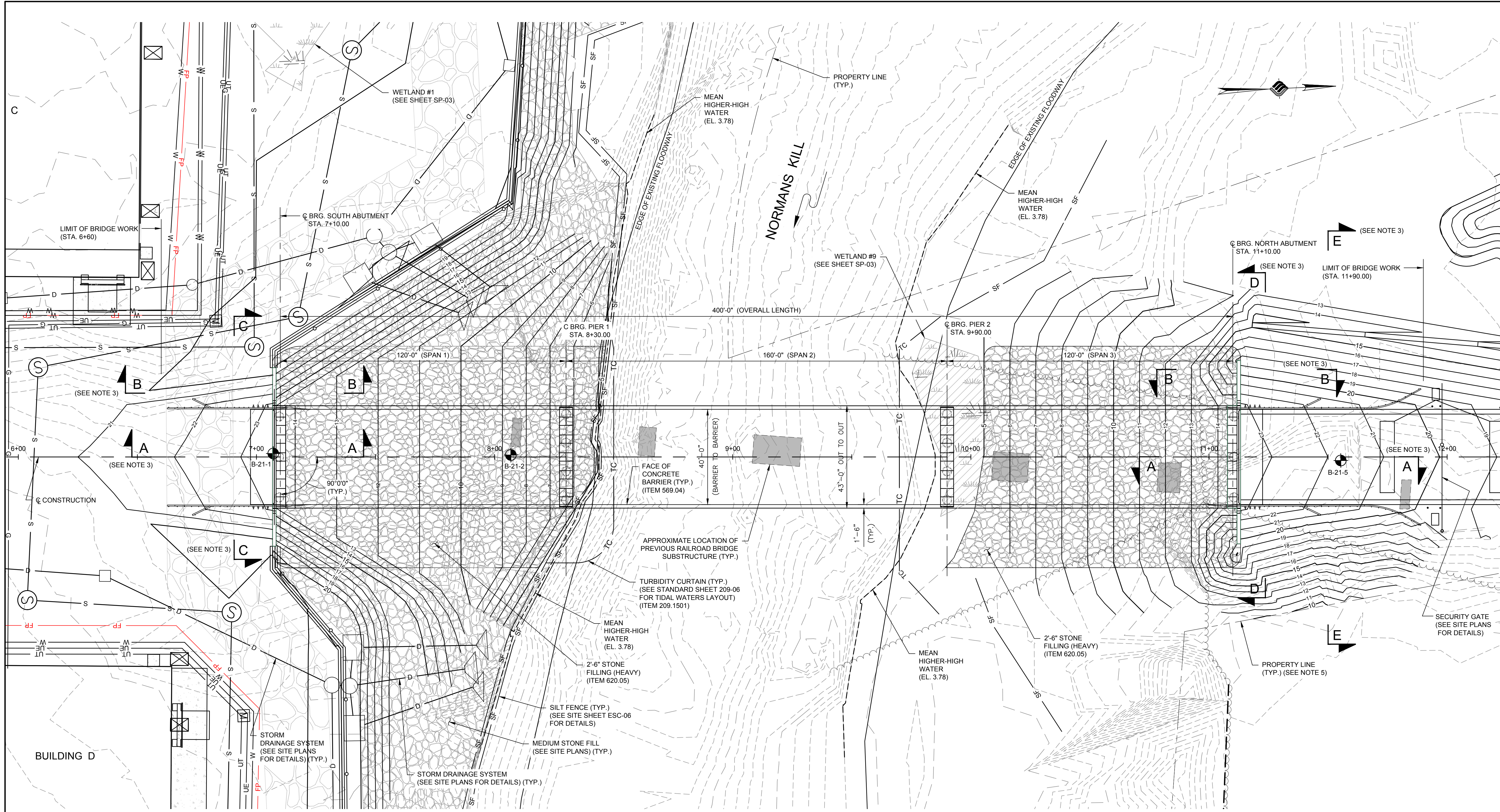
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| DRAWN | SLM |
| DESIGNED | JLR |
| CHECKED | RLJ |
| SCALE | AS SHOWN |
| DATE | 6/8/2022 |
| PROJECT | 18641.00 |



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DRAWING TITLE
BRIDGE SITE PLAN

DRAWING NUMBER
ST-01



BRIDGE SITE PLAN
 SCALE: 1" = 20'

| LEGEND | |
|--------------------------------|---------------------|
| PROPERTY LINE | --- |
| EXISTING TREE LINE | ~~~~~ |
| DITCH CENTERLINE | --- |
| ROADSIDE SWALE | --- |
| SILT FENCE | — SF — (SEE NOTE 2) |
| TURBIDITY CURTAIN | — TC — (SEE NOTE 2) |
| WETLAND AREA | |
| WETLAND AREA PERMANENT IMPACTS | |

- NOTES:**
- FOR BORING LOGS, SEE GEOTECHNICAL REPORT.
 - EROSION PROTECTION AND SEDIMENT CONTROL AND WATER HANDLING SHALL BE DESIGNED BY THE CONTRACTOR.
 - FOR SECTIONS A-A, B-B, C-C, D-D, AND E-E, SEE SHEETS ST-07 AND ST-08.
 - FOR ADDITIONAL PROPOSED GRADING INFORMATION, SEE SITE PLANS.
 - ALL PROPOSED GRADING SHALL BE WITHIN THE LIMITS OF THE EXISTING PROPERTY LINES.



N:\18641\05 ALBANY PORT EXPANSION\DRAWINGS\SHEET FILES\BRIDGE FINAL PLANS\ST-01.DWG (DATE: 06/08/22) (OVERALL DWG)



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SARATOGA SPRINGS, NEW YORK 12866
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CLIENT: **ALBANY PORT DISTRICT COMMISSION**

ALBANY, NEW YORK

PROJECT: **PORT OF ALBANY SITE INFRASTRUCTURE IMPROVEMENTS**

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|----------|----------|
| DRAWN | SLM |
| DESIGNED | JLR |
| CHECKED | RLJ |
| SCALE | 1" = 30' |
| DATE | 6/8/2022 |
| PROJECT | 18461.00 |



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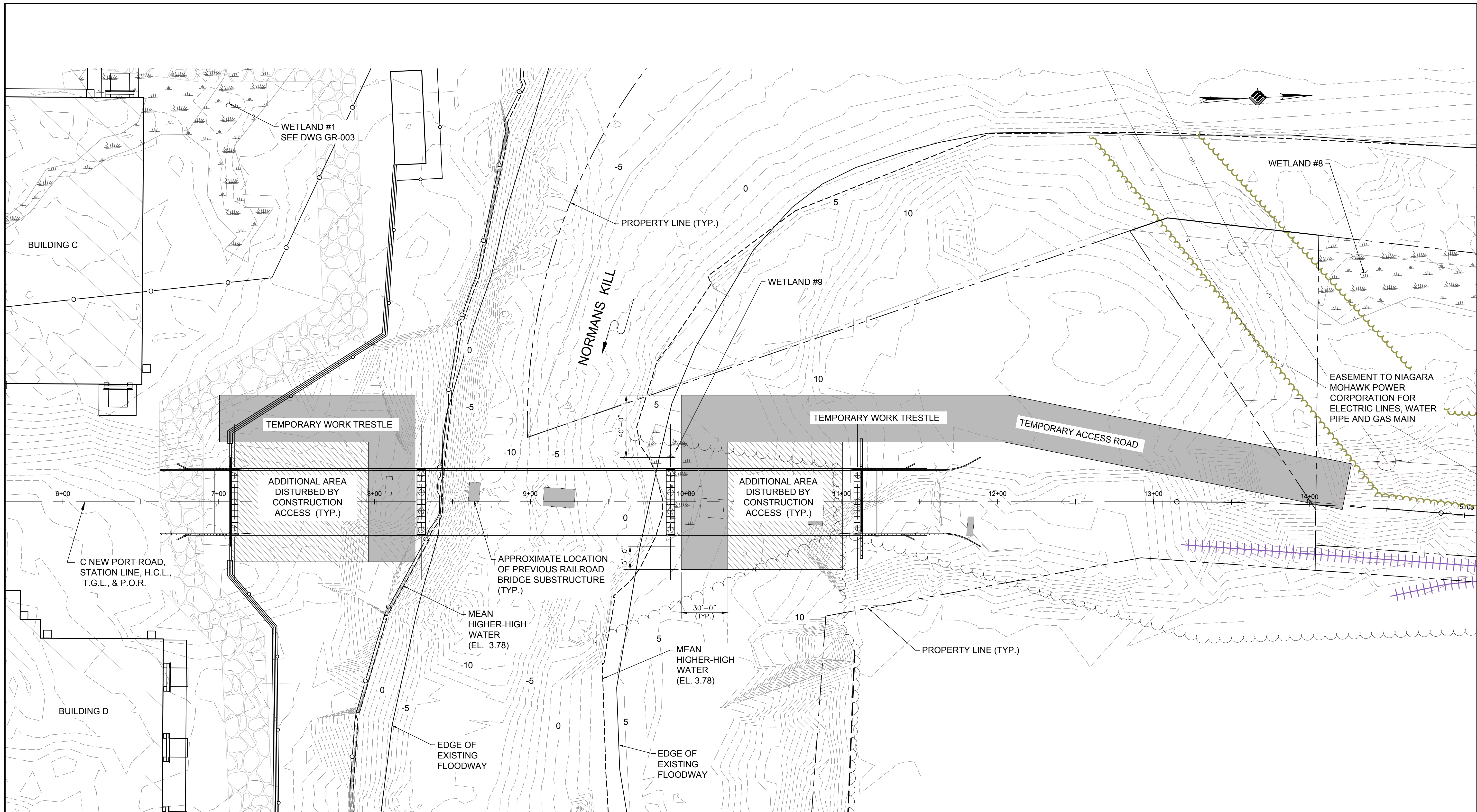
DRAWING TITLE

BRIDGE CONSTRUCTION ACCESS PLAN

DRAWING NUMBER

ST-02

2 OF 36



BRIDGE CONSTRUCTION ACCESS PLAN

SCALE: 1" = 30'

LEGEND

| | |
|--------------------|-------|
| PROPERTY LINE | --- |
| EXISTING RAIL LINE | +++++ |
| EXISTING TREE LINE | ~~~~~ |
| WETLAND AREA | |

NOTES:

- NO FILL IS ALLOWED BETWEEN THE MEAN HIGHER-HIGH WATER LINES.
- TEMPORARY CONSTRUCTION ACCESS SHALL BE OUTSIDE THE LIMITS OF MEAN HIGHER-HIGH WATER.
- PILE FOUNDATIONS FOR THE TEMPORARY WORK TRESTLE SHALL BE VIBRATED IN, RATHER THAN DRIVEN WITH AN IMPACT HAMMER. AN IMPACT HAMMER WOULD BE USED ONLY TO SEAT THE PILE WITHIN THE FIRST FEW INCHES IN THE TOP OF ROCK. IF NECESSARY, CUSHION BLOCKS, AIR BUBBLES, CURTAIN OR OTHER NOISE ATTENUATING TOOLS SHOULD BE IMPLEMENTED TO AVOID REACHING NOISE LEVELS THAT COULD CAUSE IMPACTS TO STURGEON LOCATED WITHIN THE PROJECT LIMITS.
- ALL WORK SHALL BE IN ACCORDANCE WITH ENVIRONMENTAL PERMITS.



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CLIENT: **ALBANY PORT DISTRICT COMMISSION**
 ALBANY, NEW YORK
 PROJECT: **PORT OF ALBANY SITE INFRASTRUCTURE IMPROVEMENTS**

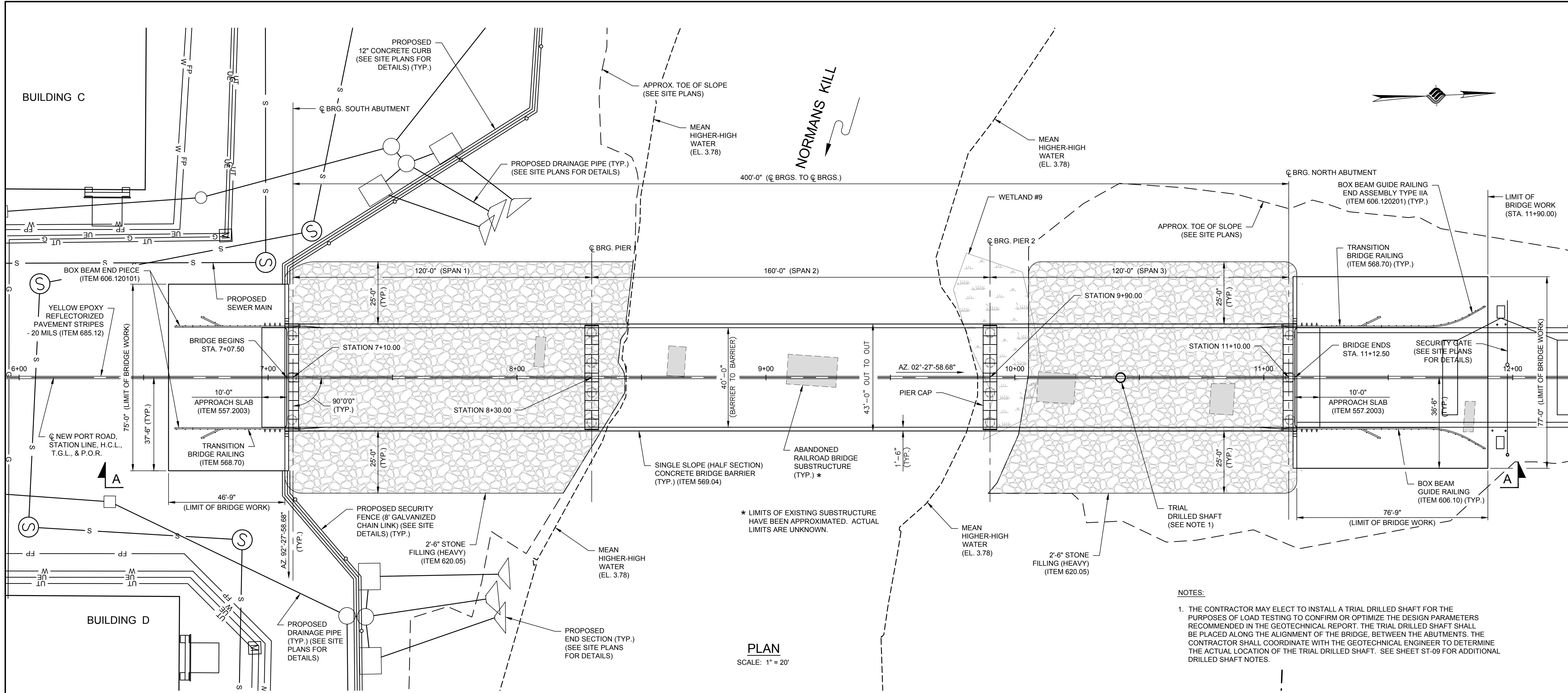
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| DRAWN | SLM |
| DESIGNED | JLR |
| CHECKED | RLJ |
| SCALE | AS SHOWN |
| DATE | 6/8/2022 |
| PROJECT | 18641.00 |



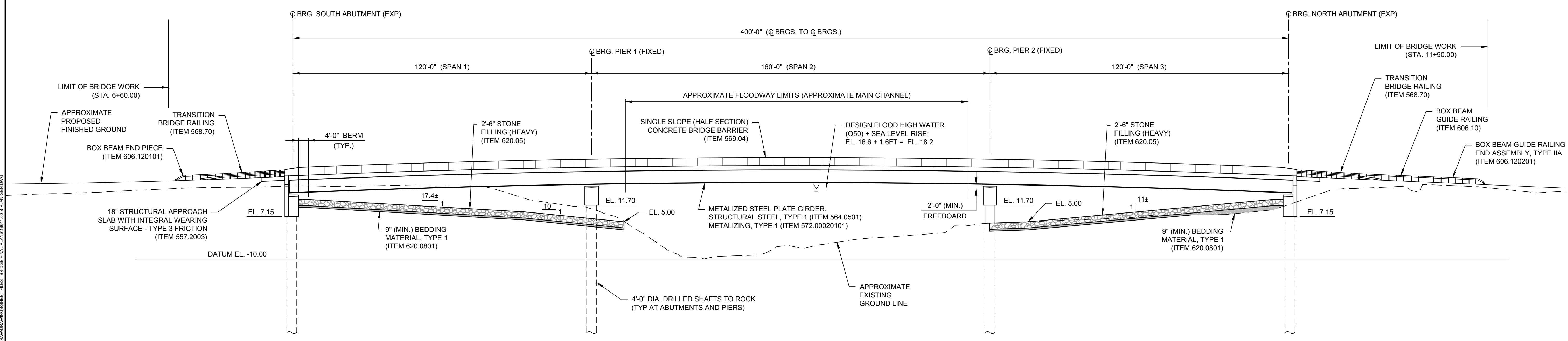
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DRAWING TITLE
GENERAL PLAN AND ELEVATION

DRAWING NUMBER
ST-03



- NOTES:
- THE CONTRACTOR MAY ELECT TO INSTALL A TRIAL DRILLED SHAFT FOR THE PURPOSES OF LOAD TESTING TO CONFIRM OR OPTIMIZE THE DESIGN PARAMETERS RECOMMENDED IN THE GEOTECHNICAL REPORT. THE TRIAL DRILLED SHAFT SHALL BE PLACED ALONG THE ALIGNMENT OF THE BRIDGE, BETWEEN THE ABUTMENTS. THE CONTRACTOR SHALL COORDINATE WITH THE GEOTECHNICAL ENGINEER TO DETERMINE THE ACTUAL LOCATION OF THE TRIAL DRILLED SHAFT. SEE SHEET ST-09 FOR ADDITIONAL DRILLED SHAFT NOTES.



N:\18641\03 ALBANY PORT EXPANSION\DRAWINGS\SHEET FILES\BRIDGE PLAN\ALBANY PORT DISTRICT COMMISSION.DWG



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PORT OF ALBANY SITE INFRASTRUCTURE IMPROVEMENTS

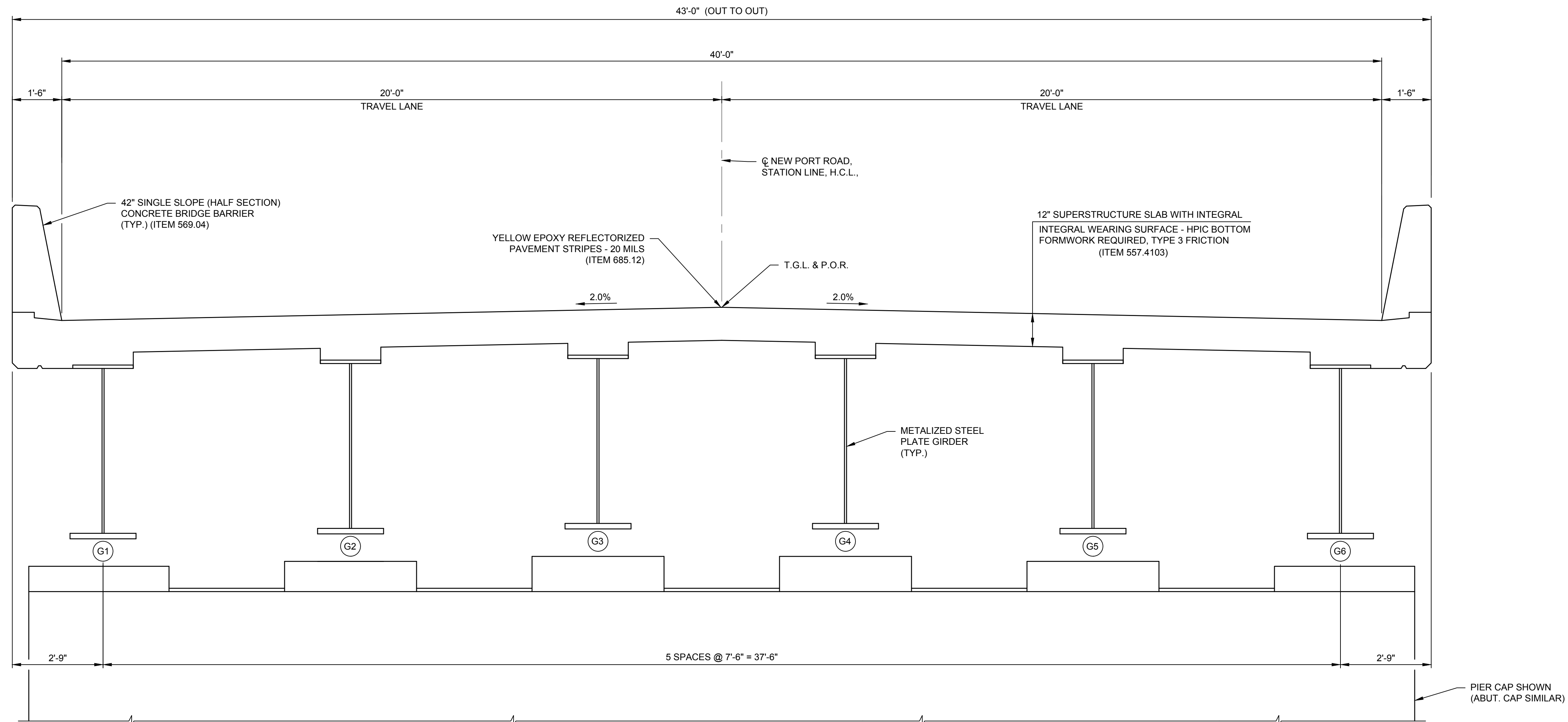
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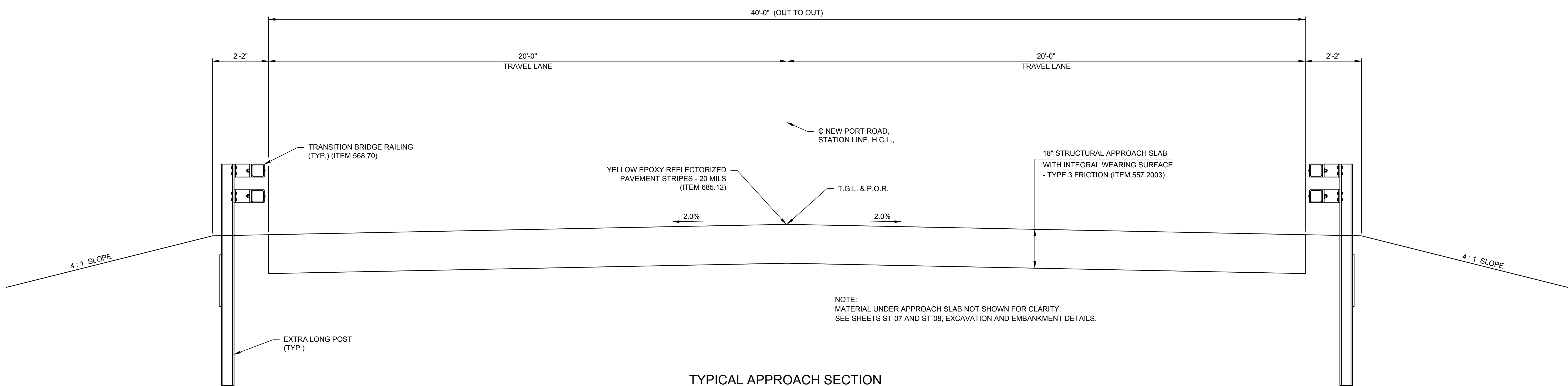
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DRAWING TITLE
TYPICAL SECTIONS

DRAWING NUMBER
ST-04



TYPICAL BRIDGE SECTION
 SCALE: 1/2" = 1'-0"



NOTE:
 MATERIAL UNDER APPROACH SLAB NOT SHOWN FOR CLARITY.
 SEE SHEETS ST-07 AND ST-08, EXCAVATION AND EMBANKMENT DETAILS.

TYPICAL APPROACH SECTION
 SCALE: 1/2" = 1'-0"

NUMBER OF ALBANY PORT EXPANSION DRAWINGS SHEET FILES: BRIDGE FINAL PLANS WITH SUBJECT APPROVALS



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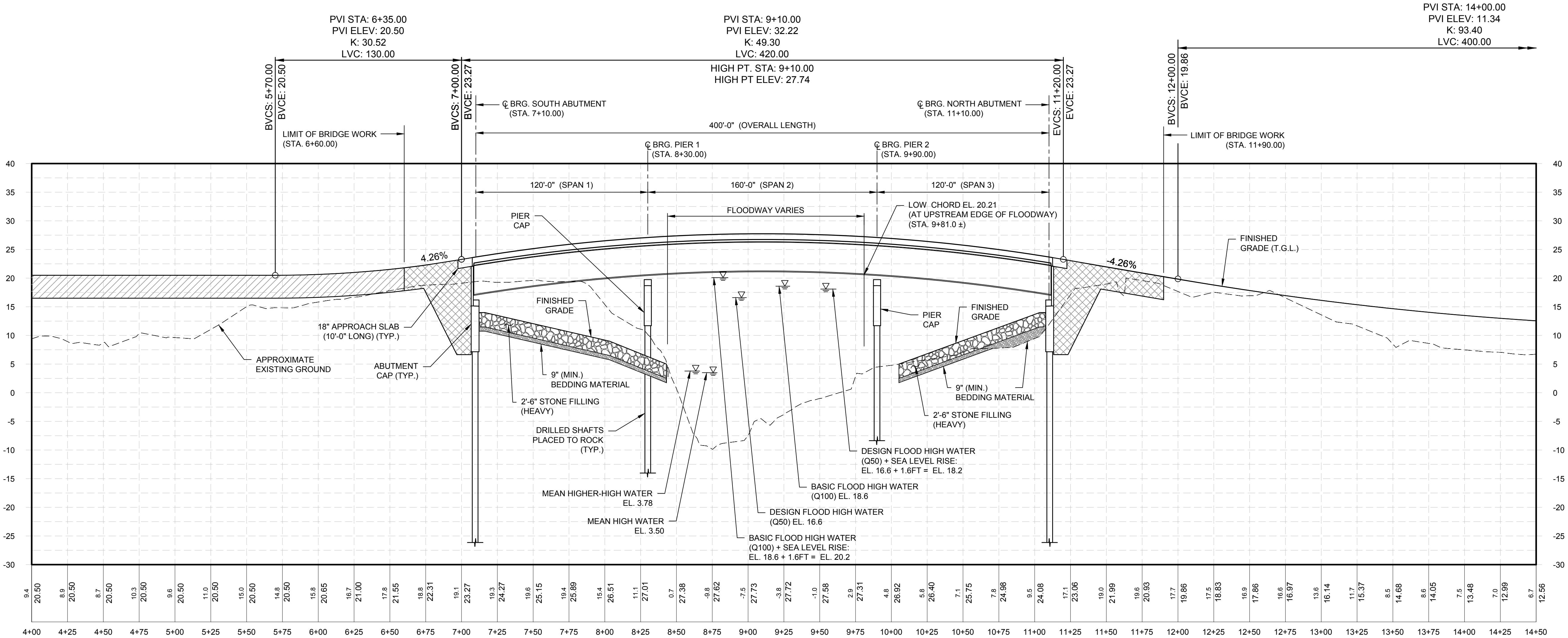
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DRAWING TITLE
BRIDGE PROFILE

DRAWING NUMBER
ST-05



LEGEND

| | |
|--|--|
| | GEOSYNTHETIC REINFORCEMENT (ITEM 207.96000017) (YARD/ROADWAY WORK) |
| | GEOSYNTHETIC REINFORCEMENT (ITEM 207.96000017) (BRIDGE WORK) |

BRIDGE PROFILE
 HORIZONTAL SCALE: 1" = 40'
 VERTICAL SCALE: 1" = 10'

N:\18641.00\ALBANY PORT EXPANSION\DRAWINGS\BRIDGE FILES\BRIDGE FINAL PLAN\ST-05.BRIDGE.DWG

GENERAL NOTES:

- DESIGN SPECIFICATIONS: NYSDOT LRFD BRIDGE DESIGN SPECIFICATIONS WITH ALL PROVISIONS IN EFFECT AS OF MARCH 2022.
- LIVE LOAD: AASHTO HL-93 AND SITE-SPECIFIC DESIGN VEHICLES AS DESCRIBED IN THE SPECIAL NOTES.
- FOR DESIGN PURPOSES, COMPRESSIVE STRENGTH OF CONCRETE SHALL BE: SUPERSTRUCTURE SLAB (ITEM 557.4413) $f_c = 4000$ PSI (CLASS HPIC) APPROACH SLABS (ITEM 557.2003) $f_c = 4000$ PSI (CLASS HP) DRILLED SHAFTS (ITEM 551.50040) $f_c = 5000$ PSI (CLASS GG) ABUTMENT & PIER CAPS (ITEM 555.02000001) $f_c = 5000$ PSI (CLASS MP) BACKWALLS, WINGWALLS & PEDESTALS (ITEM 555.09) $f_c = 4000$ PSI (CLASS HP)
- DESIGN SPECIFICATIONS: NEW YORK STATE DEPARTMENT OF TRANSPORTATION STANDARD SPECIFICATIONS FOR HIGHWAY BRIDGES WITH ALL PROVISIONS IN EFFECT AS OF MARCH 2022.
- CONSTRUCTION SPECIFICATIONS: NYSDOT STANDARD SPECIFICATIONS, CONSTRUCTION AND MATERIALS, WITH ALL PROVISIONS IN EFFECT AS OF MARCH 2022.
- DETAILS ON THE DRAWINGS LABELED AS "NOT TO SCALE" ARE INTENTIONALLY DRAWN NOT TO SCALE FOR VISUAL CLARITY. ALL OTHER DETAILS FOR WHICH NO SCALE IS SHOWN ARE DRAWN PROPORTIONAL AND ARE FULLY DIMENSIONED.
- ALL SHOP DRAWINGS SUBMITTED FOR THIS PROJECT SHALL BE IN U.S. CUSTOMARY UNITS.
- THIS BRIDGE SHALL BE MAINTAINED IN ACCORDANCE WITH THE GUIDELINES CONTAINED IN THE CURRENT EDITION OF THE AASHTO MAINTENANCE MANUAL FOR ROADWAYS AND BRIDGES AND THE ALBANY PORT DISTRICT COMMISSION (APDC) BRIDGE MAINTENANCE PROGRAM.
- IF POTENTIAL FUTURE LOADS DEVIATE FROM THE SITE-SPECIFIC DESIGN VEHICLES DESCRIBED IN THE PROJECT SPECIAL NOTES, A LOAD RATING OF THE BRIDGE SHALL BE PREPARED BY A PROFESSIONAL ENGINEER LICENSED IN THE STATE OF NEW YORK. THE LOAD RATING SHALL CONSIDER THE NUMBER AND SPACING OF AXLES, THE WEIGHT ON EACH AXLE AND THE FREQUENCY OF EACH VEHICULAR CONFIGURATION.

FOUNDATION NOTES:

- ALL FOUNDATION AND SITE PREPARATION WORK SHALL BE IN ACCORDANCE WITH THE GEOTECHNICAL ENGINEER REPORT PREPARED BY TERRACON CONSULTANTS AND DATED JANUARY 21, 2022.
- NEW EMBANKMENT FILL AND BACKFILL FOR THE BRIDGE ABUTMENTS AND WINGWALLS MUST BE COMPLETED USING STRUCTURAL FILL WHICH MEETS THE REQUIREMENTS FOR TYPE 2 SUBBASE IN SECTION 304 OF THE NYSDOT STANDARD SPECIFICATIONS FOR CONSTRUCTION AND MATERIALS. REUSE OF EXCAVATED ONSITE SOILS SHALL BE AVOIDED.
- ABUTMENT AND WINGWALL BACKFILL, ROADWAY AND EMBANKMENT FILLS SHOULD BE PLACED IN UNIFORM LOOSE LAYERS NO MORE THAN 12" THICK. EACH LIFT SHOULD BE COMPACTED TO NO LESS THAN 95 PERCENT OF ITS PROCTOR MAXIMUM DRY DENSITY.
- EXCAVATIONS MUST BE PERFORMED IN ACCORDANCE WITH OSHA 29 CFR, PART 1926, SUBPART P AND ITS APPENDICES, ALONG WITH ANY STATE AND LOCAL CODES, AS APPLICABLE. STRUCTURE EXCAVATION SHALL BE INCLUDED UNDER ITEM 206.01. CLEARING AND GRUBBING SHALL BE INCLUDED UNDER ITEM 201.06.
- ANY EXCAVATION BRACING SHALL BE DESIGNED BY A REGISTERED PROFESSIONAL ENGINEER IN THE STATE OF NEW YORK.
- IF EXCAVATION PENETRATES THE GROUNDWATER LEVEL TO ENABLE FOUNDATION CONSTRUCTION IT SHALL BE DEWATERED CONTINUOUSLY UNTIL THE FOUNDATIONS ARE CONSTRUCTED AND ADEQUATE LOAD IS APPLIED TO RESIST UPLIFT FORCES. WHILE THE METHOD OF DEWATERING IS THE RESPONSIBILITY OF THE CONTRACTOR, IT SHOULD BE EXPECTED THAT SHEET PILE OR SLURRY CUT OFF WALLS AND PUMPING OF LARGE QUANTITIES OF WATER AND ITS PROPER HANDLING SHALL BE REQUIRED.
- A MINIMUM 24-INCH-THICK BASE OF CLEAN CRUSHED STONE SHALL BE PLACED OVER A DRAINAGE/SEPARATION GEOTEXTILE BENEATH THE BRIDGE FOUNDATIONS IF THEY ARE FOUNDED BELOW GROUNDWATER LEVEL. THE STONE SHALL BE AN ASTM C33 BLEND 57 AGGREGATE AND THE GEOTEXTILE A NON-WOVEN SYNTHETIC FILTER FABRIC MEETING NYSDOT STANDARD SPECIFICATIONS SECTION 737-01 FOR DRAINAGE GEOTEXTILE.
- ALL EARTHWORK EFFORTS SHALL BE MONITORED UNDER THE DIRECTION OF THE GEOTECHNICAL ENGINEER.
- DRILLED SHAFT CONSTRUCTION SHALL BE MONITORED ON A FULL-TIME BASIS BY THE GEOTECHNICAL ENGINEER.
- BRIDGE APPROACH EMBANKMENTS SHALL BE FULLY REINFORCED WITH BIAXIAL GEOGRID WITH A MINIMUM LONG TERM DESIGN STRENGTH OF 1,000 POUNDS PER FOOT. THE GEOGRID REINFORCEMENT SHALL BE PLACED IN HORIZONTAL LAYERS WITH VERTICAL SPACING BETWEEN LAYERS NOT TO EXCEED 12 INCHES.

SUBSTRUCTURE NOTES:

- TOP OF BACKWALLS SHALL BE STEEL TROWEL FINISHED. TWO SHEETS OF SHEET GASKET (TREATED BOTH SIDES), §728-06, SHALL BE PLACED ON THE TOP OF THE BACKWALLS OF EXPANSION ABUTMENTS. THE CONTRACTOR SHALL INCLUDE THIS WORK IN THE APPROACH SLAB ITEM.
- THE CONTRACTOR, WITH THE PERMISSION OF THE ENGINEER IN CHARGE (EIC), MAY ELECT TO INTRODUCE CONSTRUCTION JOINTS IN THE ABUTMENTS AT LOCATIONS NOT SHOWN ON THE PLANS. THESE CONSTRUCTION JOINTS SHALL BE PROVIDED WITH SHEAR KEYS AND WATERSTOPS. VERTICAL CONSTRUCTION JOINTS INTRODUCED IN THE BACKWALL SHALL PREFERABLY BE PLACED MIDWAY BETWEEN THE PEDESTALS.
- THE WORK AND MATERIALS REQUIRED AT EACH CONSTRUCTION JOINT, CONSTRUCTION JOINT AND CONCRETE EXPANSION JOINT SHALL BE INCLUDED IN THE VARIOUS ITEMS OF THE CONTRACT.

HYDRAULIC NOTES

| | |
|---------------------------------|-------|
| 23. MEAN LOWER LOW WATER (MLLW) | -1.81 |
| MEAN LOW WATER (MLW) | -1.59 |
| MEAN TIDE LEVEL (MTL) | 0.91 |
| MEAN HIGH WATER (MHW) | 3.40 |
| MEAN HIGHER HIGH WATER (MHHW) | 3.78 |
| HIGHEST OBSERVED TIDE | 9.27 |
| Q50 ELEVATION | 16.6 |
| Q100 ELEVATION | 18.6 |

SUPERSTRUCTURE NOTES:

- ALL STRUCTURAL STEEL SHALL CONFORM TO ASTM A709, GRADE 50.
- DIAPHRAGMS SHALL BE FABRICATED TO FIT GIRDERS ERRECTED WITH THEIR WEBS PLUMB UNDER FULL DEAD LOAD CONDITIONS, ALSO KNOWN AS TOTAL DEAD LOAD FIT (TDLF).
- THE DIAPHRAGMS, INCLUDING CONNECTIONS, ARE CONSIDERED PRIMARY MEMBERS. THEY SHALL BE FABRICATED IN ACCORDANCE WITH THE NEW YORK STATE STEEL CONSTRUCTION MANUAL UNLESS NOTED OTHERWISE.
- ALL BOLTED CONNECTIONS FOR THE DIAPHRAGMS SHALL BE MADE WITH 1" DIA., ASTM F3125, GRADE A325, TYPE 1 HDG, HIGH-STRENGTH BOLTS.
- ALL STRUCTURAL STEEL CONNECTION FAYING SURFACES SHALL BE FABRICATED TO HAVE A CLASS B SURFACE CONDITION (KS=0.5 MIN).

METALIZING NOTES:

- ALL GIRDERS, INCLUDING BEARING STIFFENERS AND CONNECTION PLATES, SHALL BE METALIZED. METALIZING SHALL WRAP 1 INCH OVER THE TOP FLANGE. THE METALIZING SHALL BE INCLUDED UNDER ITEM 572.00020101.
- ALL PRIMARY AND SECONDARY MEMBERS SHALL BE METALIZED. THE METALIZING SHALL BE INCLUDED UNDER ITEM 572.00020101 - METALIZING. THE CONTRACTOR HAS THE OPTION OF HOT-DIP GALVANIZING THE SECONDARY MEMBERS. IF THE CONTRACTOR ELECTS TO HOT-DIP GALVANIZE THESE MEMBERS, HOT-DIP GALVANIZING SHALL MEET THE REQUIREMENTS OF §719-01 TYPE I OF THE NYSDOT STANDARD SPECIFICATIONS - CONSTRUCTION AND MATERIALS. THE HOT-DIP GALVANIZING SHALL BE INCLUDED UNDER ITEM 572.00020101 - METALIZING.
- STEEL COMPONENTS OF THE BRIDGE BEARINGS, INCLUDING THE SOLE PLATE AND THE MASONRY PLATE, SHALL BE METALIZED AND THE METALIZING SHALL BE INCLUDED IN THE METALIZING ITEM.
- ALL EXPOSED STEEL SURFACES ON DIAPHRAGMS AND SHIMS SHALL BE METALIZED UNDER ITEM 572.00020101.
- ALL BOLTS, NUTS, AND WASHERS SHALL BE GALVANIZED IN ACCORDANCE WITH THE NEW YORK STATE STEEL CONSTRUCTION MANUAL.

STEEL ERECTION NOTES:

- THE CONTRACTOR SHALL PROVIDE FOR THE STABILITY OF STRUCTURAL STEEL DURING ALL PHASES OF ERECTION AND CONSTRUCTION, IN ACCORDANCE WITH SUBSECTION 204 OF THE NEW YORK STATE STEEL CONSTRUCTION MANUAL (SCM). THE GIRDERS ON THIS BRIDGE SHALL BE STABILIZED DURING ERECTION BY USE OF FALSEWORK, TEMPORARY BRACING, COMPRESSION FLANGE STIFFENING TRUSSES, CHOOSING ALTERNATE PICKING POINTS, OR BY USE OF A HOLDING CRANE UNTIL A SUFFICIENT NUMBER OF GIRDERS HAVE BEEN ERRECTED AND DIAPHRAGMS INSTALLED. THE METHODS USED BY THE CONTRACTOR SHALL BE DOCUMENTED ON THE ERECTION DRAWINGS WITH ALL SUPPORTING STABILITY CALCULATIONS SUBMITTED AND STAMPED BY A LICENSED AND REGISTERED NEW YORK STATE PROFESSIONAL ENGINEER AND SUBMITTED TO THE EIC IN ACCORDANCE WITH THE SCM.
- THE DESIGN OF THIS STRUCTURE ASSUMES THAT THE STRUCTURAL STEEL IS COMPLETELY ERRECTED BEFORE IT IS ALLOWED TO DEFLECT UNDER ITS OWN DEAD LOAD. DEFLECTIONS INCURRED DURING THE VARIOUS STAGES OF THE ERECTION METHOD ARE NOT CONSIDERED. THEREFORE, THE ACTUAL ERECTION METHODS AND SEQUENCES EMPLOYED BY THE CONTRACTOR MAY HAVE A SUBSTANTIAL EFFECT ON THE FINAL STEEL PROFILE. THE CONTRACTOR SHALL BE RESPONSIBLE FOR TAKING ALL NECESSARY COMPENSATORY ACTION TO ENSURE THAT THE FINAL ALIGNMENT AND PROFILE OF THE ERRECTED STEEL CONFORMS TO SUBSECTION 1213, 1214, AND 1215 OF THE SCM. ANY CORRECTIVE WORK NECESSARY TO RE-POSITION PREVIOUSLY ERRECTED STEEL TO ACHIEVE ACCEPTABLE ALIGNMENT AND PROFILE MUST BE APPROVED BY THE EIC AND SHALL BE PERFORMED AT NO ADDITIONAL COST TO THE OWNER.
- IF THE CONTRACTOR ELECTS TO MOVE THE SPLICE LOCATION SHOWN ON THE PLANS THE CONTRACTOR SHALL HAVE A LICENSED AND REGISTERED NEW YORK STATE PROFESSIONAL ENGINEER REDESIGN THE SPLICE. THE SHOP DRAWINGS AND DESIGN CALCULATIONS FOR THE REDESIGNED SPLICE SHALL BE RESUBMITTED TO THE EIC IN ACCORDANCE WITH THE SCM. REDESIGN SHALL BE INCLUDED IN THE STEEL BID ITEM.
- OVERSIZED HOLES SHALL NOT BE PERMITTED FOR THE DIAPHRAGM MEMBERS OR THEIR CONNECTIONS.
- DIMENSIONS FOR THICKNESSES OF STEEL ROLLED ANGLE SHAPES AND STRUCTURAL TUBING ARE SHOWN ACCORDING TO THE AISC MANUAL.

SUPERSTRUCTURE SLAB NOTES:

- THE CONTRACTOR'S ATTENTION IS DIRECTED TO THE NON-STANDARD SUPERSTRUCTURE SLAB THICKNESS AND COMPRESSIVE STRENGTH. THE PROVISIONS OF THE CURRENT SPECIFICATIONS FOR PAYMENT OF ITEM 557.4103 SHALL STILL APPLY. THE DESIGN OF FORMWORK AND ANY TEMPORARY BRACING FOR THE INCREASED LOAD IS THE CONTRACTOR'S RESPONSIBILITY.
- THE PROVISIONS OF THE CURRENT SPECIFICATIONS FOR SUPERSTRUCTURE SLABS ALLOW THE OPTION OF 3 FORMING SYSTEMS FOR THE UNDERSIDE OF THE SLABS. ON THIS BRIDGE, ONLY THE FOLLOWING OPTION WILL BE PERMITTED: PERMANENT CORRUGATED METAL FORMS.
- IN ORDER TO PREVENT MOVEMENT OF THE BRIDGE OVERHANG BRACKET DURING THE DECK CONCRETE PLACEMENT, AS WELL AS TO PREVENT LATERAL DISTORTION OF THE GIRDER WEB, A DEEP OVERHANG BRACKET THAT IS BRACED BY THE BOTTOM FLANGE SHALL BE USED.
- THE CONTRACTOR SHALL MAKE NO DEVIATIONS FROM THE HAUNCH DETAILS SHOWN ON THESE PLANS WITHOUT THE PERMISSION OF THE EIC.
- THE DETAILS FOR THE BARRIER REINFORCEMENT ARE FOR THE SLIP-FORMED OR CAST-IN-PLACE OPTION ONLY. BARRIER AND ANCHORAGE REINFORCEMENT ORIGINATING IN THE SLAB SHALL BE INCLUDED IN THE BARRIER ITEM.
- THE PROVISIONS OF THE CURRENT SPECIFICATIONS FOR PERMANENT CONCRETE TRAFFIC BARRIER FOR STRUCTURES ALLOWS THE OPTION OF THREE CONSTRUCTION METHODS: CAST-IN-PLACE, SLIP FORMED, OR PRECAST. HOWEVER, ON THIS BRIDGE, ONLY CAST-IN-PLACE AND SLIP FORMING ARE ALLOWED FOR THE SINGLE SLOPE (HALF-SECTION) CONCRETE BRIDGE BARRIER.
- TOP SURFACES OF NEW BRIDGE DECKS AND APPROACH SLABS SHALL BE SEALED IN ACCORDANCE WITH ITEM 559.01 - PROTECTIVE SEALING OF STRUCTURAL CONCRETE ON NEW BRIDGE DECKS AND BRIDGE DECK OVERLAYS.
- NO BAR LIST IS PROVIDED FOR SUPERSTRUCTURE SLABS OR APPROACH SLABS. §557-3.17 OF THE STANDARD SPECIFICATIONS SHALL APPLY. EXCEPT THAT THE EIC WILL TRANSMIT THE DOCUMENTS TO THE DESIGNER FOR REVIEW FOR CONFORMANCE WITH THE DESIGN REQUIREMENTS IN THE CONTRACT DOCUMENTS. A REVIEW TIME OF TWO DAYS PER PLACEMENT DRAWING SUBMITTED WITH A MINIMUM OF 15 DAYS FOR EACH SUBMISSION WILL BE ALLOWED UPON RECEIPT OF THE SUBMISSION. THE DESIGNER WILL NOT BE

- CHECKING LENGTHS, NUMBER OF BARS, WEIGHTS, OR BAR MARKS. CORRECTIONS WILL BE RETURNED TO THE CONTRACTOR. WHEN THE DOCUMENTS ARE SATISFACTORY, THEY WILL BE RETURNED TO THE CONTRACTOR STAMPED "REVIEWED". PLACEMENT DRAWINGS REQUIRED SHALL INCLUDE PLAN VIEW DRAWINGS, SECTION VIEWS, TRANSVERSE SECTIONS, AND ANY OTHER DETAILS REQUIRED TO CLARIFY THE REBAR PLACEMENT.
- CARE SHALL BE TAKEN TO PREVENT CONTAMINATION OF THE WATERWAY BY THE CONCRETE SEALER. IF THE MANUFACTURER'S INSTRUCTIONS REQUIRE MIXING OF THE SEALER PRIOR TO APPLICATION, MIXING SHALL OCCUR IN A MANNER THAT WILL PREVENT CONTAMINATION OF THE WATERWAY. THE CONTRACTOR SHALL HAVE AVAILABLE FOR IMMEDIATE USE MATERIALS TO SOAK UP OR CONTAIN ANY ACCIDENTAL SPILLS. PRIOR TO THE APPLICATION OF THE SEALER, ANY OPENINGS IN THE SURFACE OF THE BRIDGE DECK OR IN THE WALKING SURFACE, SUCH AS SCUPPERS OR OPEN DRAINS SHALL BE COVERED TO PREVENT CONTAMINATION OF THE WATERWAY. CARE SHALL BE TAKEN TO PREVENT SPRAYED SEALER FROM ENTERING THE WATERWAY BY ROLLING THE SEALER OR BY PHYSICALLY ISOLATING THE AREA TO BE SPRAYED FROM THE WATERWAY BY THE USE OF TARPS OR OTHER BARRIER-TYPE MEANS TO THE SATISFACTION OF THE EIC.

STREAM PROTECTION NOTE:

- DURING THE COURSE OF CONSTRUCTION, THE CONTRACTOR SHALL CONDUCT OPERATIONS IN SUCH A MANNER AS TO PREVENT OR REDUCE TO A MINIMUM ANY DAMAGE TO ANY STREAM FROM POLLUTION BY DEBRIS, SEDIMENT, OR OTHER FOREIGN MATERIAL, OR FROM MANIPULATION OF EQUIPMENT AND/OR MATERIALS IN OR NEAR SUCH STREAMS. THE CONTRACTOR SHALL NOT RETURN DIRECTLY TO A STREAM ANY WATER WHICH HAS BEEN USED FOR WASH PURPOSES OR OTHER SIMILAR OPERATIONS WHICH CAUSE THIS WATER TO BECOME POLLUTED WITH SAND, SILT, CEMENT, OIL, OR OTHER IMPURITIES. IF THE CONTRACTOR USES WATER FROM A STREAM, THE CONTRACTOR SHALL CONSTRUCT AN INTAKE OR TEMPORARY DAM REQUIRED TO PROTECT AND MAINTAIN WATER RIGHTS AND TO SUSTAIN FISH LIFE DOWNSTREAM.

DECK PLACEMENT NOTES:

- CONCRETE PLACEMENT AND FINISHING OPERATIONS SHALL BE PERFORMED AS RAPIDLY AS POSSIBLE. THE EIC MAY DIRECT THE CONTRACTOR TO STOP PLACEMENT OPERATIONS AT ANY TIME IF, IN THE ENGINEER'S OPINION, CONCRETE PLACED DURING THE PLACEMENT HAS STARTED TO SET, OR IS ABOUT TO SET, AND FURTHER PLACEMENT OF CONCRETE WILL CAUSE DEFLECTION CRACKING.
- LONGITUDINAL CONSTRUCTION JOINTS NOT SHOWN IN THE PLANS ARE NOT PERMITTED.
- THE CONTRACTOR SHALL OPERATE FINISHING MACHINE(S) AS CLOSE TO THE SKEW ANGLE AS PRACTICABLE FOR SKEW ANGLES BETWEEN 0° AND 35°. WHEN THE SKEW ANGLE IS GREATER THAN 35° THE FINISHING MACHINE(S) SHALL BE OPERATED AT A MAXIMUM SKEW ANGLE OF 35°.
- THE CONTRACTOR SHALL PLACE WET BURLAP CURING BLANKETS ON THE CONCRETE DECK WITHIN 30 MINUTES OF THE CONCRETE BEING DEPOSITED INTO THE FORMS OR 5 MINUTES AFTER FINISHING, WHICHEVER COMES FIRST. THE PLACEMENT OF THE TURF DRAG TEXTURE SHALL NOT INTERFERE WITH THESE REQUIREMENTS.
- IF THE CONTRACTOR'S DECK PLACEMENT OPERATION IS STOPPED PRIOR TO COMPLETION, WHETHER BY THE CONTRACTOR'S DECISION OR BY DIRECTION OF THE EIC, THE CONTRACTOR SHALL PROVIDE A FINISHED DECK GRADE WHICH MATCHES THE PLANNED PROFILE. ANY SUBSEQUENT REVISIONS TO DECK FORMS MADE NECESSARY BY SUCH ACTION SHALL BE AT NO COST TO THE OWNER
- PLACEMENTS 2 AND 3 SHALL BE ACCOMPLISHED BY THE SIMULTANEOUS OPERATION OF TWO FINISHING MACHINES AND CREWS. A MINIMUM RATE OF 30 CUBIC YARDS PER HOUR SHALL BE MAINTAINED BY EACH MACHINE.
- THE CONCRETE DECK SLAB FOR THIS STRUCTURE SHALL BE PLACED ACCORDING TO THE POURING SEQUENCE SHOWN ON THE CONTRACT PLANS. REQUESTS FOR ANY ALTERNATE DECK POURING SEQUENCE SHALL BE SUBMITTED TO THE EIC. THE SUBMITTAL REQUIREMENTS ARE PROVIDED IN THE NYSDOT "PROCEDURE FOR APPROVAL OF ALTERNATE DECK POURING SEQUENCE ON CONTINUOUS BRIDGES" IN THE CONSTRUCTION INSPECTION MANUAL. NO RELATED WORK MAY BE PROGRESSD BY THE CONTRACTOR UNTIL THE WRITTEN APPROVAL OF THE ALTERNATE PROCEDURE IS RECEIVED FROM THE EIC. THE EIC WILL REVIEW THE REQUEST AND REPLY WITHIN FIFTEEN (15) WORK DAYS AFTER RECEIPT OF ALL THE REQUIRED SUBMITTAL DOCUMENTS FROM THE CONTRACTOR.
- THE CONTRACTOR SHALL PROVIDE TO THE EIC THE PROPOSED SET RETARDING WATER REDUCING ADMIXTURE (ASTM C494 TYPE D, SRWR) AND A COPY OF THE MANUFACTURER'S LITERATURE SPECIFYING THE RECOMMENDED RANGE TO PROVIDE SUFFICIENT RETARDATION. THIS SRWR DOSAGE SHALL NOT BE REDUCED AS THE PLACEMENT PROGRESSES. THE EIC WILL REJECT ANY CONCRETE DELIVERY THAT CONTAINS AN ADMIXTURE DOSAGE RATE BEYOND THE MANUFACTURER'S RECOMMENDED RANGE. ANY SUPPLIER CODES DENOTING SRWR SHALL BE GIVEN TO THE EIC FOR MONITORING PURPOSES.
- THE VALUES SHOWN IN THE CAMBER AND HAUNCH TABLES ARE BASED ON THE DECK PLACEMENT SEQUENCE SHOWN ON THE PLANS. IF THE DECK PLACEMENT SEQUENCE IS ALTERED, THE CAMBER AND HAUNCH TABLES NEED TO BE RECOMPUTED. THE CONTRACTOR SHALL HAVE A LICENSED AND REGISTERED NEW YORK STATE PROFESSIONAL ENGINEER RECOMPUTE AND SEAL AND SIGN THESE TABLES AND SUBMIT THEM TO THE EIC FOR APPROVAL.
- CONSTRUCTION JOINTS SHALL BE PLACED PARALLEL TO THE SKEW ANGLE. DECK CONCRETE SHALL BE PLACED SO THAT THE LEADING EDGE IS PARALLEL TO THE SKEW FINISHING MACHINE(S) SHALL BE OPERATED AS CLOSE TO THE SKEW ANGLE AS PRACTICABLE. TEXTURING MAY BE DONE LONGITUDINAL, TRANSVERSE OR PARALLEL TO THE ALIGNMENT OF THE FINISHING MACHINE.
- ALL AREAS SHOWN ON THE PLANS AS "PLACEMENT 1" SHALL BE PLACED DURING THE INITIAL CONTINUOUS WORK PERIOD. SUBSEQUENT PLACEMENTS (CONTINUOUS PLACEMENTS) WILL NOT BE PERMITTED UNTIL 72 HOURS OF ACCEPTABLE CURING AFTER THE COMPLETION OF THE PREVIOUS PLACEMENT HAS BEEN ACHIEVED.
- THE CONTRACTOR MAY DIVIDE PLACEMENT 2 AND/OR PLACEMENT 3 INTO SEPARATE SEGMENTS PROVIDED THE 72 HOUR WAITING PERIOD BETWEEN PLACEMENTS IS OBSERVED.

MULTI-ROTATIONAL BEARING NOTES:

- THE CONTRACTOR'S ATTENTION IS DIRECTED TO THE NET VERTICAL UPLIFT AT EACH ABUTMENT BEARING LOCATION FOR THE EXTREME LIMIT STATE. IT IS THE RESPONSIBILITY OF THE CONTRACTOR TO DESIGN THE BEARING ASSEMBLY FOR UPLIFT RESTRAINT.
- IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO VERIFY THE TOP OF PEDESTAL ELEVATIONS PRIOR TO CASTING NEW PEDESTALS AND INSTALLING THE NEW BEARINGS.

ENVIRONMENTAL NOTES:

- THE CONTRACTOR MUST FOLLOW AND COMPLY WITH APPROVED PERMITS ISSUED BY THE NYSDOC AND USACE AND REFER TO ALL EXHIBITS AND RELATED DOCUMENTATION.
- THE CONTRACTOR SHALL FOLLOW THE SOIL MANAGEMENT PLAN PREPARED FOR THE PROJECT SITE, AS APPLICABLE.
- THE CONTRACTOR IS RESPONSIBLE FOR THEIR CONSTRUCTION MEANS AND METHODS TO BE IMPLEMENTED AND SHALL OBTAIN OR MODIFY ANY APPLICABLE PERMIT.
- A STORMWATER POLLUTION PREVENTION PLAN (SWPPP) AND CONSTRUCTION SPILL PREVENTION CONTROL AND COUNTERMEASURE (SPCC) PLAN SHALL BE IMPLEMENTED BY THE CONTRACTOR TO AVOID POTENTIAL WATER QUALITY IMPACTS. THE CONTRACTOR SHALL BE RESPONSIBLE TO PREPARE AND SUBMIT TO THE REGULATORY AGENCIES A NOTICE OF INTENT (NOI) PRIOR TO THE START OF THE CONSTRUCTION PHASE. A NOTICE OF TERMINATION (NOT) SHALL BE PREPARED AND SUBMITTED BY THE CONTRACTOR WITH THE COMPLETION OF THE CONSTRUCTION ACTIVITIES.
- BEST MANAGEMENT PRACTICES SHALL BE DEVELOPED, IMPLEMENTED, ADJUSTED AND MAINTAINED AT ALL TIMES DURING CONSTRUCTION, INCLUDING TURBIDITY CONTROLS TO PREVENT SILTATION TO NORMANS KILL AND HUDSON RIVER.
- NO FILL OR EXCAVATION IS PROPOSED BELOW THE MEAN HIGHER HIGH WATER LINE (MHHW) OR WITHIN THE FLOODWAY. ANY FILL/EXCAVATION BELOW THE MHHW LINE OR ADDITIONAL WETLAND IMPACTS WILL REQUIRE AUTHORIZATION FROM STATE AND FEDERAL AGENCIES AND REQUIRED PERMITS AND AMENDMENTS SHALL BE THE CONTRACTOR'S RESPONSIBILITY.
- THE CONTRACTOR SHALL REVIEW ENVIRONMENTAL REQUIREMENTS OF ANY PROPOSED STAGING AREAS WITH THE ALBANY PORT DISTRICT COMMISSION (APDC) REPRESENTATIVE OR PROJECT MANAGER AT LEAST SEVENTY-TWO (72) HOURS PRIOR TO USE.
- ANY MATERIAL TO BE STOCKPILED FOR PERIODS GREATER THAN 24 HOURS SHALL BE PROTECTED BY APPROPRIATE EROSION CONTROL DEVICES.
- ALL TREES ARE TO REMAIN UNLESS OTHERWISE DESIGNATED ON THE PLANS
- CONTRACTOR SHALL NOT STAGE OR OPERATE EQUIPMENT WITHIN THE DRIPLINE OF TREES, UNLESS AUTHORIZED.
- SINCE IT IS OFTEN DIFFICULT FOR CONSTRUCTION PERSONNEL TO CORRECTLY IDENTIFY PROTECTED SPECIES, THE CONTRACTOR SHALL BE DIRECTED NOT TO HARM, HARASS OR KILL ANY ANIMAL SPECIES ENCOUNTERED DURING PROJECT CONSTRUCTION. IF SUSPICIOUS PROTECTED SPECIES ARE ENCOUNTERED AND MAY BE COMPROMISED BY CONSTRUCTION ACTIVITIES, THE CONTRACTOR SHALL CONTACT THE APDC PROJECT MANAGER WITHIN 24 HOURS.
- A TIME OF YEAR RESTRICTION SHALL BE IMPLEMENTED, AND TREE REMOVAL SHALL ONLY BE PERFORMED BETWEEN NOVEMBER 1ST TO MARCH 31ST TO PREVENT POTENTIAL IMPACTS TO THE NORTHERN LONG-EARED BAT (NLEB), AS PER GUIDELINES FROM UNITED STATES FISH & WILDLIFE SERVICES (USFWS). ADDITIONALLY, SILT FENCE OR CONSTRUCTION SAFETY FENCE SHALL BE INSTALLED AS NECESSARY TO IDENTIFY TREE CLEARING LIMITS. TREE CLEARING SHALL NOT BE PERFORMED AFTER DUSK.
- DRIVING OF PILES OR SHEET PILES SHALL BE DISCARDED, AND VIBRATORY OR ROTARY METHODS SHALL BE USED.
- TO ENSURE THE U.S. COAST GUARD (USCG) IS AWARE OF THE CONSTRUCTION ACTIVITY ON THE WATERWAY, THE CONTRACTOR SHALL CONTACT USCG OFFICE AT LEAST 30 DAYS PRIOR TO COMMENCEMENT OF ANY WORK FOR REVIEW OF CONSTRUCTION PLANS AND DETERMINATION OF OTHER REQUIREMENTS FOR WORK IN THE AREA.
- IF THE CONSTRUCTION MEANS AND METHOD REQUIRE WATERBORNE EQUIPMENT, WATERWAY CLOSURES/RESTRICTIONS OR SAFETY ZONES ESTABLISHED, REQUESTS SHALL BE MADE BY THE CONTRACTOR A MINIMUM OF 90 DAYS IN ADVANCE. CONTACT MR. JEFFREY YUNKER, USCG NEW YORK, VIA E-MAIL AT JEFFREY.M.YUNKER@USCG.MIL, OR BY PHONE AT 718-354-4195.
- ANY SPILLAGE OF OIL OR OIL-BASED PRODUCTS DURING CONSTRUCTION MUST BE PROMPTLY REPORTED TO THE USCG BY CALLING 1-800-424-8802, AND ANY OTHER AGENCY AS PER CONSTRUCTION SPCC PLAN FROM THE CONTRACTOR.
- INSTALLATION AND MAINTENANCE OF EROSION AND SEDIMENT CONTROLS IS THE RESPONSIBILITY OF THE CONTRACTOR THROUGHOUT THE DURATION OF CONSTRUCTION ACTIVITIES AND ANY SUBSEQUENT SOIL DISTURBANCE ACTIVITIES NEAR DRAINAGE AND WETLAND AREAS.
- THE CONTRACTOR SHALL USE APPROPRIATE DUST CONTROL METHODS DURING CONSTRUCTION ACTIVITIES, SUCH AS WATER SPRAYS.
- THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE STABILIZATION OF EXPOSED SOILS FOLLOWING COMPLETION OF CONSTRUCTION ACTIVITIES IN DESIGNATED AREAS.
- LAND CLEARING AND GRUBBING SHALL BE PERFORMED IN SUCH A MANNER AS TO MINIMIZE DAMAGE OUTSIDE THE PROJECT FOOTPRINT.
- THE CONTRACTOR SHALL DISPOSE OF DEBRIS AND SOLID WASTE GENERATED BY THE PROJECT ACCORDING TO APPLICABLE FEDERAL, STATE, AND LOCAL REGULATIONS.
- THE CONTRACTOR SHALL STAGE AND SERVICE CONSTRUCTION EQUIPMENT IN DESIGNATED UPLAND AREAS.
- THE CONTRACTOR SHALL PERFORM CONSTRUCTION VEHICLE MAINTENANCE AND INSPECTIONS TO REDUCE THE POTENTIAL FOR INCIDENTAL RELEASE OF VEHICLE FLUIDS.
- THE CONTRACTOR SHALL MAINTAIN SPILL KITS TO RAPIDLY RESPOND TO AND LIMIT IMPACTS FROM ACCIDENTAL RELEASES OF VEHICLE FLUIDS.



McFarland Johnson

60 RAILROAD PLACE
SUITE 402
SARATOGA SPRINGS, NEW YORK 12866
P: 518-580-9380 F: 518-580-9383
SaratogaROM@mjinco.com

PROJECT MILESTONE

GMP PLANS

| NO. | DATE | DESCRIPTION |
|-----|----------|-----------------------|
| 1 | 06/08/22 | GMP BID SET |
| 2 | 10/28/22 | GMP BID SET REVISIONS |
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CLIENT:
ALBANY PORT DISTRICT COMMISSION

PROJECT:
**ALBANY, NEW YORK
PORT OF ALBANY SITE INFRASTRUCTURE
IMPROVEMENTS**

| | |
|----------|----------|
| DRAWN | SLM |
| DESIGNED | DRW |
| CHECKED | LAS |
| SCALE | NO SCALE |
| DATE | 6/8/2022 |
| PROJECT | 18641.00 |



IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECT DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.

DRAWING TITLE

GENERAL NOTES

DRAWING NUMBER

ST-06

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McFarland Johnson

60 RAILROAD PLACE
SUITE 402
SARATOGA SPRINGS, NEW YORK 12866
P: 518-580-9380 F: 518-580-9383
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GMP PLANS

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CLIENT:
ALBANY PORT DISTRICT COMMISSION

ALBANY, NEW YORK

PROJECT:
PORT OF ALBANY SITE INFRASTRUCTURE IMPROVEMENTS

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| DRAWN | SLM |
| DESIGNED | JLR |
| CHECKED | RLJ |
| SCALE | AS SHOWN |
| DATE | 6/8/2022 |
| PROJECT | 18641.00 |



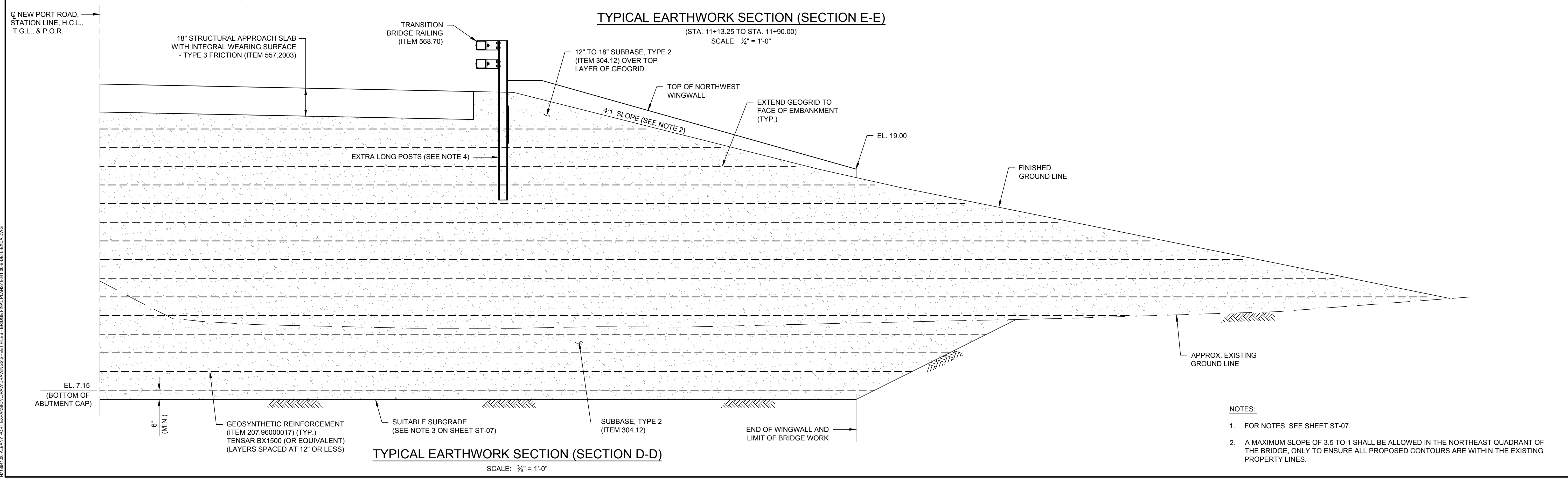
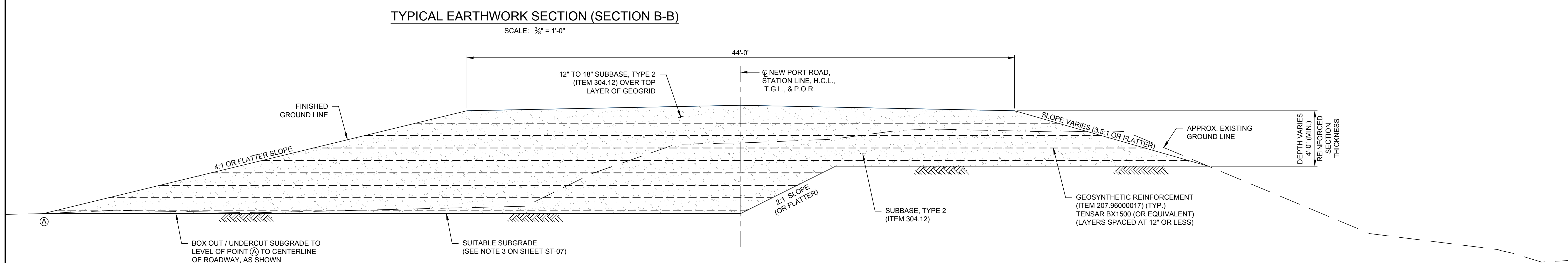
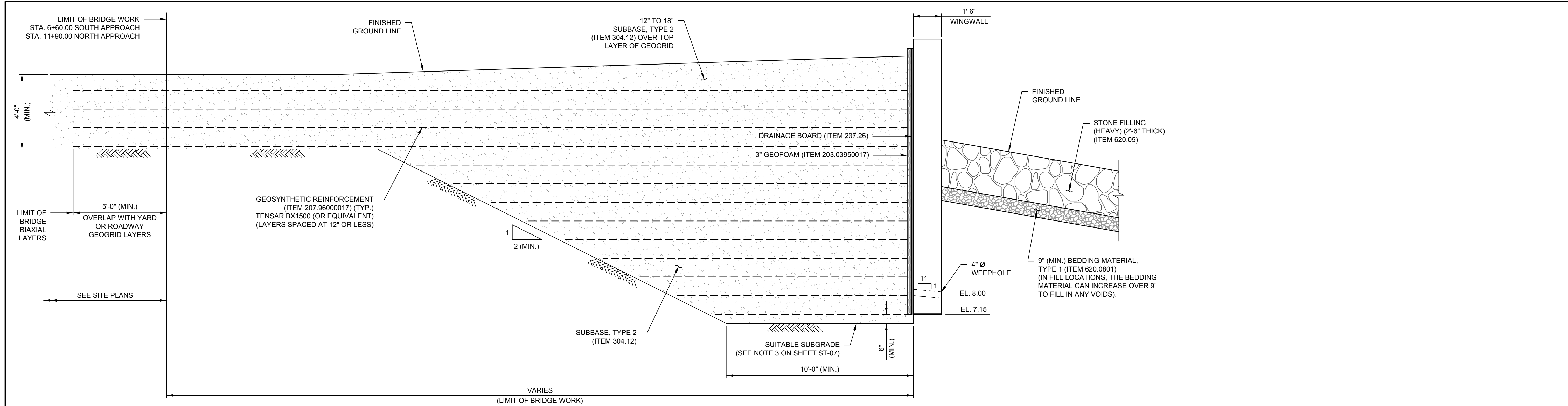
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DRAWING TITLE
EXCAVATION AND EMBANKMENT DETAILS (2 of 2)

DRAWING NUMBER

ST-08

8 OF 35



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McFarland Johnson
 60 RAILROAD PLACE
 SUITE 402
 SARATOGA SPRINGS, NEW YORK 12866
 P: 518-580-9380 F: 518-580-9383
 SaratogaROM@mjinc.com

PROJECT MILESTONE
GMP PLANS

| NO. | DATE | DESCRIPTION |
|-----|----------|-----------------------|
| 1 | 06/08/22 | GMP BID SET |
| 2 | 10/28/22 | GMP BID SET REVISIONS |
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CLIENT: **ALBANY PORT DISTRICT COMMISSION**
 ALBANY, NEW YORK
 PROJECT: **PORT OF ALBANY SITE INFRASTRUCTURE IMPROVEMENTS**

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|----------|----------|
| DRAWN | SLM |
| DESIGNED | CLG |
| CHECKED | SIW |
| SCALE | AS SHOWN |
| DATE | 6/8/2022 |
| PROJECT | 18641.00 |



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DRAWING TITLE
DRILLED SHAFT DETAILS (ABUTMENTS AND PIERS)

DRAWING NUMBER

ST-09

| DRILLED SHAFT NO. | ASSUMED TOP OF SOUND ROCK ELEVATION | MINIMUM ROCK SOCKET DEPTH | ESTIMATED TIP ELEVATION | ACTUAL DRILLED SHAFT LENGTH BELOW CUT OFF |
|-------------------|-------------------------------------|---------------------------|-------------------------|---|
| 1 | -70.0± | 15.0 | -85.0± | |
| 2 | -70.0± | 15.0 | -85.0± | |
| 3 | -70.0± | 15.0 | -85.0± | |
| 4 | -87.0± | 15.0 | -102.0± | |
| 5 | -87.0± | 15.0 | -102.0± | |
| 6 | -87.0± | 15.0 | -102.0± | |
| 7 | -87.0± | 15.0 | -102.0± | |
| 8 | -87.0± | 15.0 | -102.0± | |
| 9 | -87.0± | 15.0 | -102.0± | |
| 10 | -87.0± | 15.0 | -102.0± | |
| 11 | -87.0± | 15.0 | -102.0± | |
| 12 | -87.0± | 15.0 | -102.0± | |
| 13 | -87.0± | 15.0 | -102.0± | |
| 14 | -87.0± | 15.0 | -102.0± | |

TRIAL DRILLED SHAFT NOTES:

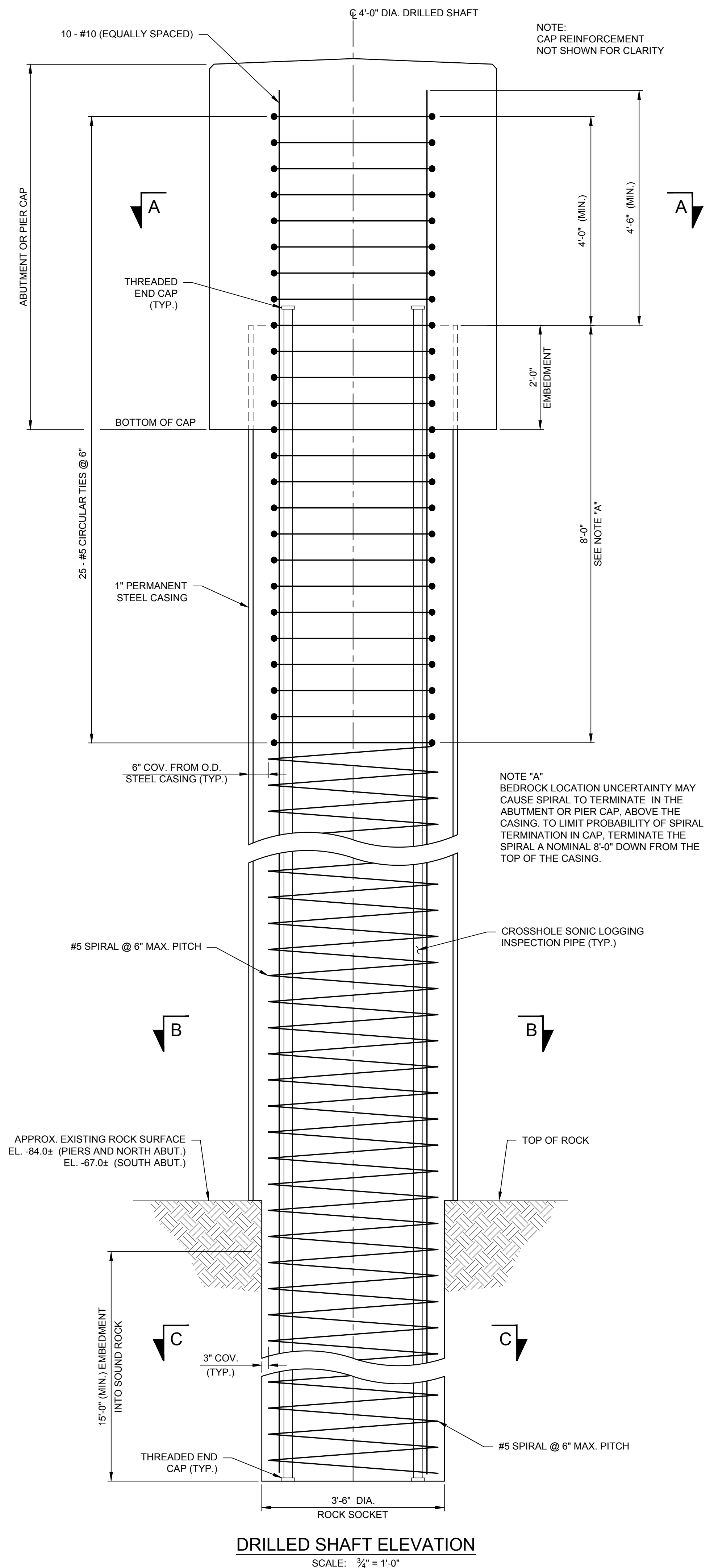
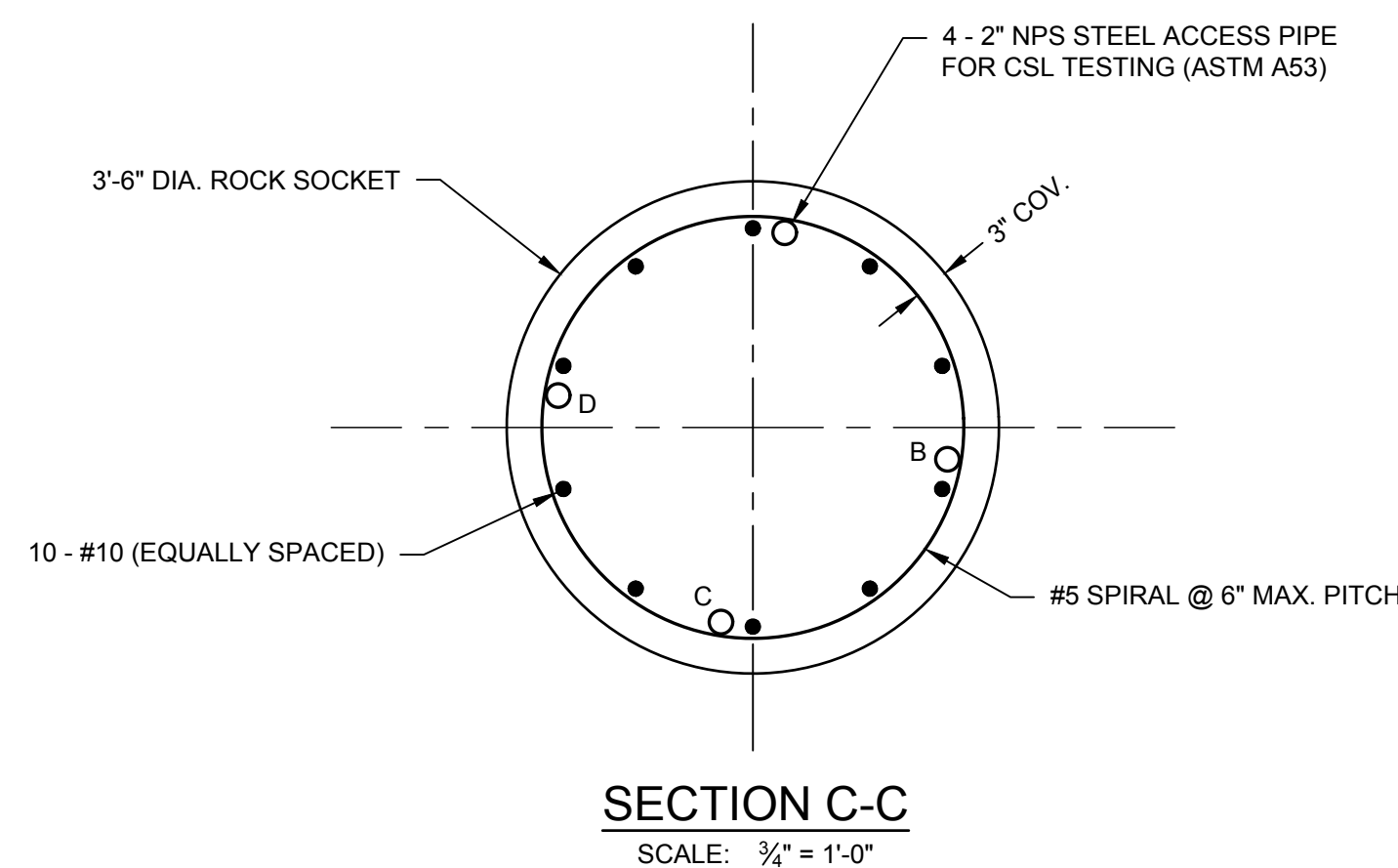
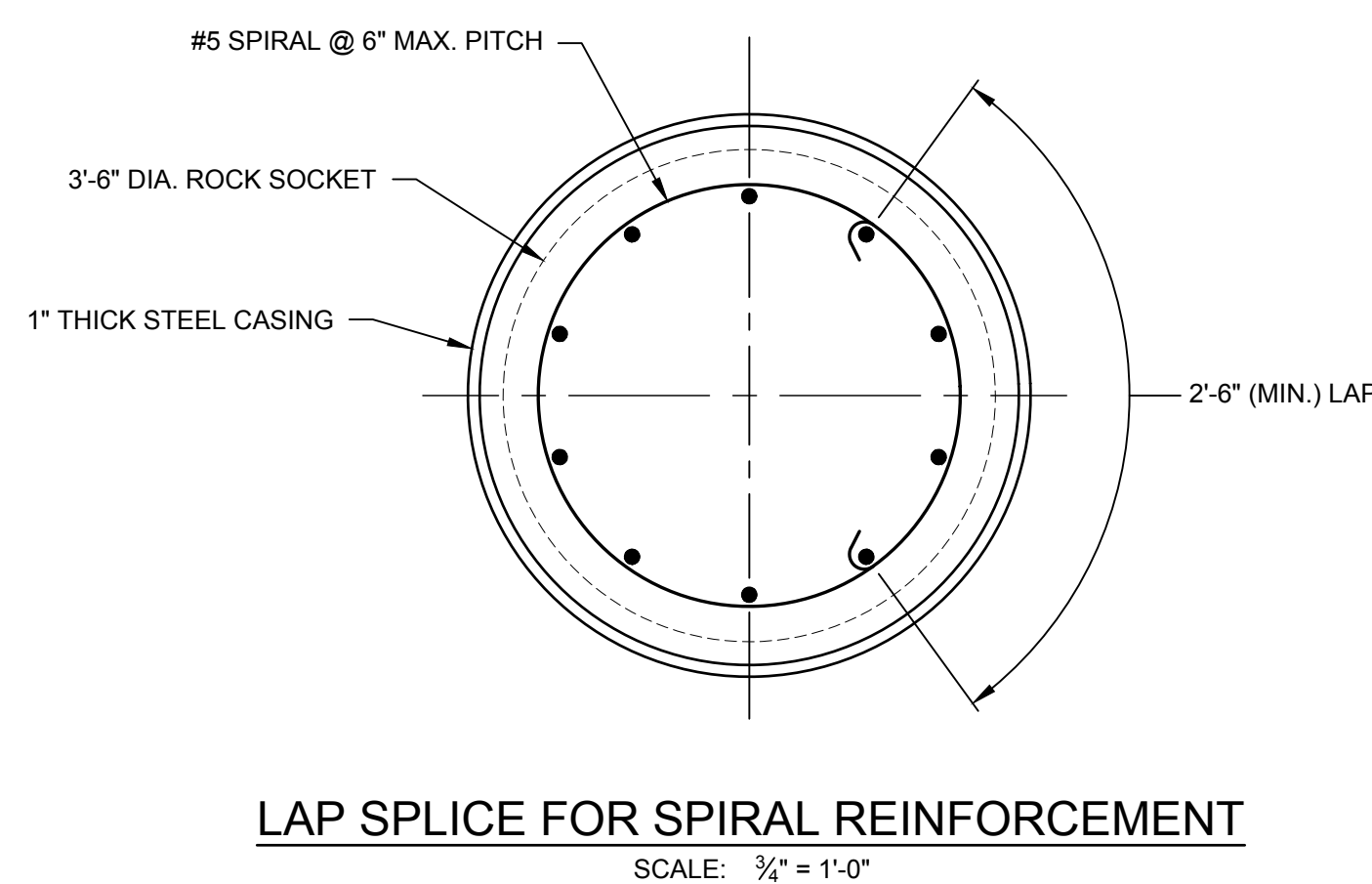
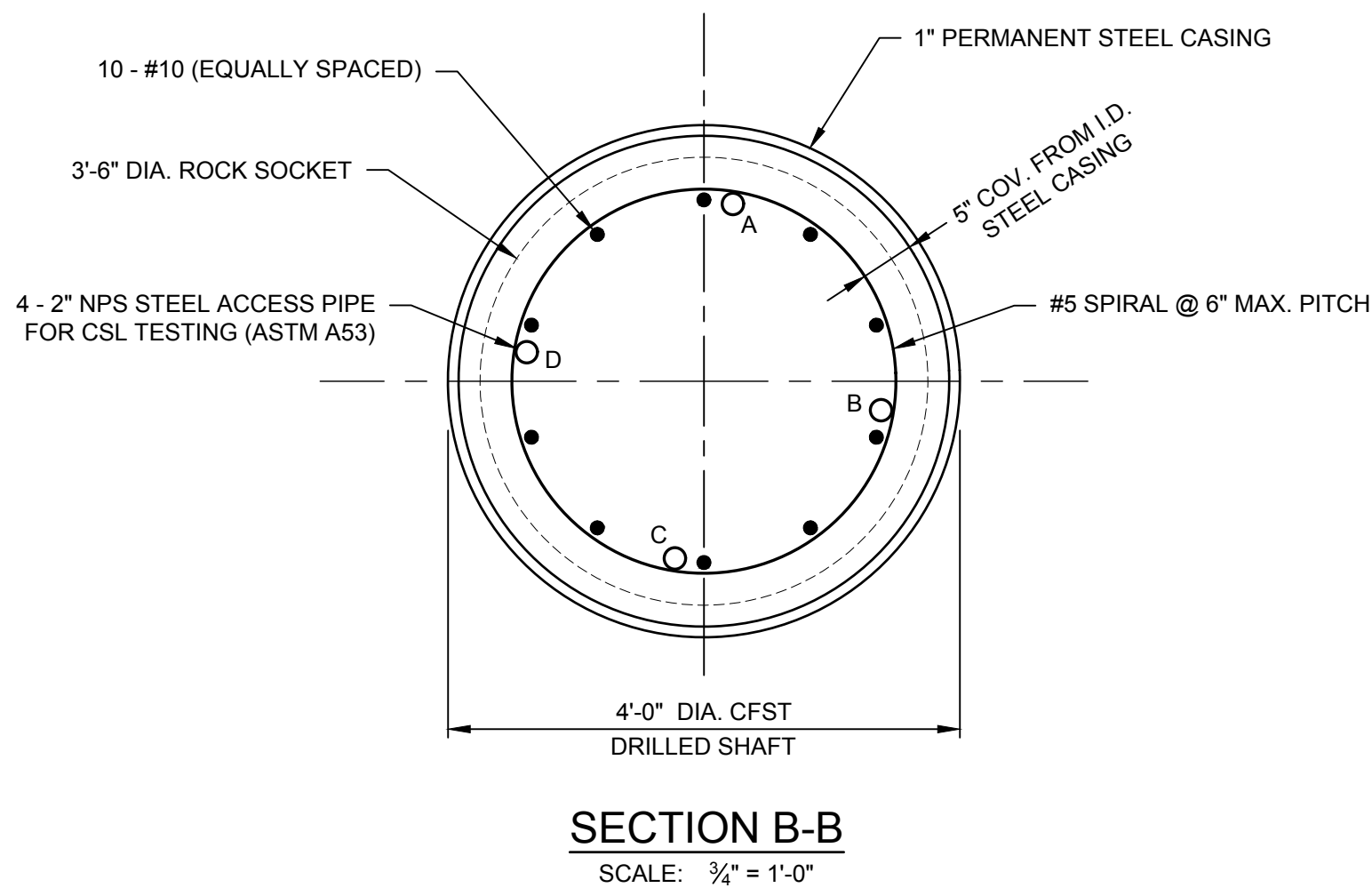
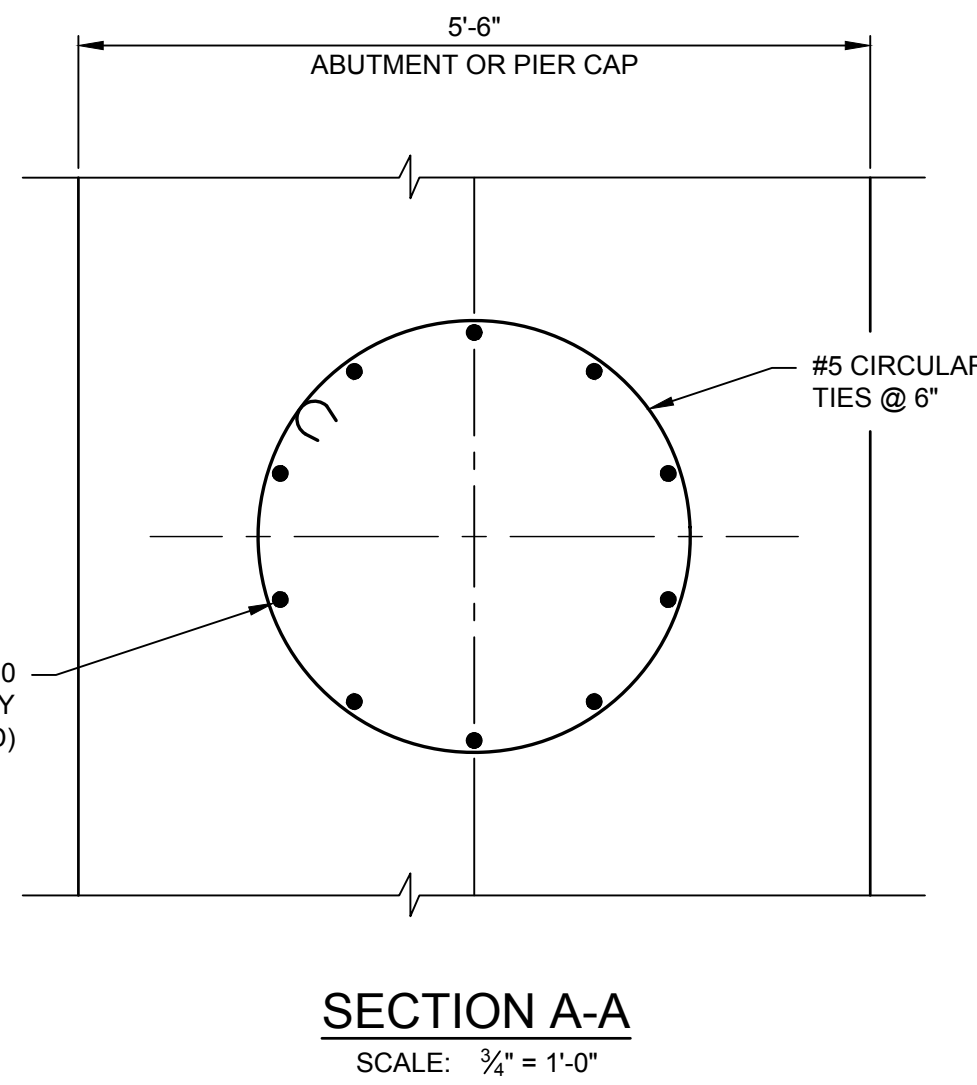
- THE CONTRACTOR MAY ELECT TO INSTALL A TRIAL DRILLED SHAFT FOR THE PURPOSES OF LOAD TESTING TO CONFIRM OR OPTIMIZE THE DESIGN PARAMETERS RECOMMENDED IN THE GEOTECHNICAL REPORT. THE TRIAL DRILLED SHAFT SHALL BE INCLUDED UNDER ITEM 551.52.
- THE TRIAL DRILLED SHAFT SHALL BE LOCATED ON THE ALIGNMENT OF THE BRIDGE, BETWEEN THE ABUTMENTS. THE CONTRACTOR SHALL COORDINATE WITH THE GEOTECHNICAL ENGINEER TO DETERMINE A SUITABLE LOCATION.
- THE TRIAL DRILLED SHAFT SHALL BE INSTALLED IN THE SAME MANNER AND USING THE SAME EQUIPMENT WHICH WILL BE USED TO INSTALL THE REMAINING FOUNDATION SHAFTS.
- THE TRIAL DRILLED SHAFT SHALL BE TESTED TO AT LEAST 200 PERCENT OF THE DESIGN LOAD USING AN OSTERBERG LOAD CELL OR OTHER SUITABLE TEST METHOD. ALL WORK FOR THE OSTERBERG LOAD CELL TESTS SHALL CONFORM TO THE REQUIREMENTS OF THE SPECIAL PROVISION FOR ITEM 551.50230017.
- A RESISTANCE FACTOR OF UP TO 0.70 CAN BE APPLIED TO THE NOMINAL RESISTANCE ESTABLISHED BY THE LOAD TEST TO DETERMINE THE IN-PLACE FACTORED AXIAL RESISTANCE.

DRILLED SHAFT NOTES:

- AFTER COMPLETION OF THE SHAFT INSTALLATION, THE ENGINEER WILL COMPLETE THE "ACTUAL DRILLED SHAFT LENGTH" TABLE FOR INCLUSION IN THE AS-BUILT PLANS.
- DRILLED SHAFT CONCRETE SHALL MEET THE REQUIREMENTS OF CLASS GG CONCRETE WITH A COMPRESSIVE STRENGTH OF 5000 PSI AT 28 DAYS.
- PERMANENT STEEL CASING, CONCRETE AND REINFORCEMENT IN DRILLED SHAFTS SHALL BE INCLUDED UNDER ITEM 551.50040.
- INSPECTION (ACCESS) PIPE AND GROUTING SHALL BE INCLUDED UNDER ITEM 551.56. THE BOTTOM OF THE PIPES SHALL BE LOCATED A MAXIMUM OF 2" ABOVE THE BOTTOM OF REINFORCEMENT CAGE.
- ESTIMATED TOP OF SOUND ROCK ELEVATION IS PROVIDED IN THE DRILLED SHAFT TABLE. ACTUAL SOUND ROCK ELEVATION TO BE FIELD VERIFIED BY THE ENGINEERING GEOLOGIST.
- AN ENGINEERING GEOLOGIST SHALL BE PRESENT ON SITE TO ASCERTAIN THE TOP OF SOUND ROCK DURING DRILLING AND THE FINAL DEPTH OF THE ROCK SOCKET.
- ALL SHAFTS WILL BE TESTED TO VERIFY CONCRETE INTEGRITY IN ACCORDANCE WITH ITEM 551.56. CROSSHOLE SONIC LOGGING (CSL) OF DRILLED SHAFTS, PERFORM TESTING BETWEEN ACCESS PIPE PAIRS A-B, B-C, C-D, D-A, A-C AND B-D.
- CONDUCT CSL TESTING WITHIN 3 TO 45 DAYS AFTER CONCRETING EACH SHAFT.
- EQUIPMENT REQUIRED TO INSTALL THE SHAFTS SHALL BE MOBILIZED UNDER ITEM 551.53, FURNISHING EQUIPMENT FOR INSTALLING DRILLED SHAFTS.
- DRILLED SHAFT CONCRETE SHALL BE PLACED IN ONE CONTINUOUS POUR WITHOUT THE USE OF CONSTRUCTION JOINTS.
- STEEL CASINGS SHALL CONFORM TO ASTM 252, GRADE 3 (45 KSI).
- SPIRAL REINFORCEMENT MAY BE SPLICED WITH MECHANICAL CONNECTORS IN LIEU OF A LAP SPLICE. ALL SPIRAL REINFORCEMENT WITH LAP SPLICES OR MECHANICAL CONNECTORS SHALL CONFORM TO ASTM A706 OR A615 GRADE 60. THE MINIMUM LAP LENGTH FOR THIS REINFORCEMENT SHALL BE 2'-6". SPIRAL ENDS SHALL BE ANCHORED WITH 1.5 EXTRA TURNS OF SPIRAL BAR.
- EXPOSED AREAS OF CFST DRILLED SHAFTS SHALL BE COATED WITH COAL TAR EPOXY-POLYIMIDE. LIMITS OF COATING SHALL BE FROM BOTTOM OF CAP TO 4'-0" BELOW FINISHED GROUND.
- ALL DRILLED SHAFT VERTICAL REINFORCEMENT SHALL CONFORM TO ASTM A615 GRADE 60. IF REINFORCEMENT SPLICES ARE NECESSARY THEY SHALL BE EITHER A 5'-2" LAP SPLICE OR SPLICED WITH MECHANICAL CONNECTORS. LAP SPLICES ARE PROHIBITED WITHIN THE TOP AND BOTTOM 6'-0" OF THE DRILLED SHAFT, MEASURING FROM THE BOTTOM OF CAP AND TOP OF ROCK. ALL SPLICES SHALL BE STAGGERED WITH A 2'-0" MINIMUM CLEAR DISTANCE BETWEEN SPLICES OF ADJACENT BARS.
- FOR DESIGN PURPOSES, THE MAXIMUM AXIAL LOAD PER DRILLED SHAFT IS:

PIER:
 SERVICE LIMIT STATE: 1180 KIPS
 STRENGTH LIMIT STATE: 1540 KIPS
 EXTREME EVENT LIMIT STATE: 1180 KIPS

ABUTMENT:
 SERVICE LIMIT STATE: 1075 KIPS
 STRENGTH LIMIT STATE: 1250 KIPS
 EXTREME EVENT LIMIT STATE: 850 KIPS





McFarland Johnson

60 RAILROAD PLACE
SUITE 402
SARATOGA SPRINGS, NEW YORK 12866
P: 518-580-9380 F: 518-580-9383
SaratogaROM@mjinc.com

PROJECT MILESTONE

GMP PLANS

| NO. | DATE | DESCRIPTION |
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CLIENT:
ALBANY PORT DISTRICT COMMISSION

ALBANY, NEW YORK

PROJECT:
PORT OF ALBANY SITE INFRASTRUCTURE IMPROVEMENTS

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|----------|----------|
| DRAWN | SLM |
| DESIGNED | CLG |
| CHECKED | RLJ |
| SCALE | AS SHOWN |
| DATE | 6/8/2022 |
| PROJECT | 18641.00 |



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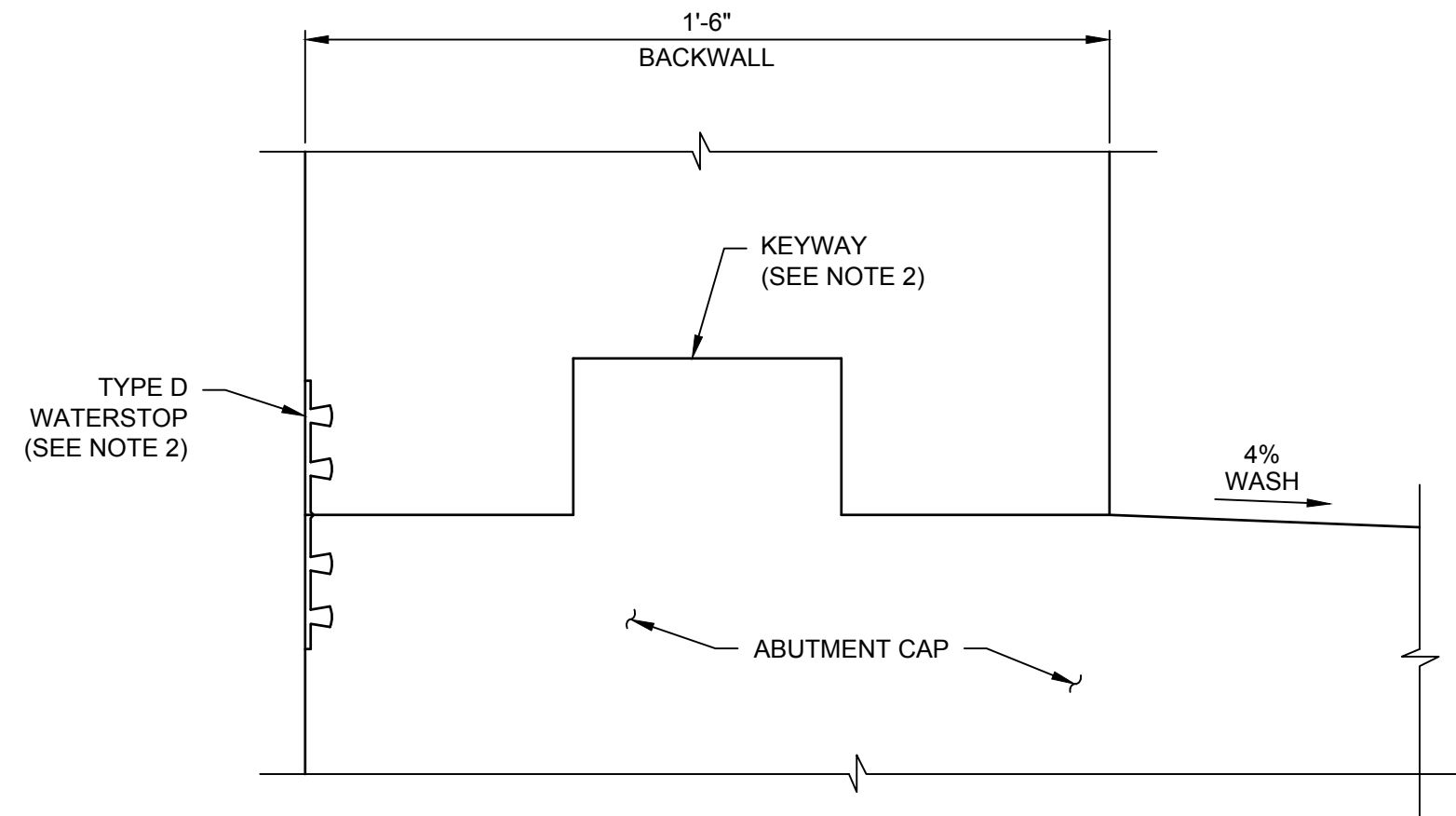
DRAWING TITLE

**ABUTMENT DETAILS
(2 OF 2)**

DRAWING NUMBER

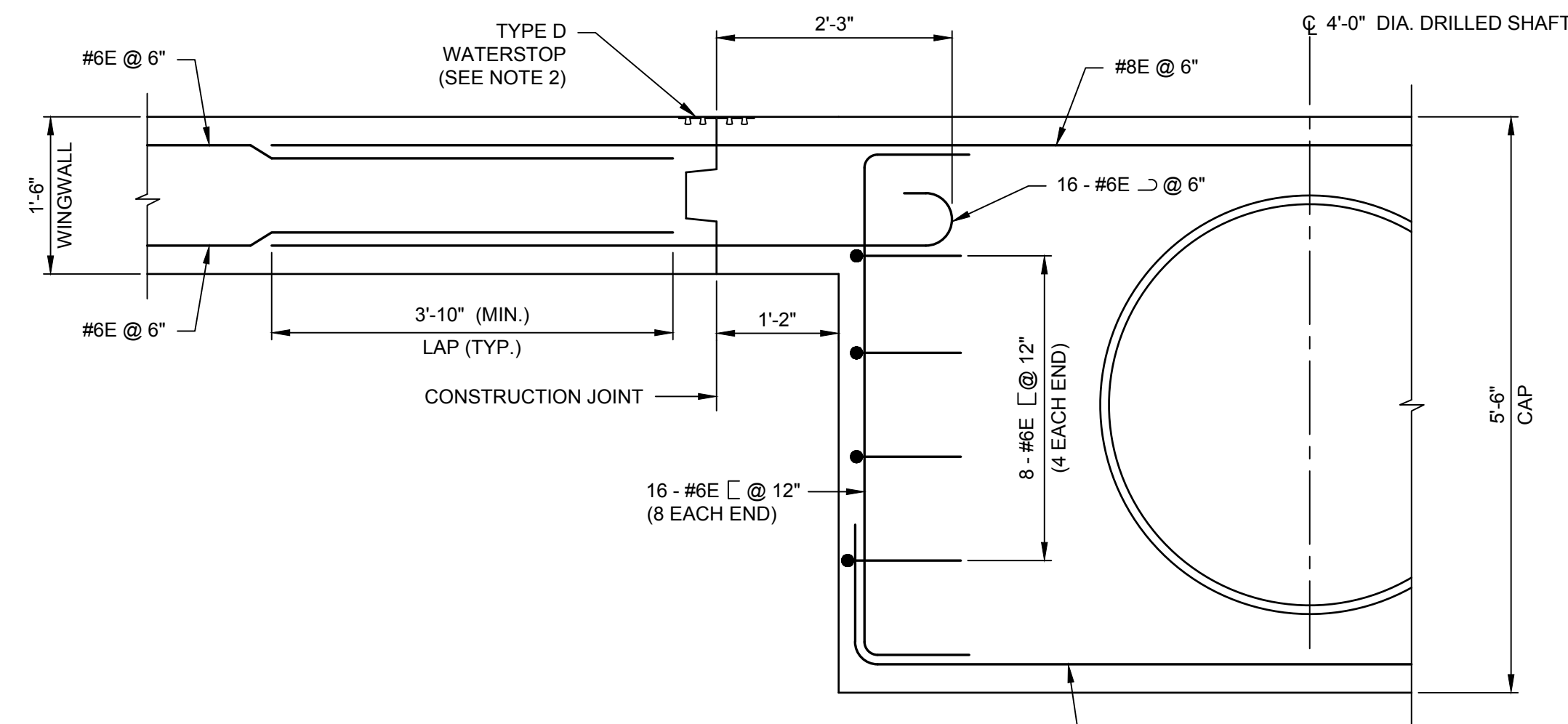
ST-13

13 OF 35



**CONSTRUCTION JOINT DETAIL
AT ABUTMENT CAP AND BACKWALL**

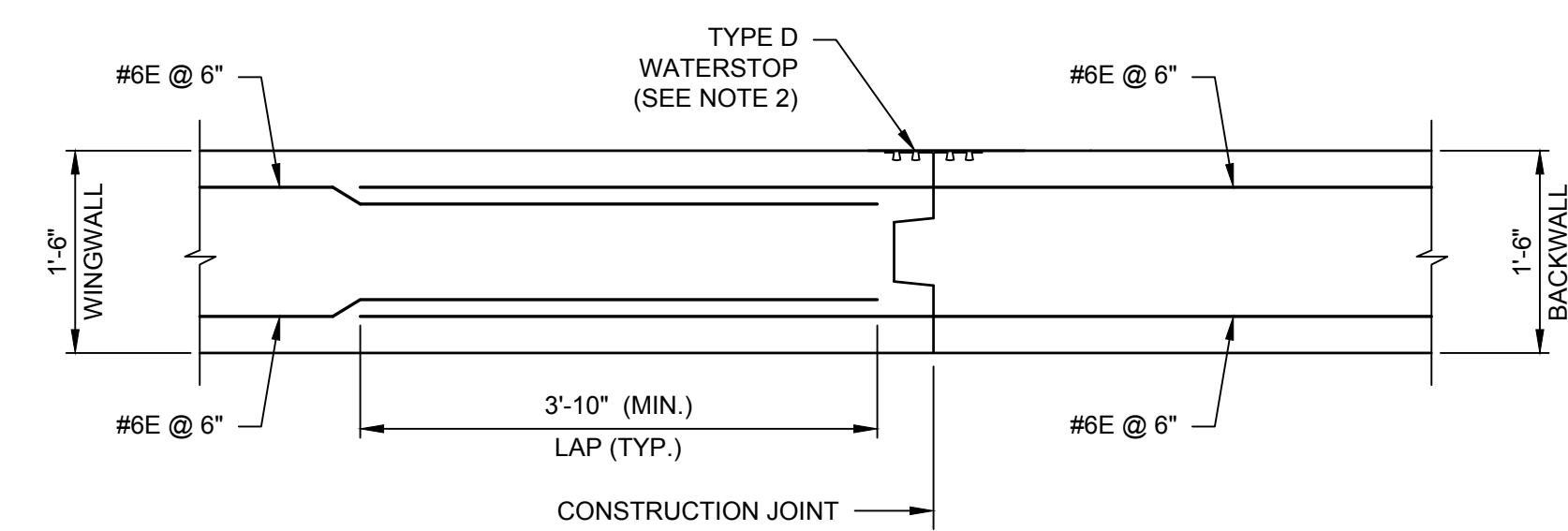
SCALE: 3" = 1'-0"



SECTION D-D

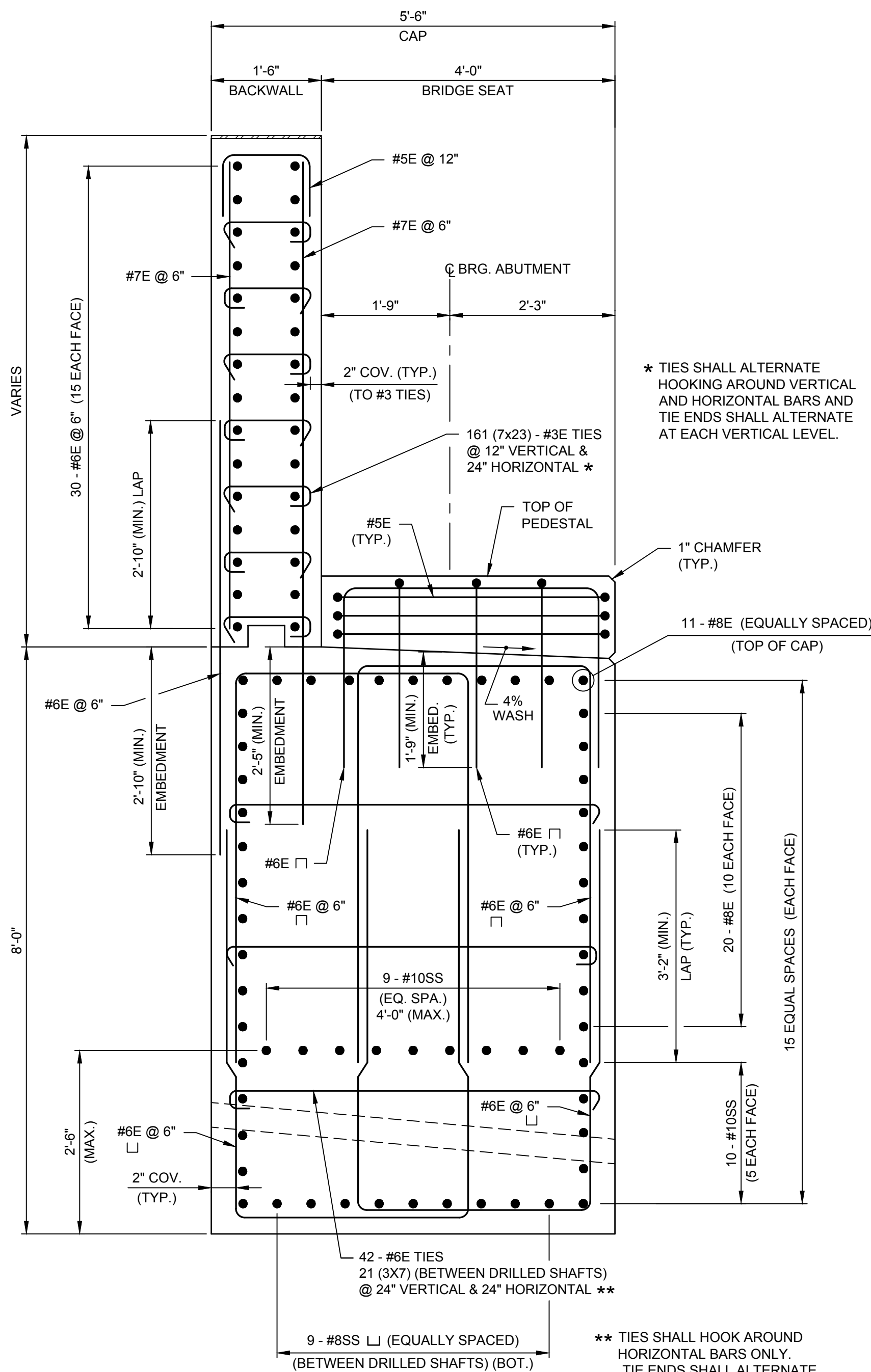
SCALE: 3/4" = 1'-0"

NOTE "A"
ALL LONGITUDINAL BARS SHALL BE BENT 90° AT ENDS (EXCEPT FOR THE BARS THAT ARE LAPPING TO WINGWALL BARS).



SECTION E-E

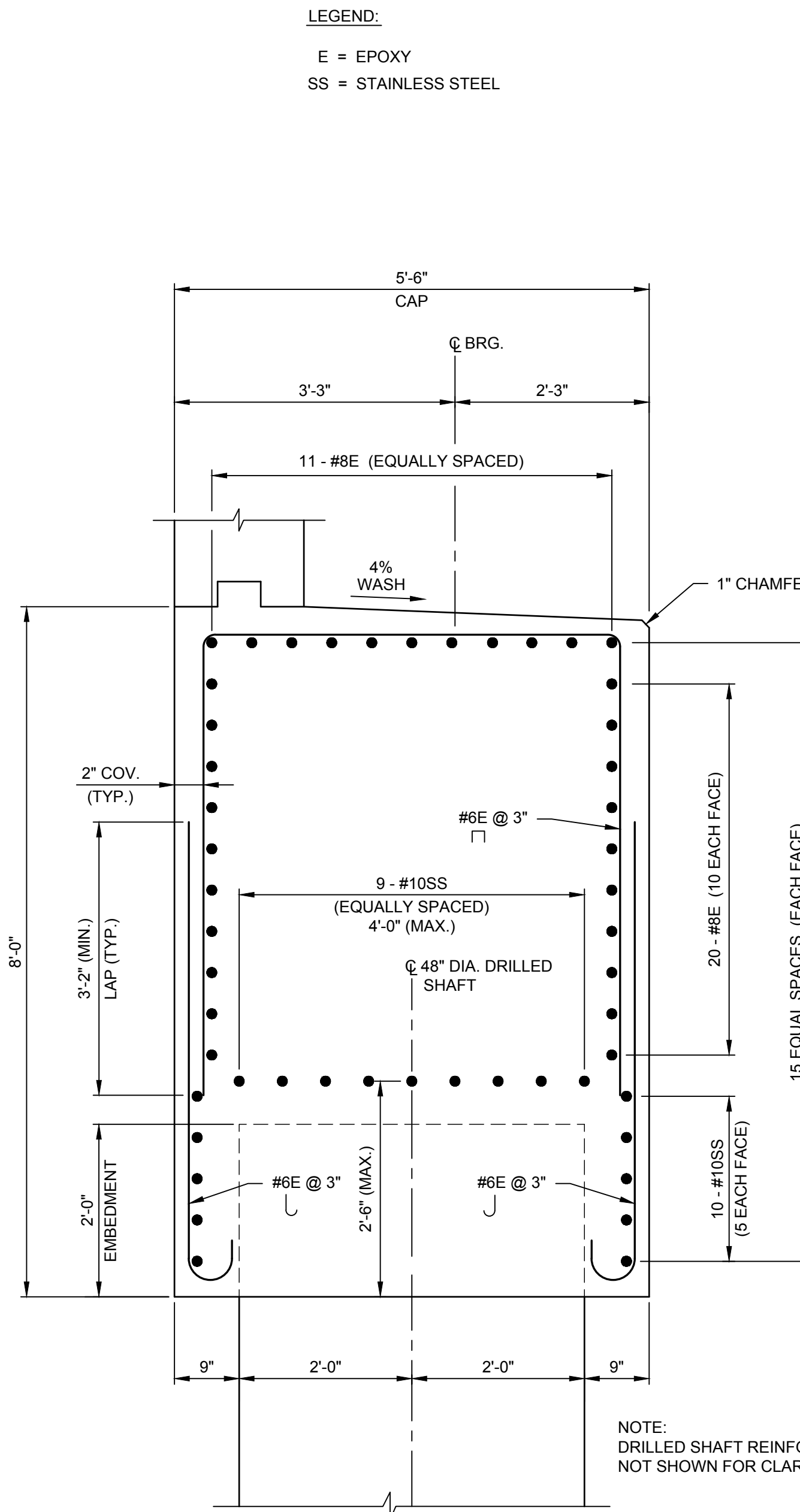
SCALE: 3/4" = 1'-0"



SECTION A-A

SCALE: 3/4" = 1'-0"

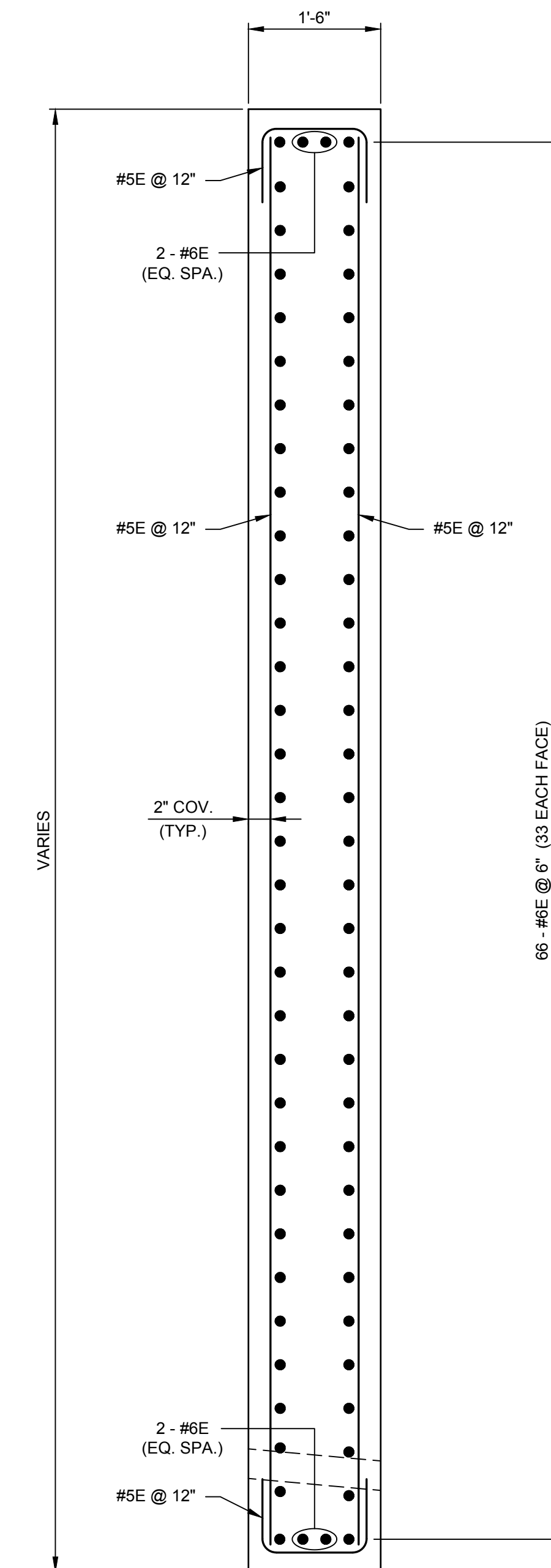
** TIES SHALL HOOK AROUND HORIZONTAL BARS ONLY. TIE ENDS SHALL ALTERNATE AT EACH VERTICAL LEVEL.



SECTION B-B

SCALE: 3/4" = 1'-0"

NOTE:
DRILLED SHAFT REINFORCEMENT NOT SHOWN FOR CLARITY



SECTION C-C

SCALE: 3/4" = 1'-0"

NOTES:

- FOR LOCATION OF SECTIONS A-A, B-B, C-C, D-D, AND E-E, SEE SHEET ST-12.
- FOR KEYWAY DETAILS AND TYPE D WATERSTOP, SEE SHEET ST-29.



McFarland Johnson
 60 RAILROAD PLACE
 SUITE 402
 SARATOGA SPRINGS, NEW YORK 12866
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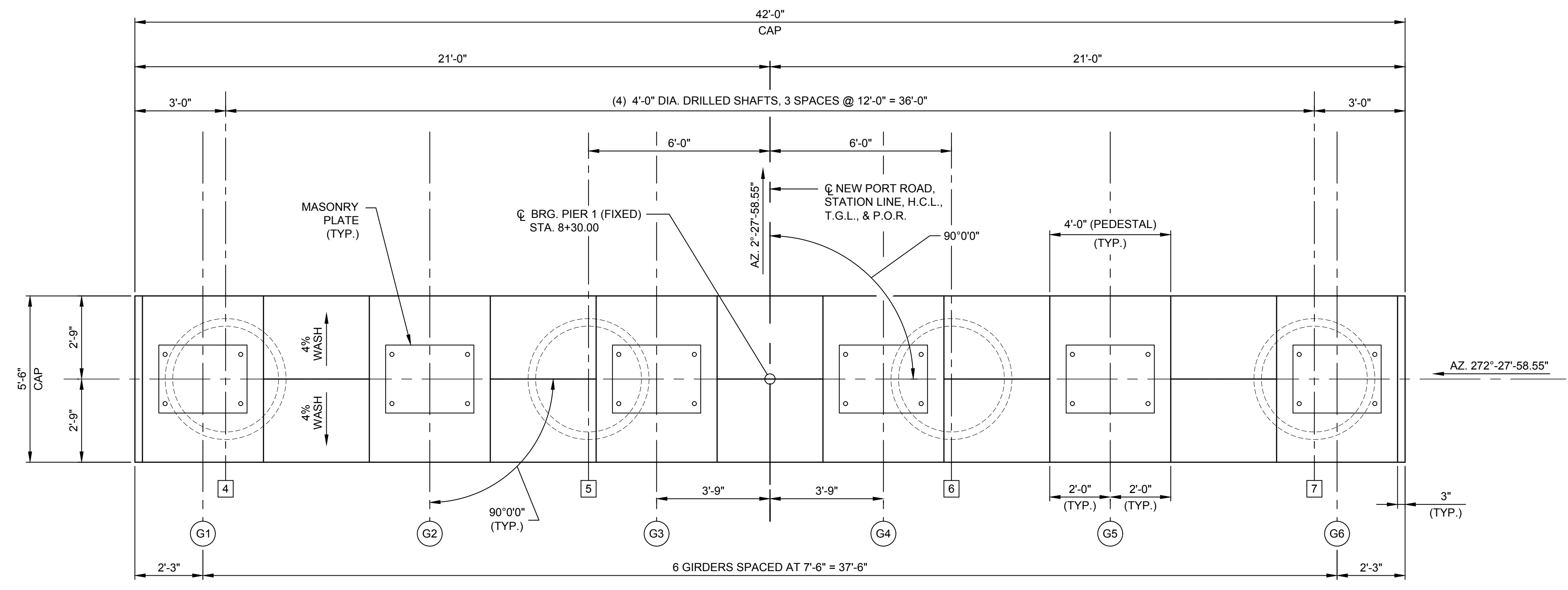
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| DRAWN | SLM |
| DESIGNED | CLG |
| CHECKED | SIW |
| SCALE | AS SHOWN |
| DATE | 6/8/2022 |
| PROJECT | 18641.00 |



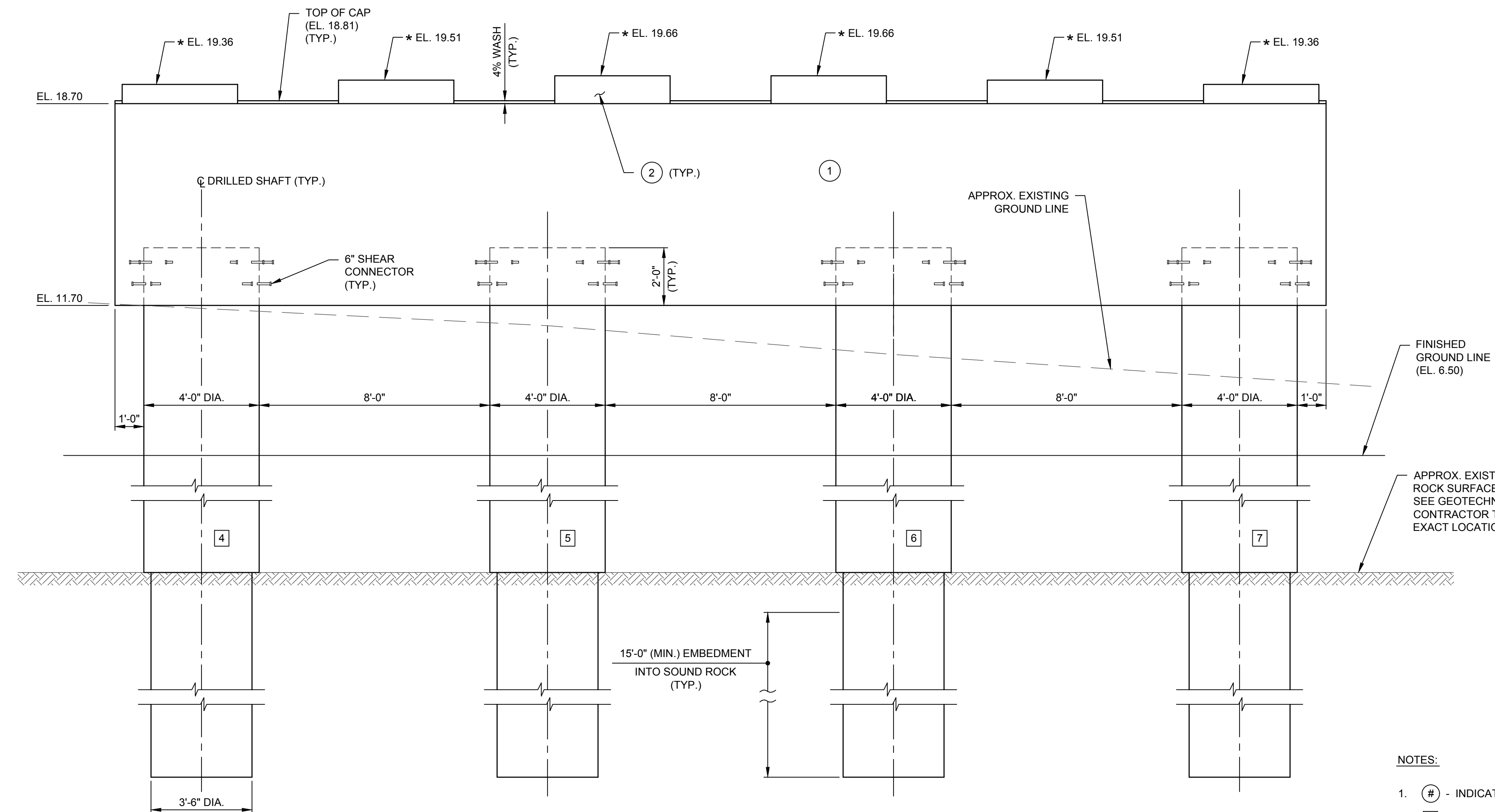
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DRAWING TITLE
PIER 1 PLAN AND ELEVATION

DRAWING NUMBER
ST-14



PIER 1 PLAN
 SCALE: 3/8" = 1'-0"



PIER 1 ELEVATION
 SCALE: 3/8" = 1'-0"

- NOTES:**
1. (#) - INDICATES CONCRETE PLACEMENT NUMBER.
 2. (#) - INDICATES DRILLED SHAFT NUMBER.
 3. * ELEVATIONS MAY HAVE TO BE ADJUSTED TO ACCOMMODATE THE ACTUAL BEARINGS FURNISHED. IT IS THE RESPONSIBILITY OF THE CONTRACTOR TO COORDINATE ANY CHANGES IN THE BEARINGS WHICH MAY AFFECT THE PEDESTAL ELEVATIONS OR DIMENSIONS
 4. THE 28 DAY COMPRESSIVE STRENGTH OF THE CONCRETE PIER CAP SHALL BE 5000 PSI.

N:\18641\05 ALBANY PORT EXPANSION\DRAWINGS\DWG\PIER1.DWG



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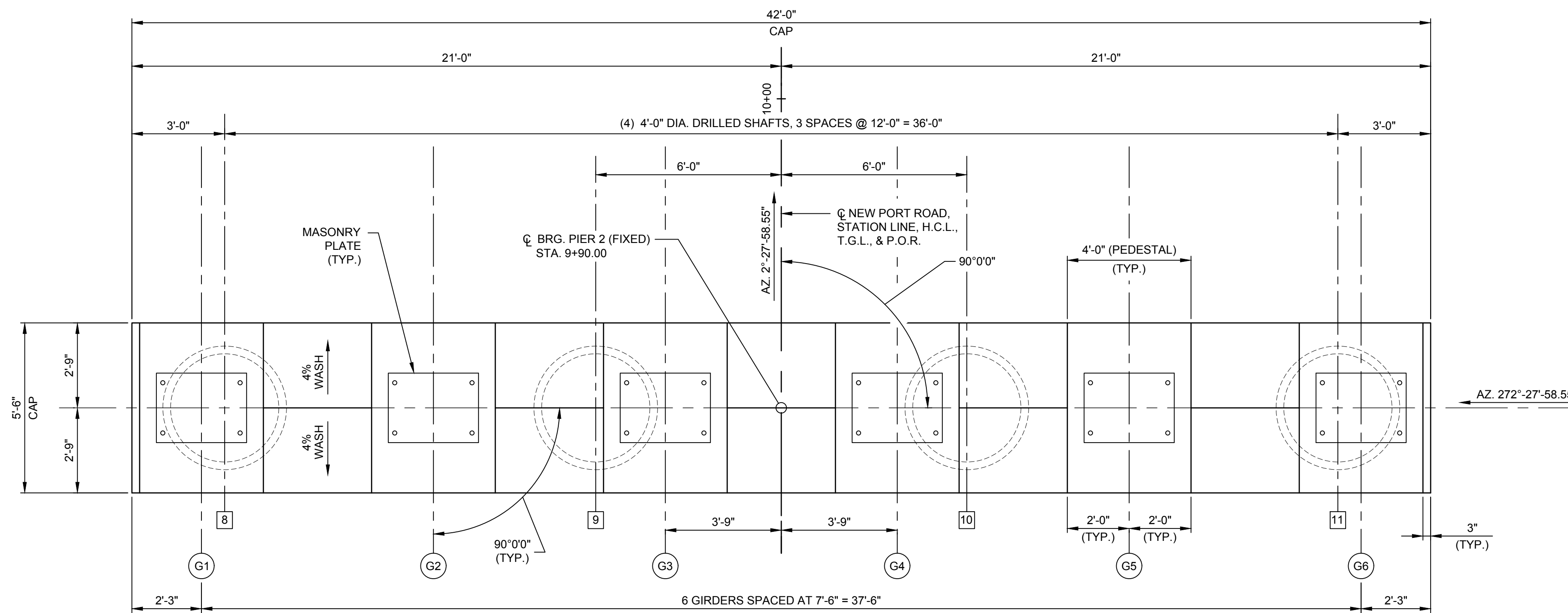
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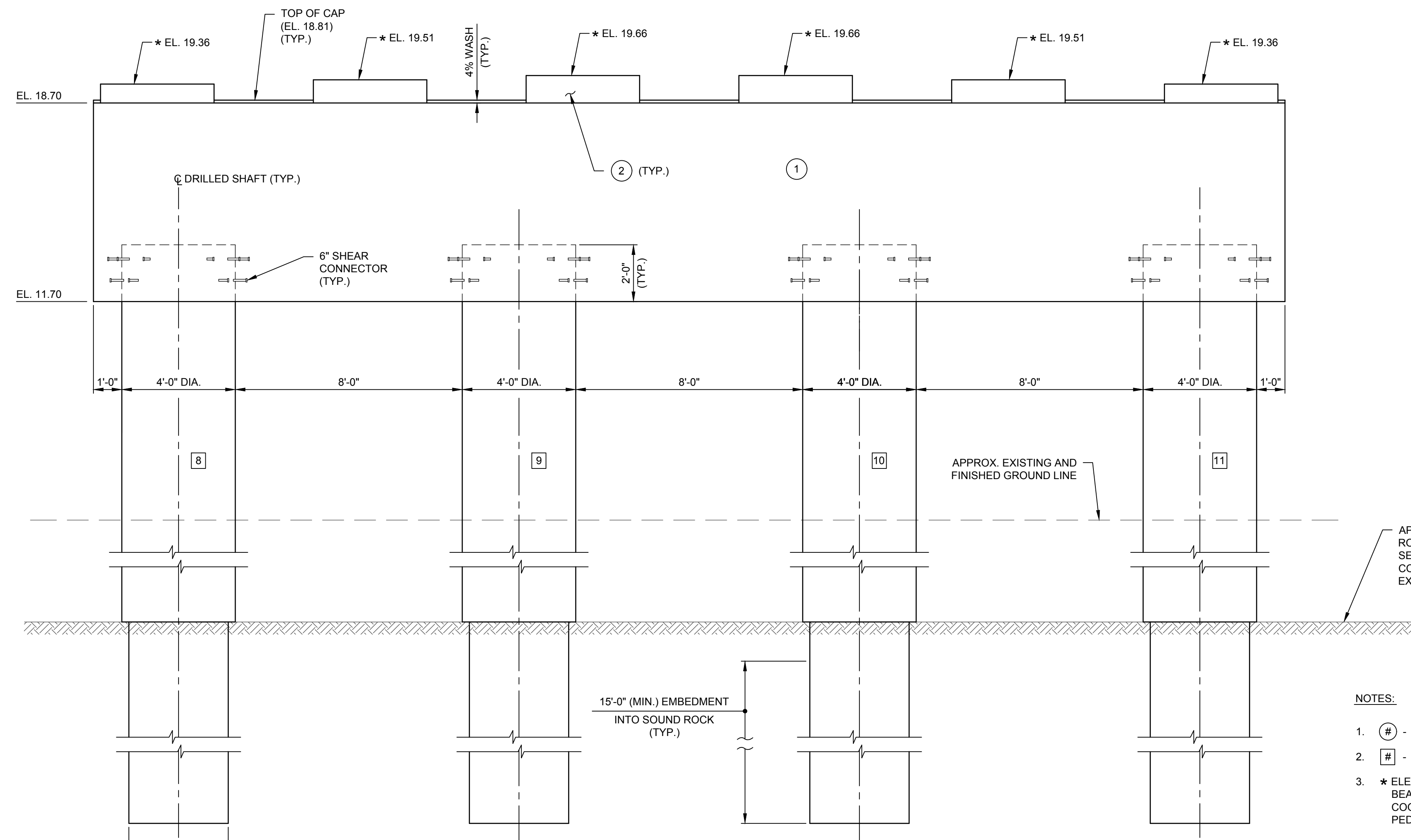
PIER 2 PLAN AND ELEVATION

DRAWING NUMBER

ST-15



PIER 2 PLAN
 SCALE: 3/8" = 1'-0"



PIER 2 ELEVATION
 SCALE: 3/8" = 1'-0"

NOTES:

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- # - INDICATES DRILLED SHAFT NUMBER.
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- THE 28 DAY COMPRESSIVE STRENGTH OF THE CONCRETE PIER CAP SHALL BE 5000 PSI.

APPROX. EXISTING ROCK SURFACE (EL. -84.0±)
 SEE GEOTECHNICAL REPORT FOR BORING LOGS.
 CONTRACTOR TO FIELD VERIFY EXACT LOCATION OF BEDROCK UNKNOWN.



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 60 RAILROAD PLACE
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 PROJECT: **PORT OF ALBANY SITE INFRASTRUCTURE IMPROVEMENTS**

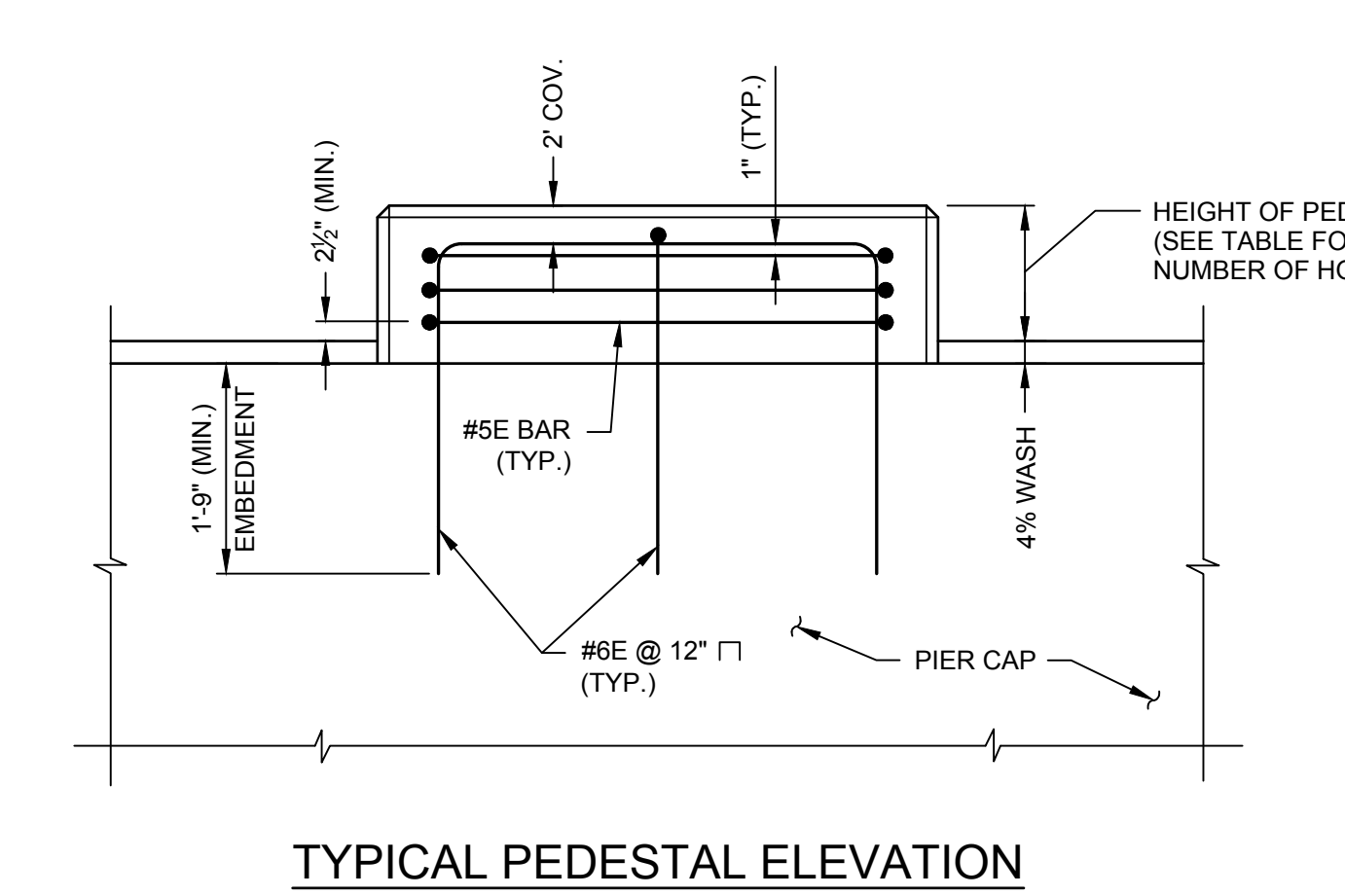
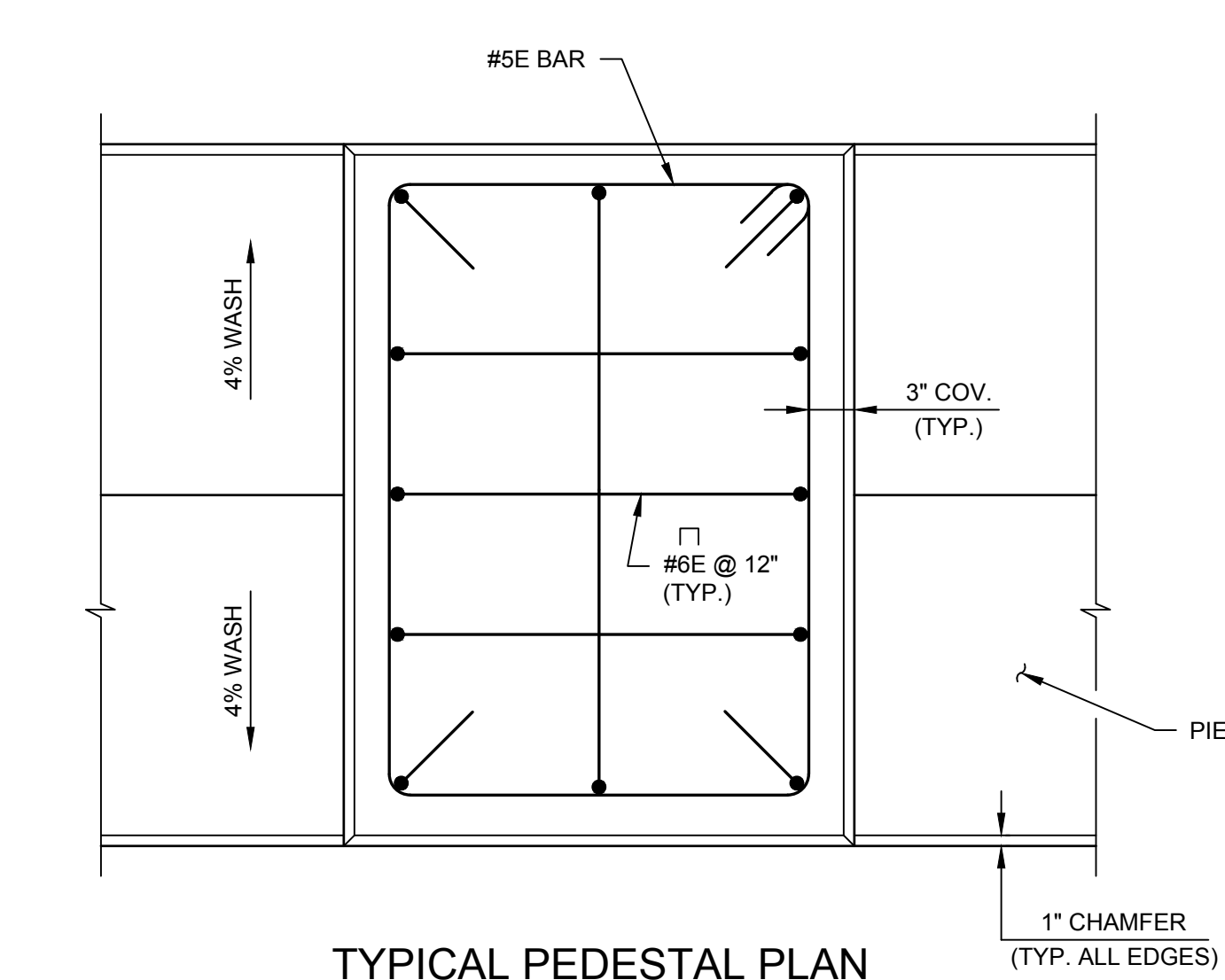
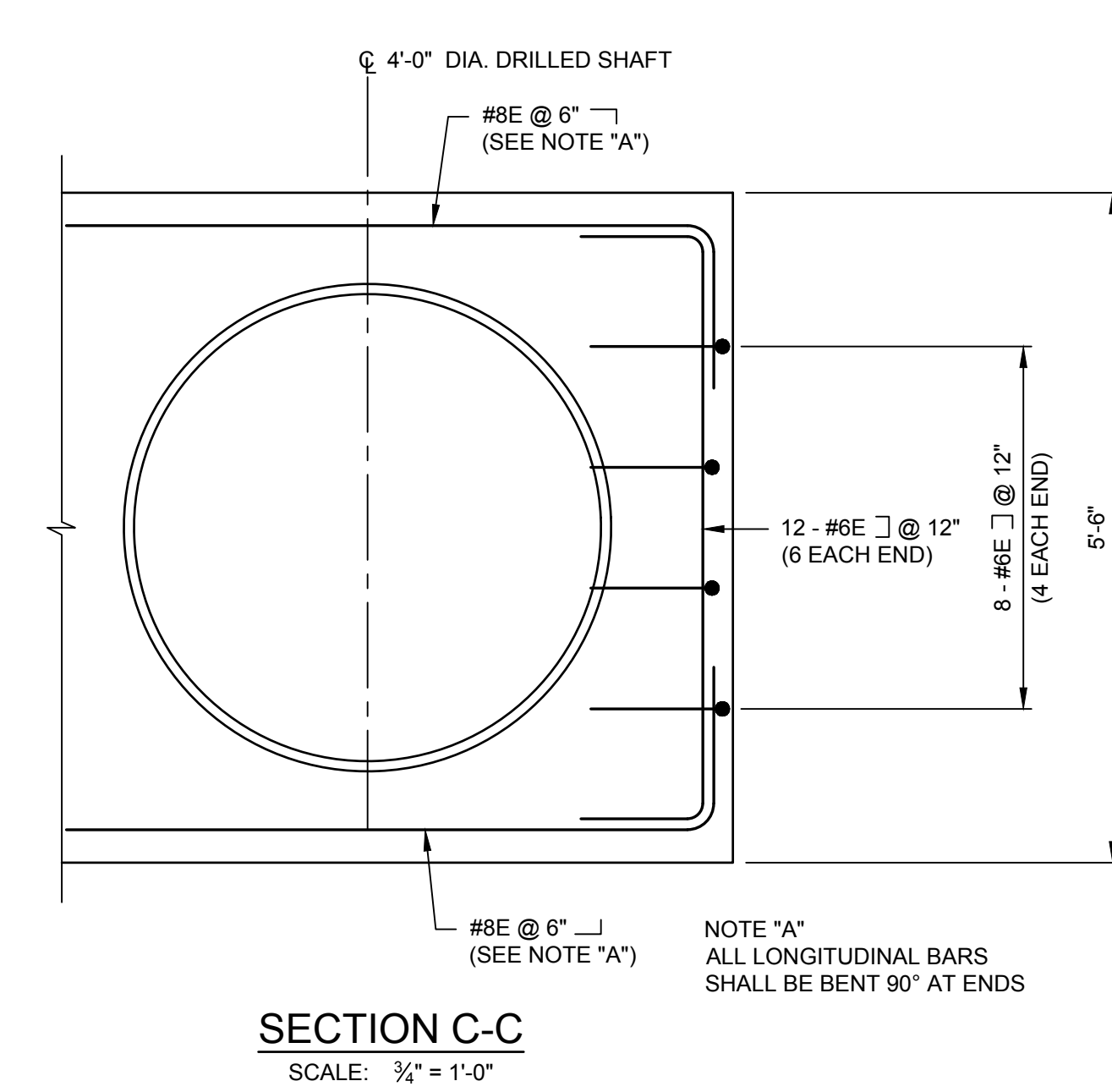
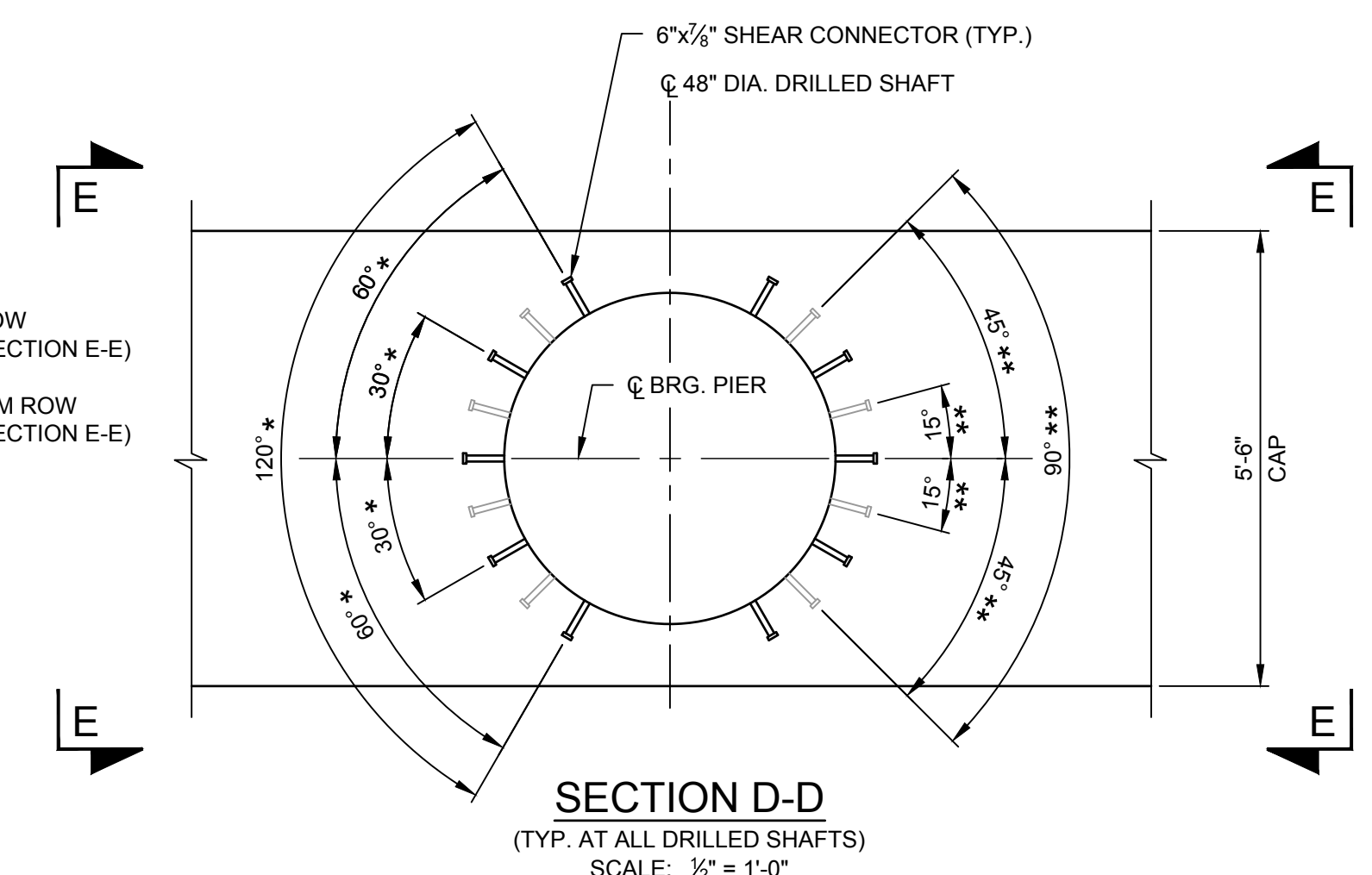
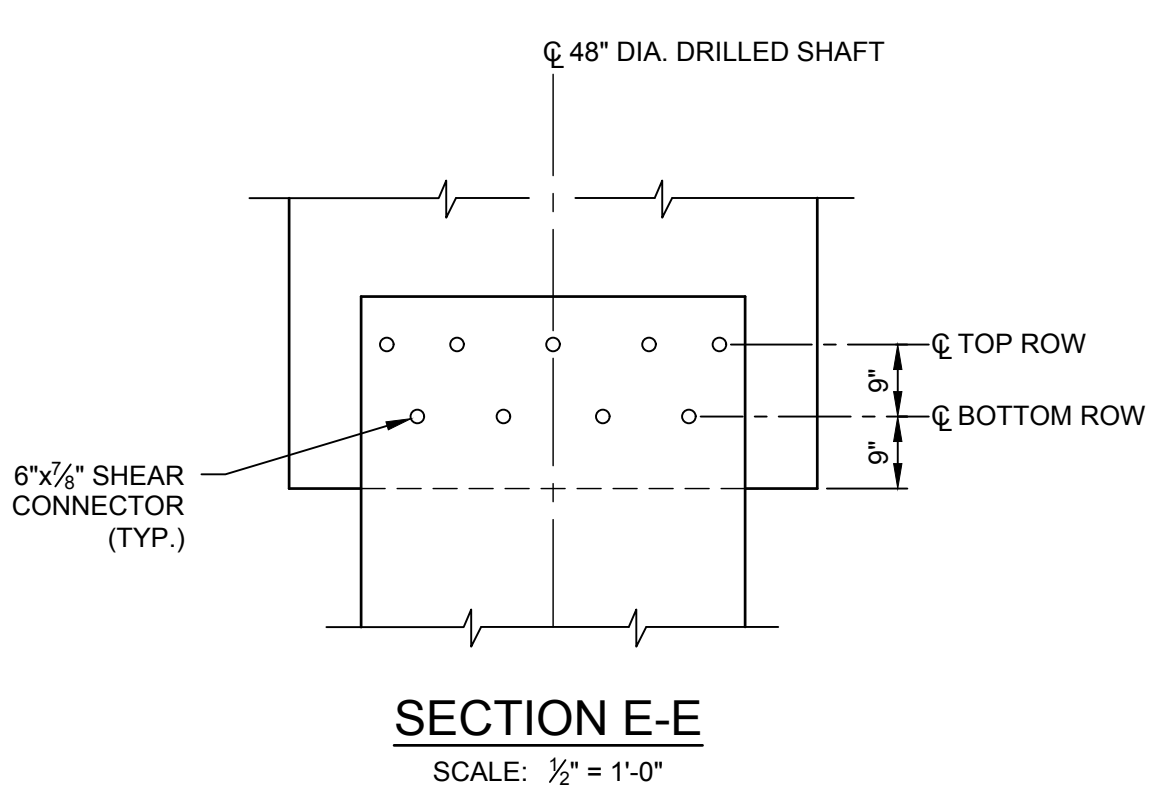
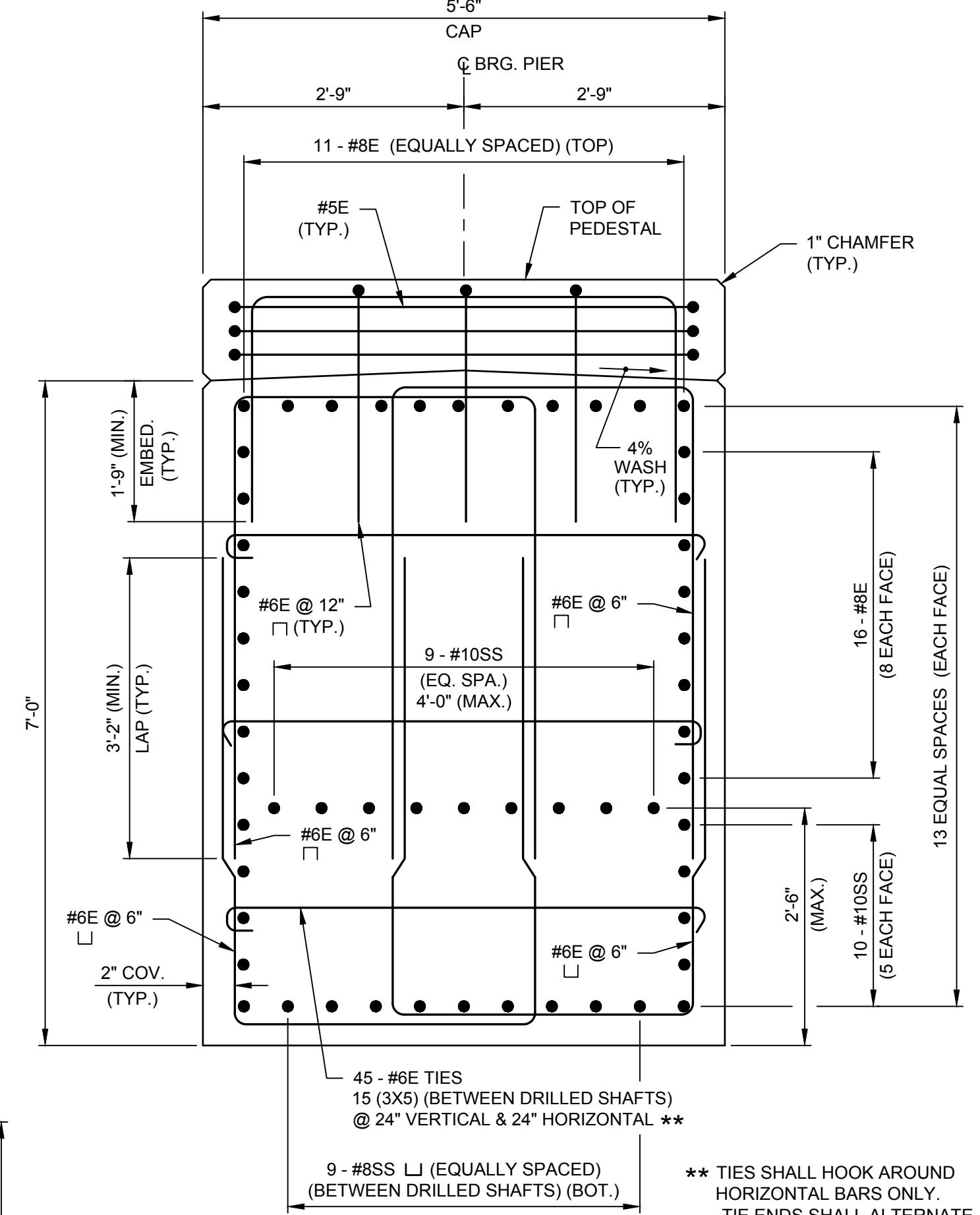
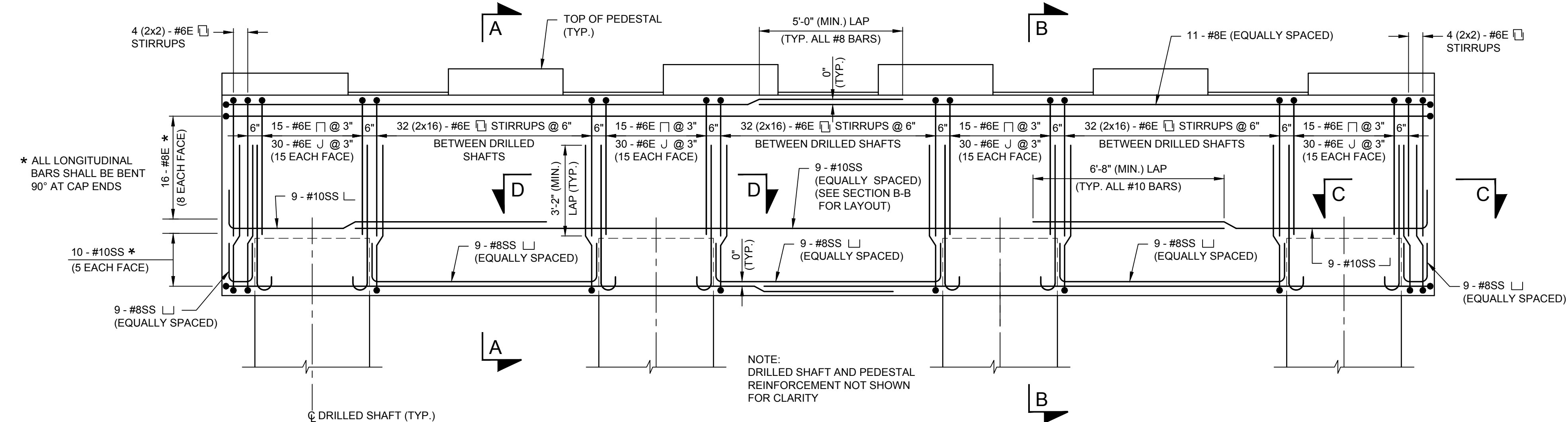
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| CHECKED | SIW |
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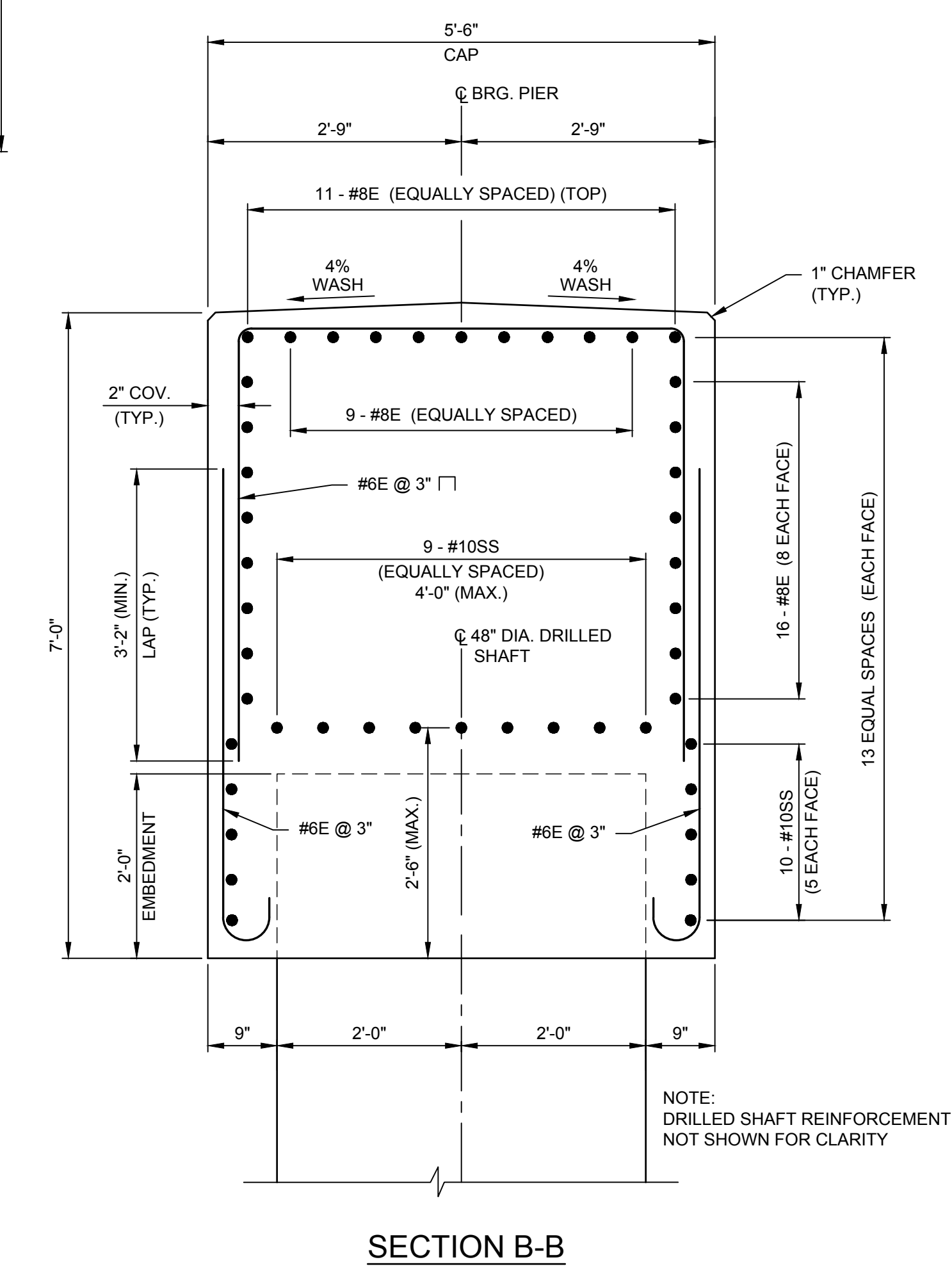
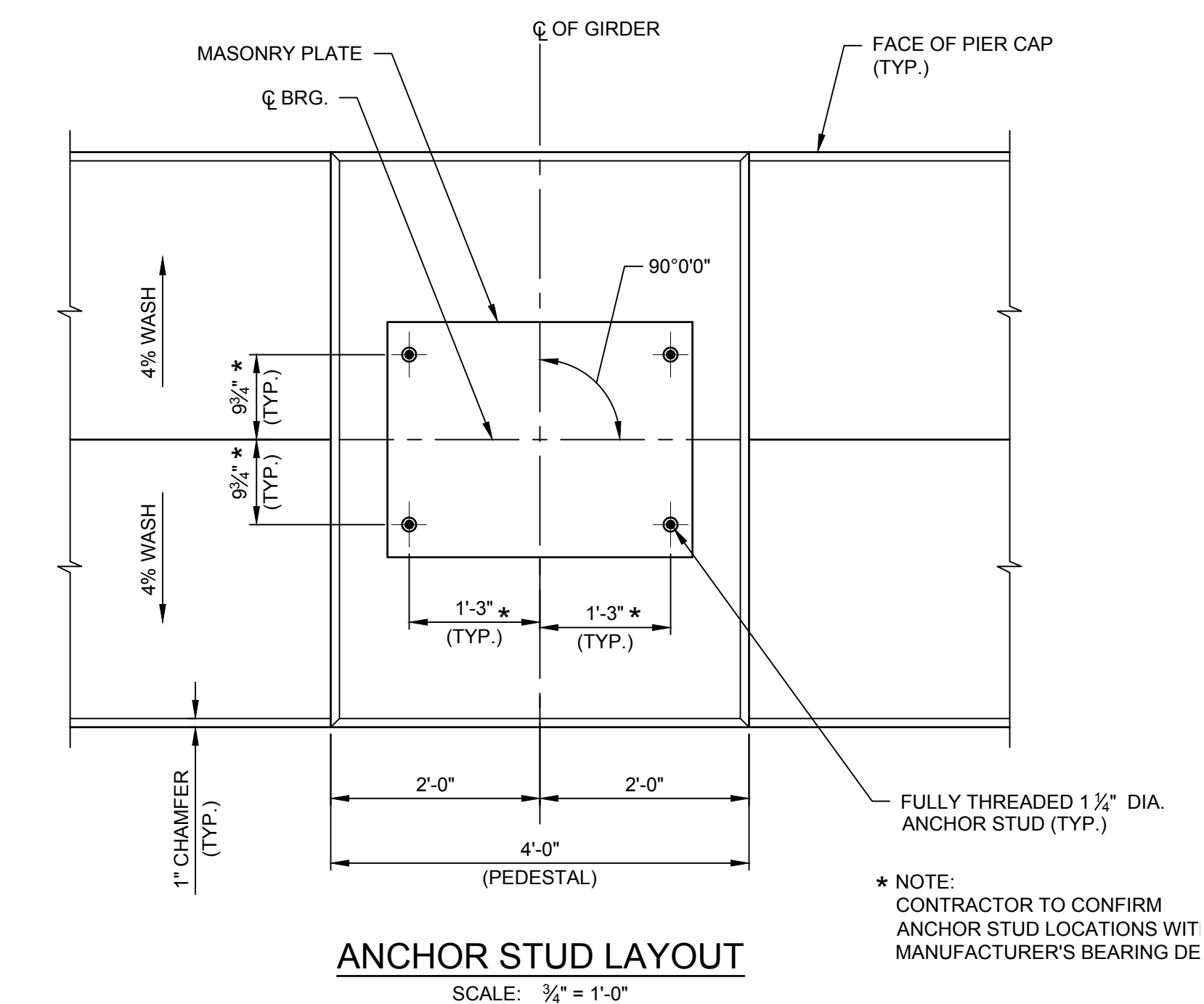
DRAWING TITLE
PIER DETAILS

DRAWING NUMBER
ST-16



| PEDESTAL HEIGHT | NUMBER OF HOOPS |
|-----------------|-----------------|
| 6" TO 8" | 1 |
| 8" TO 11" | 2 |
| 11" TO 14" | 3 |
| 14" TO 17" | 4 |
| 17" TO 20" | 5 |

PEDESTAL REINFORCEMENT DETAILS
 SCALE: 3/4" = 1'-0"



N:\18641\05 ALBANY PORT DISTRICT COMMISSION\DRAWINGS\PIER DETAILS\PIER.DWG



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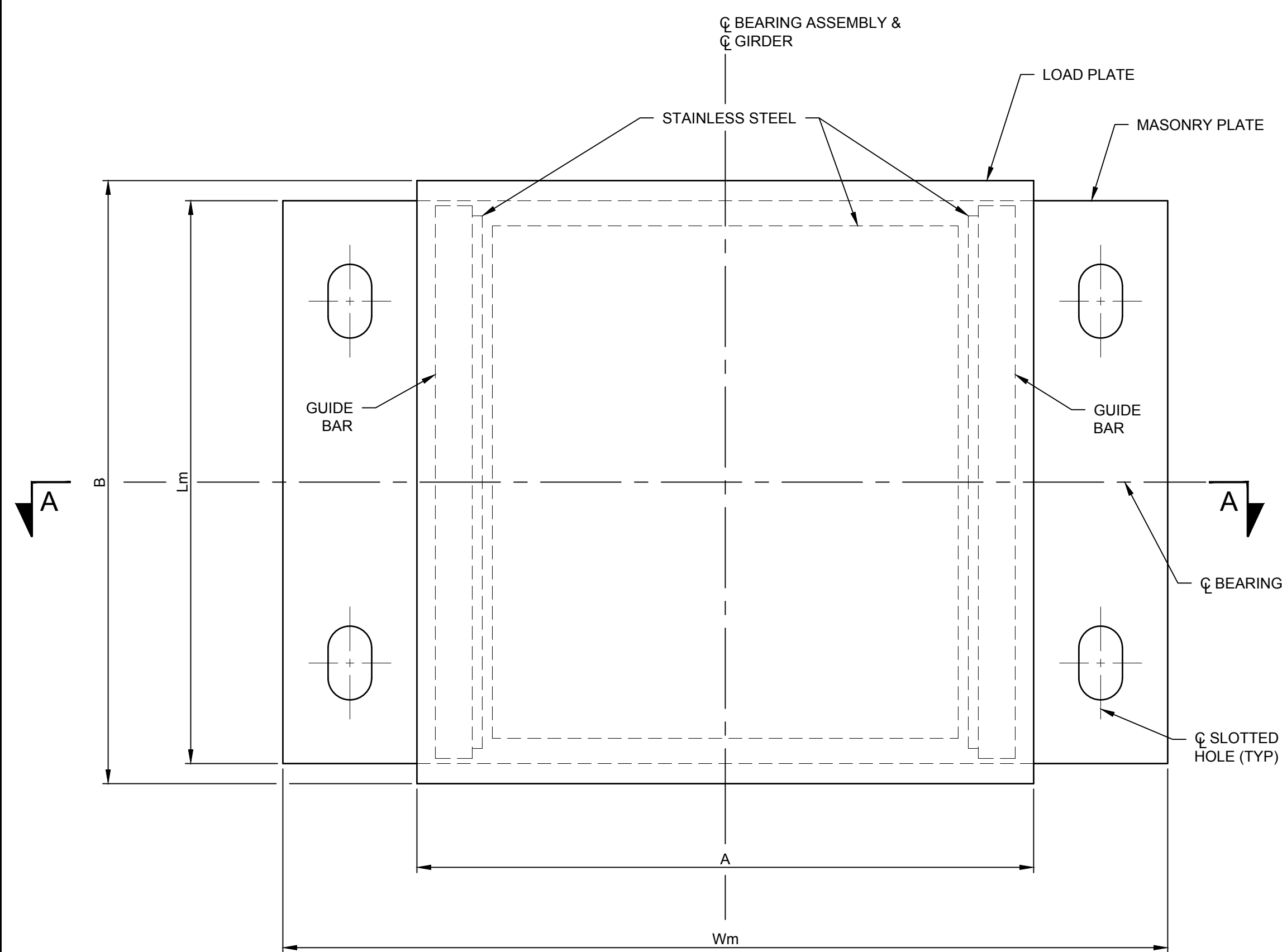


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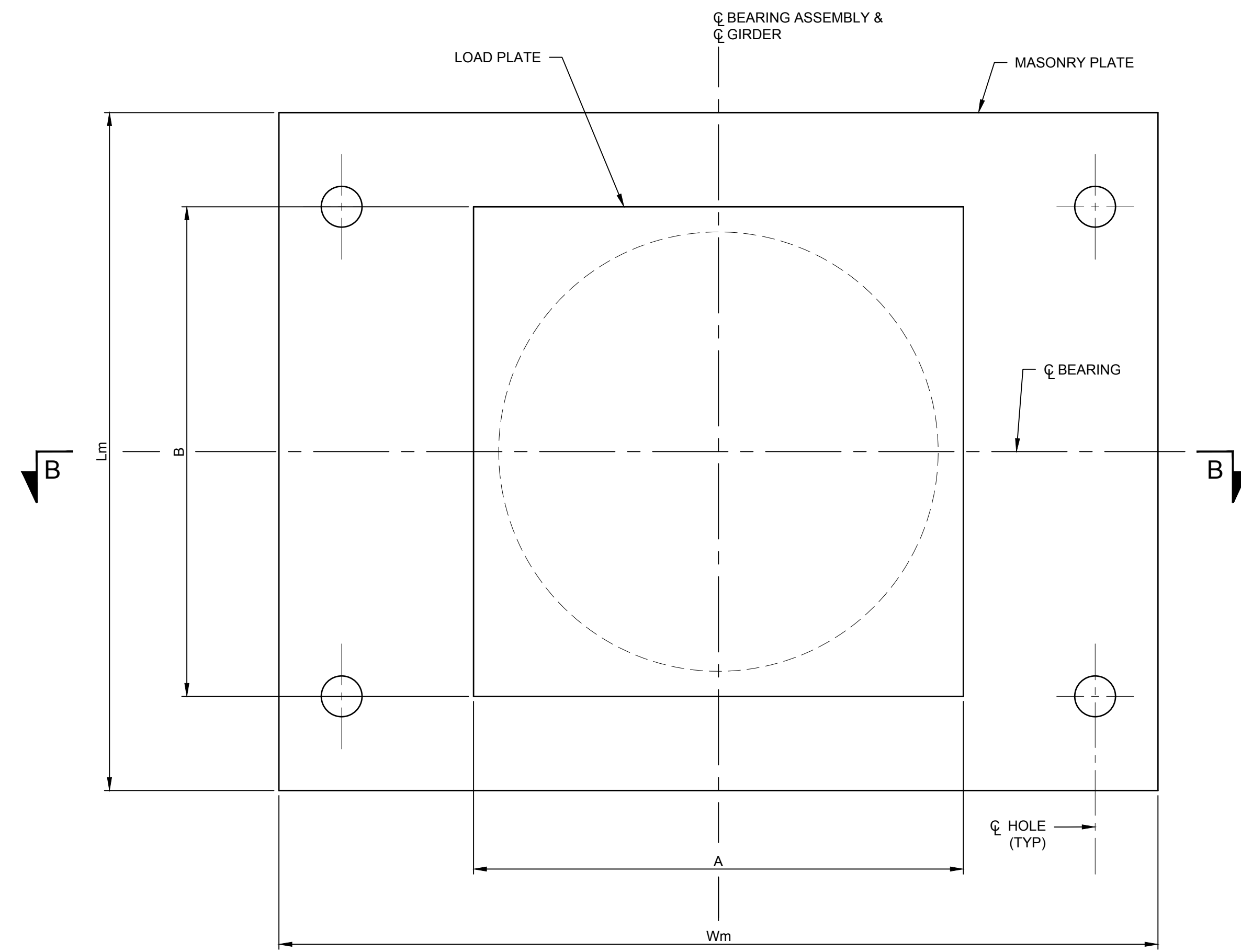
DRAWING TITLE
MULTI-ROTATIONAL BEARING DETAILS (1 OF 2)

DRAWING NUMBER

ST-17



GUIDED EXPANSION M.R. BEARING (AT ABUTMENT)
 (SOLE PLATE NOT SHOWN FOR CLARITY)
 SCALE: 3" = 1'-0"



FIXED M.R. BEARING (AT PIER)
 (SOLE PLATE NOT SHOWN FOR CLARITY)
 SCALE: 3" = 1'-0"

NOTES:

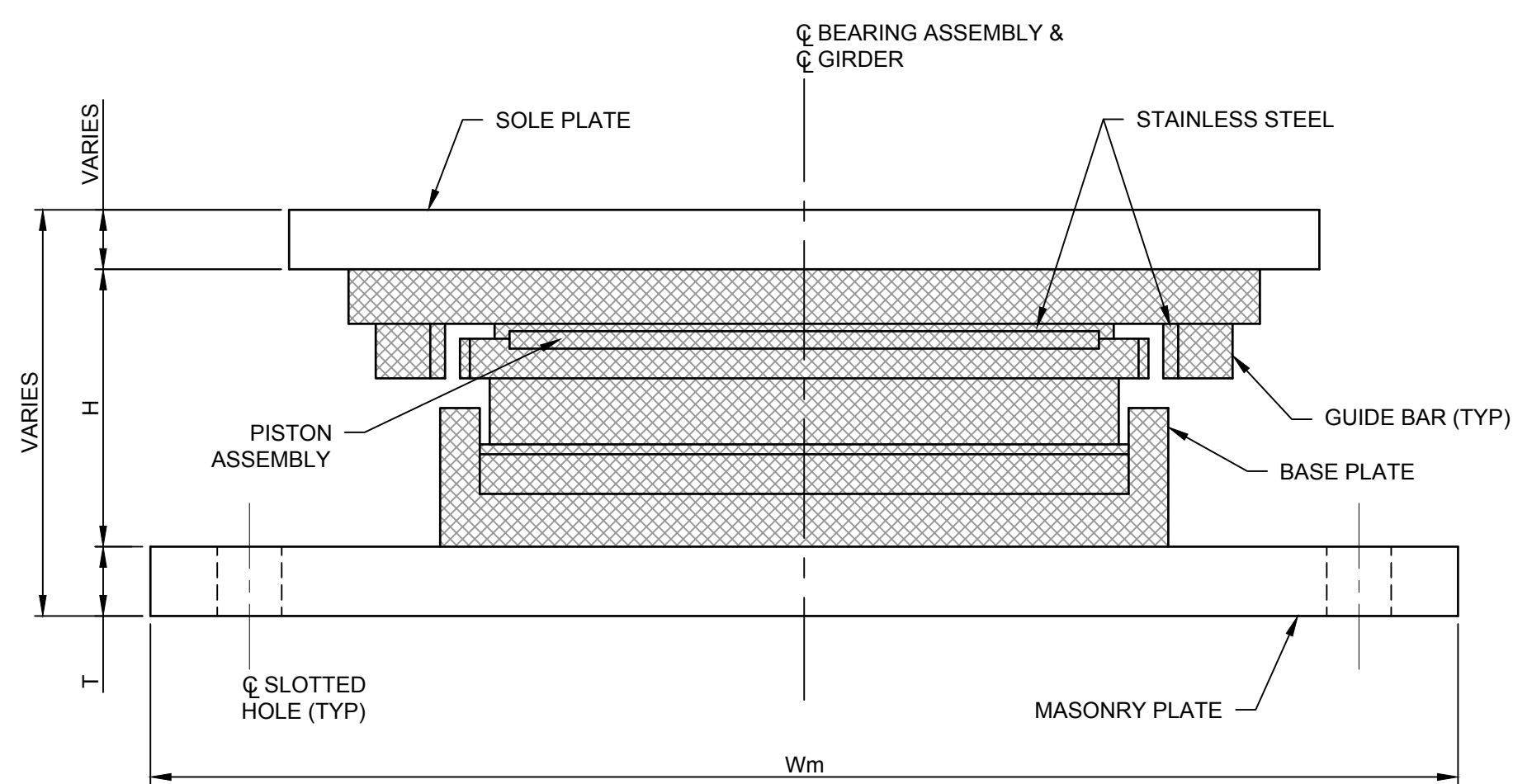
THE CONTRACTOR SHALL SUPPLY MULTI-ROTATIONAL STRUCTURAL BRIDGE BEARINGS CONFORMING TO THE REQUIREMENTS OF STANDARD SPECIFICATION SECTION 565 AND SUBJECT TO THE FOLLOWING CONDITIONS:

- THE BEARING DEVICE SUPPLIED SHALL BE CAPABLE OF TRANSMITTING THE LOADS AND MOVEMENT SHOWN ON THESE PLANS.
- THE CONTRACTOR'S ATTENTION IS DIRECTED TO THE NET VERTICAL UPLIFT AT EACH ABUTMENT BEARING LOCATION FOR THE EXTREME LIMIT STATE. IT IS THE RESPONSIBILITY OF THE CONTRACTOR TO DESIGN THE BEARING ASSEMBLY FOR UPLIFT RESTRAINT.
- THE DIMENSION "H" IN THE BEARING TABLE REPRESENTS THE ASSUMED TOTAL HEIGHT OF BEARING MECHANISM BETWEEN THE SOLE PLATE AND MASONRY PLATE USED BY THE DESIGNER TO ESTABLISH THE PEDESTAL ELEVATIONS. THE MINIMUM PEDESTAL HEIGHT SHALL NOT BE CHANGED WITHOUT WRITTEN APPROVAL OF THE ENGINEER-IN-CHARGE (EIC).
- ALL STEEL SHALL CONFORM TO ASTM A709, GRADE 50, METALIZED.
- ALL STEEL FABRICATION SHALL CONFORM TO THE PROVISIONS OF THE LATEST EDITION OF THE NEW YORK STATE STEEL CONSTRUCTION MANUAL.
- ALL METAL COMPONENTS OF THE BEARING SYSTEM WHICH ARE LIABLE TO COME INTO CONTACT DURING TRANSLATION SHALL HAVE A TEFLON SLIDING SURFACE FINISH.
- ALL EXPANSION BEARINGS SHALL HAVE A MAXIMUM FRICTION COEFFICIENT OF 3%.
- THE BEARING DEVICE, MASONRY PLATE, SOLE PLATE, ANCHOR STUDS, NUTS, WASHER PLATES, BEARING PAD, AND METALIZING SHALL BE INCLUDED IN THE UNIT PRICE BID FOR THE BEARING ITEMS.
- IF THE ANCHOR STUDS ARE SET UNDER THE SOLE PLATE, A MINIMUM CLEARANCE EQUAL TO TWO TIMES THE THICKNESS OF ANCHOR NUT PLUS 1" SHALL BE MAINTAINED BETWEEN THE TOP OF MASONRY PLATE AND BOTTOM OF THE SOLE PLATE.
- FOR BEARINGS OF CAPACITY > 500 KIPS, USE 3/8" MINIMUM BEARING PAD IN LIEU OF 1/2".
- WHEN THE THICKNESS OF THE MASONRY PLATE OR ANY OTHER PLATE BEING WELDED EXCEEDS 1 1/2", THE PLATES SHALL BE PREHEATED IN ACCORDANCE WITH THE NEW YORK STATE STEEL CONSTRUCTION MANUAL.
- SECTION 565-2.04 IS TO BE MODIFIED SUCH THAT WHERE PAINTING IS INDICATED, THE BEARINGS SHALL BE METALIZED ACCORDING TO ITEM 572.00020101, METALIZING. THE COST OF METALIZING SHALL BE INCLUDED IN THE BEARING ITEM.

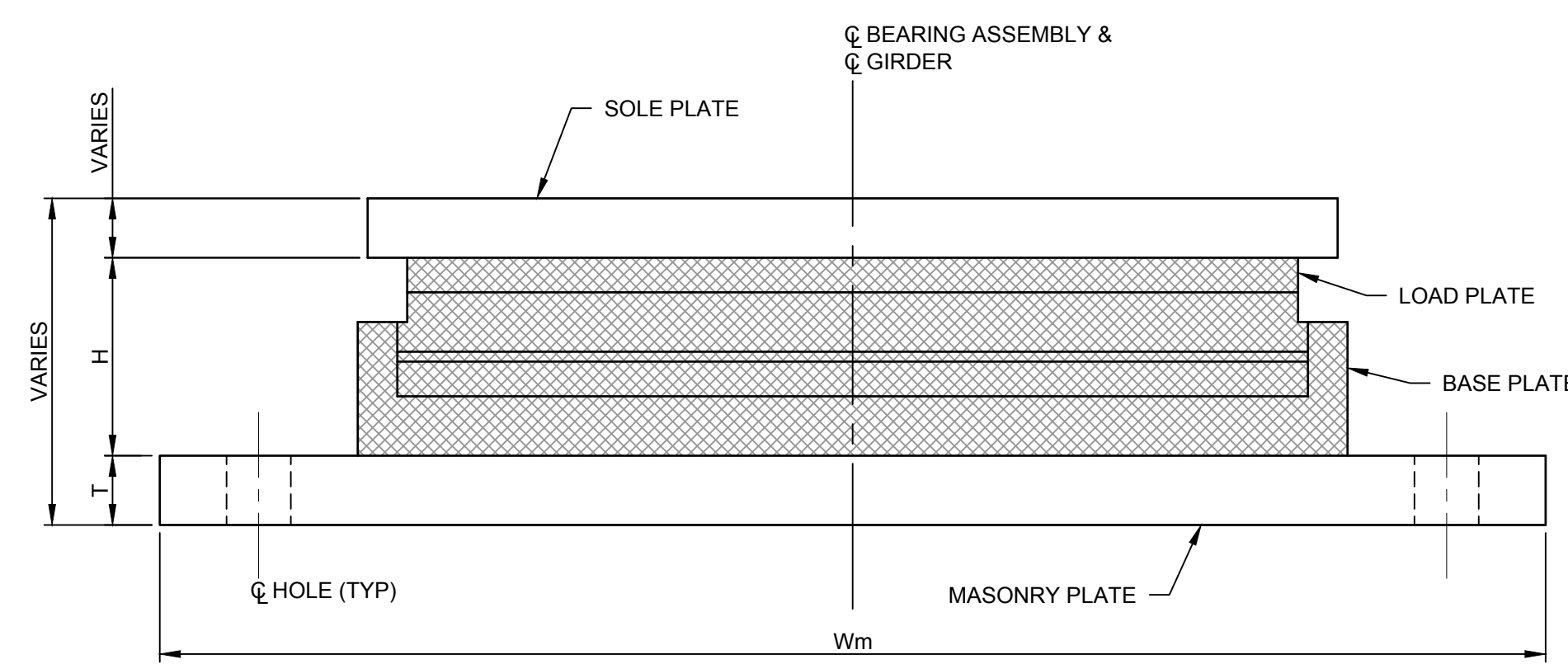
WHENEVER JACKING OF THE SUPERSTRUCTURE IS REQUIRED TO RESET THE BEARINGS, THE CONTRACTOR SHALL SUBMIT A JACKING PROCEDURE TO THE E.I.C. FOR APPROVAL.

LEGEND:

INDICATES PARTS DESIGNED BY THE MANUFACTURER



SECTION A-A
GUIDED EXPANSION M.R. BEARING (AT ABUTMENT)
 SCALE: 3" = 1'-0"



SECTION B-B
FIXED M.R. BEARING (AT PIER)
 SCALE: 3" = 1'-0"

| LOCATION | FIX./EXP. | ITEM NO. | QUANTITY REQUIRED | DESIGN LOADS (KIPS) | | | | | | | | | | ONE WAY LONG. MOVEMENT ** | (G) GUIDE CLEARANCE | BEARING TABLE | | | | | | | | | | | | | | | | |
|----------------|-----------|----------|-------------------|---------------------|----|-------|-------|--------|------------------|------|------|-------------------|---------------|---------------------------|---------------------|---------------|------|--------------|-------|------------|-------|-------|-------|------------|------|--------------|-------|-----------|------|-------------|--------|--------|
| | | | | VERTICAL (SERVICE) | | | | | STRENGTH | | | EXTREME VERT. *** | MASONRY PLATE | | | | | WASHER PLATE | | SOLE PLATE | | | | LOAD PLATE | | ANCHOR STUDS | | WELD SIZE | | | | |
| | | | | DC | DW | LL+IM | VERT. | HORIZ. | ROTATION (RAD) * | Lm | Wm | | T | | | Et | EI | Am | Bm | Awp | Bwp | LS | WS | T1 | T2 | A | B | BRG. H | DIA. | STUDS/ BRG. | W1 | W2 |
| SOUTH ABUTMENT | EXP | 565.1523 | 6 | 97 | 6 | 293 | 527 | 60 | 0.01373 | -108 | 2.65 | 0.25 | 21.00 | 32.00 | 1.75 | 2.50 | 3.75 | 2.75 | 1.625 | 3.75 | 2.625 | 25.00 | 26.00 | 0.75 | 1.75 | 23.00 | 22.50 | 7.00 | 1.25 | 4 | 0.3125 | 0.3125 |
| PIER 1 | FIX | 565.1725 | 6 | 342 | 22 | 378 | 974 | 143 | 0.01323 | 180 | 0.00 | N/A | 27.00 | 34.00 | 1.75 | 2.50 | 3.75 | 1.625 | 1.625 | 2.625 | 2.625 | 24.00 | 26.00 | 1.375 | 1.75 | 19.50 | 19.50 | 5.00 | 1.25 | 4 | 0.3125 | 0.3125 |
| PIER 2 | FIX | 565.1725 | 6 | 342 | 22 | 378 | 974 | 143 | 0.01323 | 180 | 0.00 | N/A | 27.00 | 34.00 | 1.75 | 2.50 | 3.75 | 1.625 | 1.625 | 2.625 | 2.625 | 24.00 | 26.00 | 1.375 | 1.75 | 19.50 | 19.50 | 5.00 | 1.25 | 4 | 0.3125 | 0.3125 |
| NORTH ABUTMENT | EXP | 565.1523 | 6 | 97 | 6 | 293 | 527 | 60 | 0.01373 | -108 | 2.65 | 0.25 | 21.00 | 32.00 | 1.75 | 2.50 | 3.75 | 2.75 | 1.625 | 3.75 | 2.625 | 25.00 | 26.00 | 0.75 | 0.75 | 23.00 | 22.50 | 7.00 | 1.25 | 4 | 0.3125 | 0.3125 |

TABLE DIMENSIONS ARE IN INCHES UNLESS OTHERWISE NOTED.

*MAXIMUM ROTATION AT C OF BEARINGS DUE TO LL+I

**MAXIMUM ONE WAY LONGITUDINAL MOVEMENT (EXPANSION OR CONTRACTION) OF THE SUPERSTRUCTURE WHEN BEARINGS ARE SET AT 68°F PLUS 1 INCH OF TOLERANCE

*** NEGATIVE VERTICAL LOADS DENOTES UPLIFT.

T2 IS UPSTATION OF T1



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| PROJECT | 18641.00 |

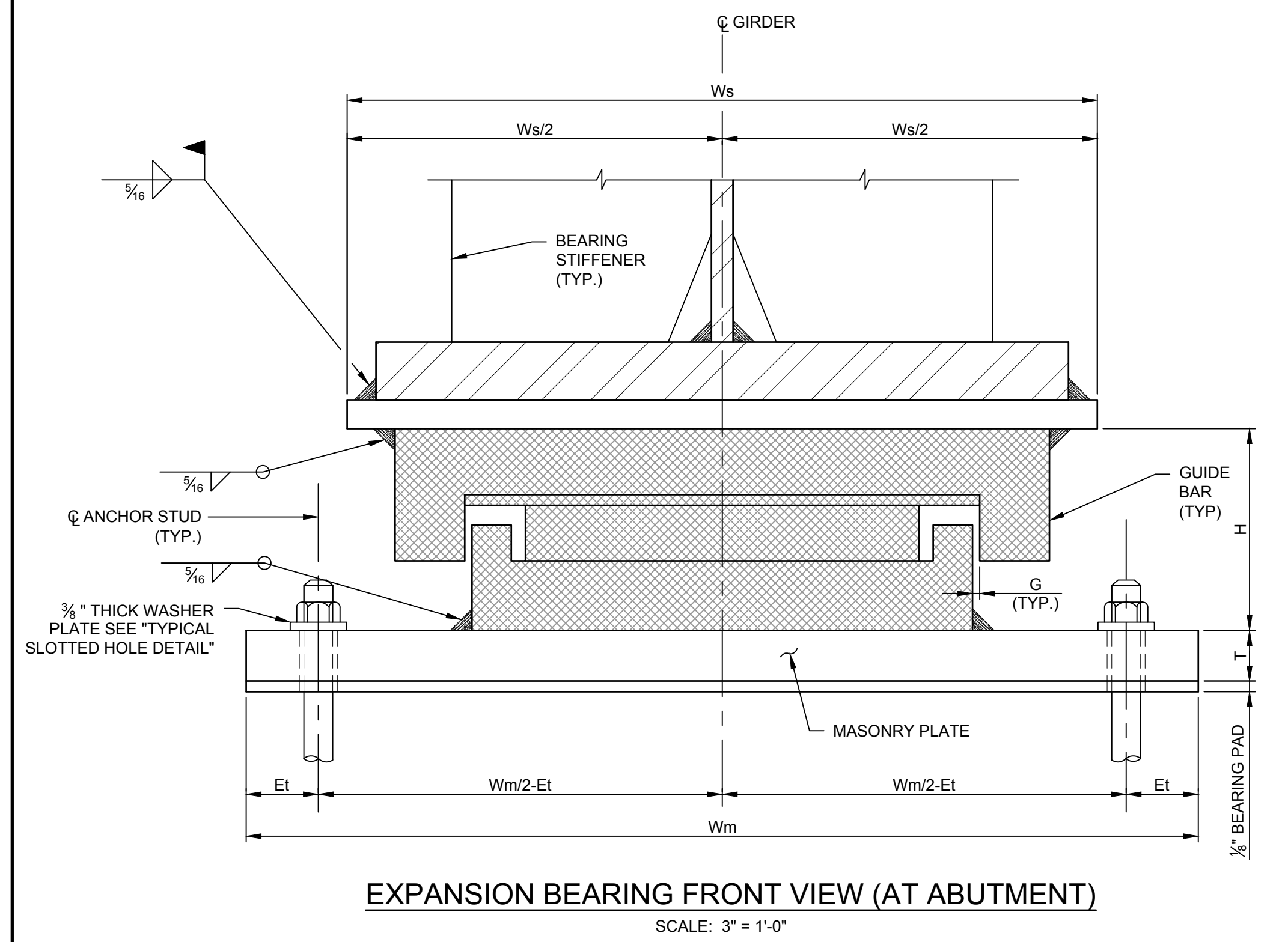


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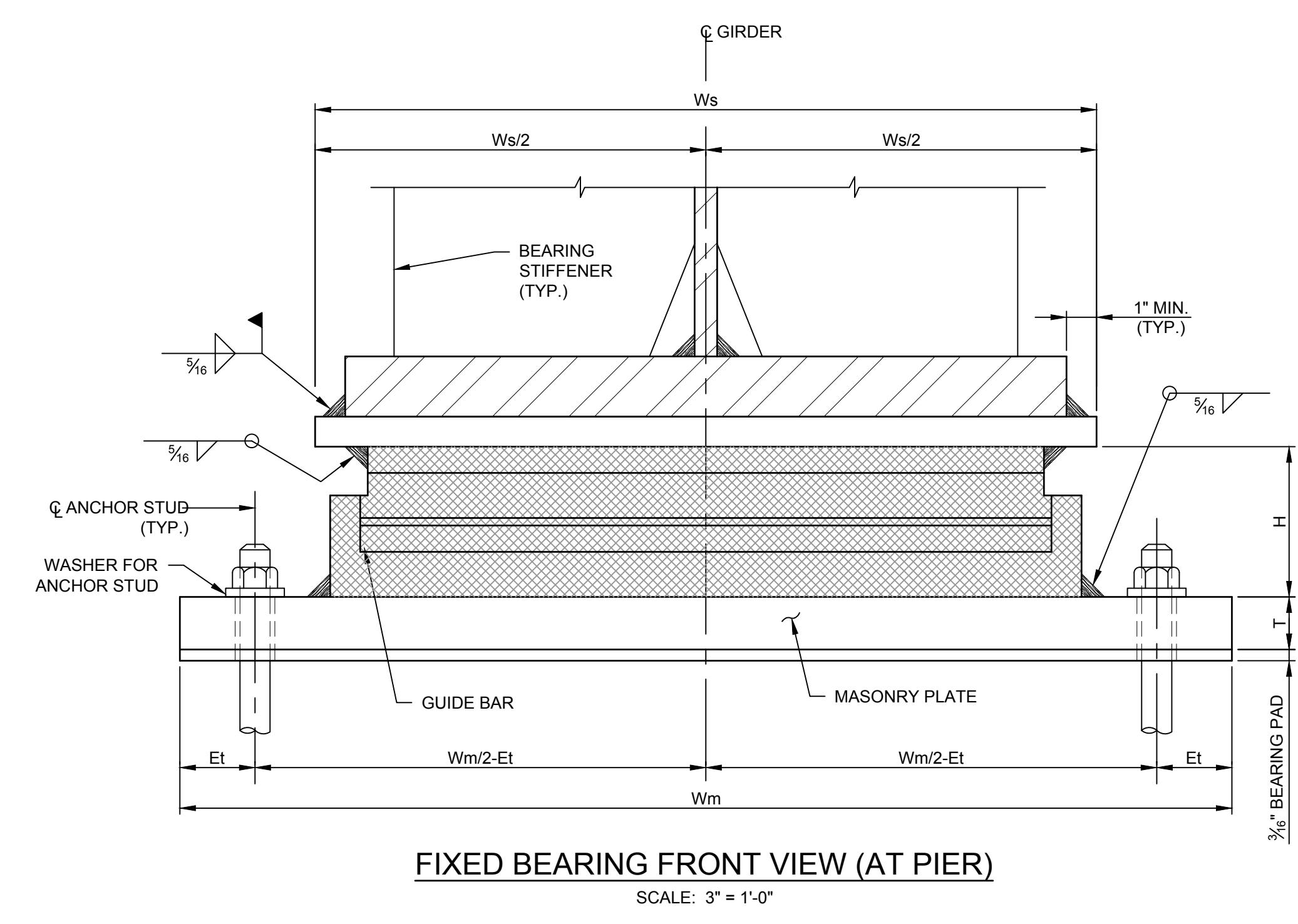
DRAWING TITLE
MULTI-ROTATIONAL BEARING DETAILS (2 OF 2)

DRAWING NUMBER

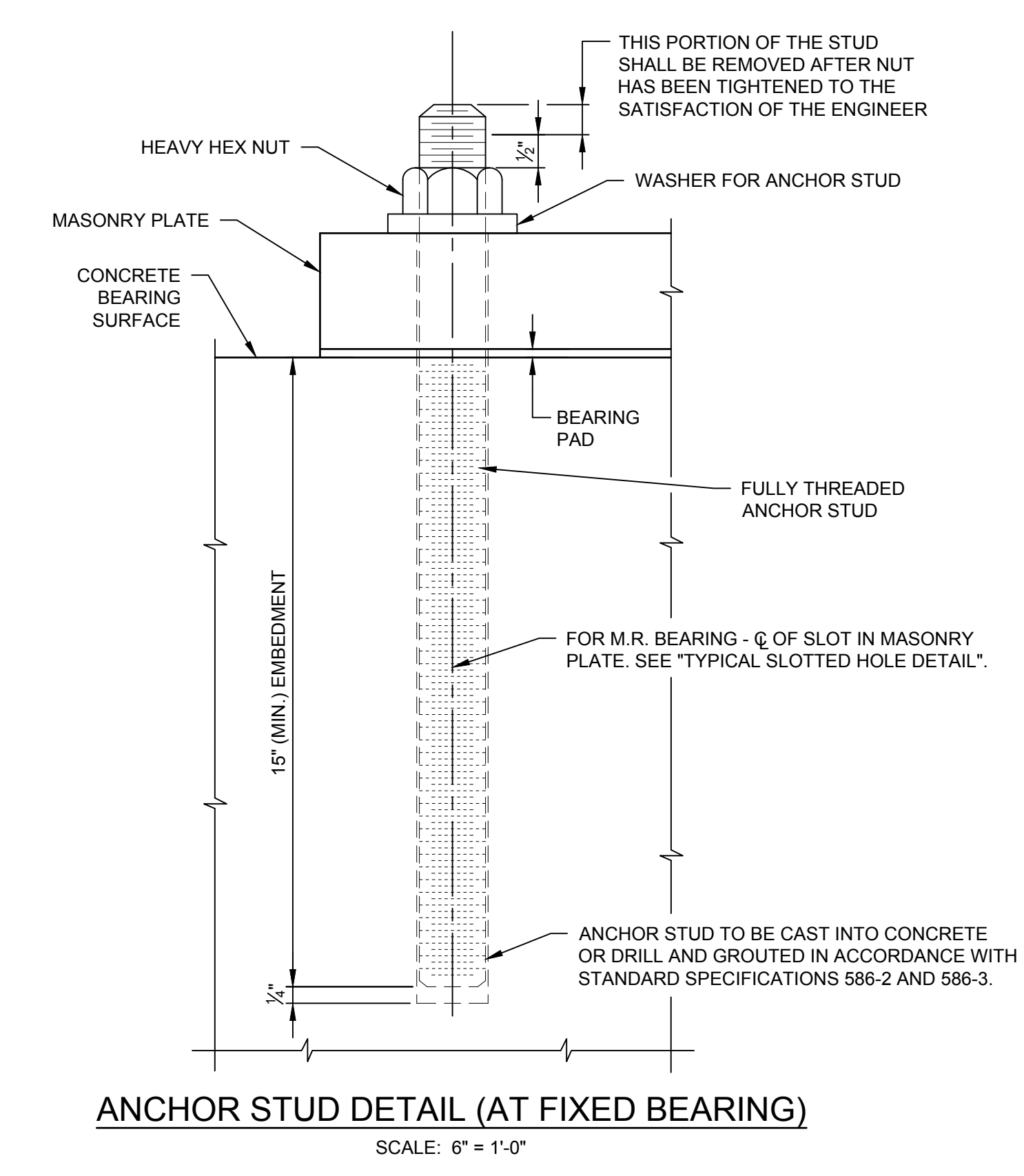
ST-18



EXPANSION BEARING FRONT VIEW (AT ABUTMENT)
 SCALE: 3" = 1'-0"

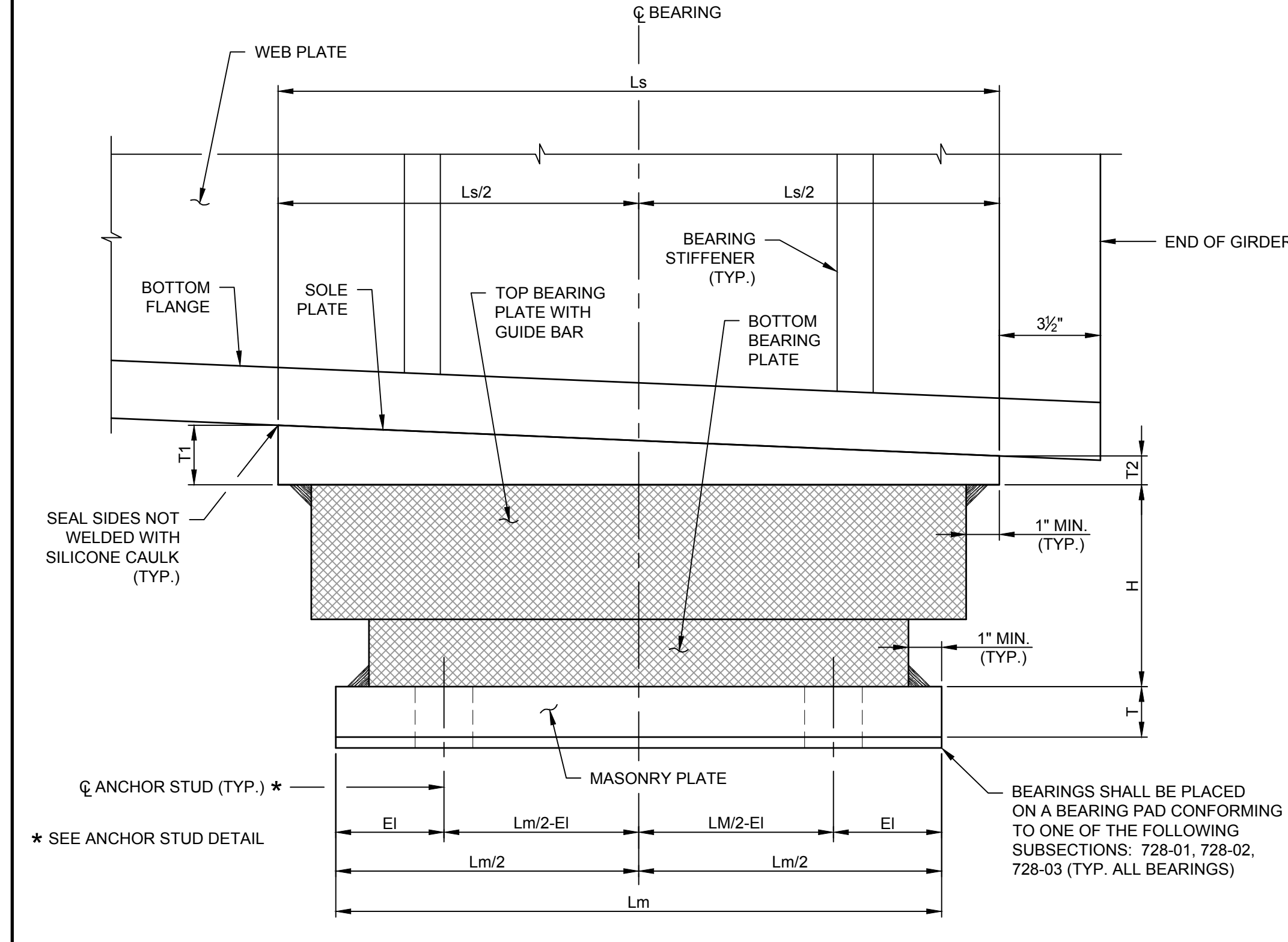


FIXED BEARING FRONT VIEW (AT PIER)
 SCALE: 3" = 1'-0"

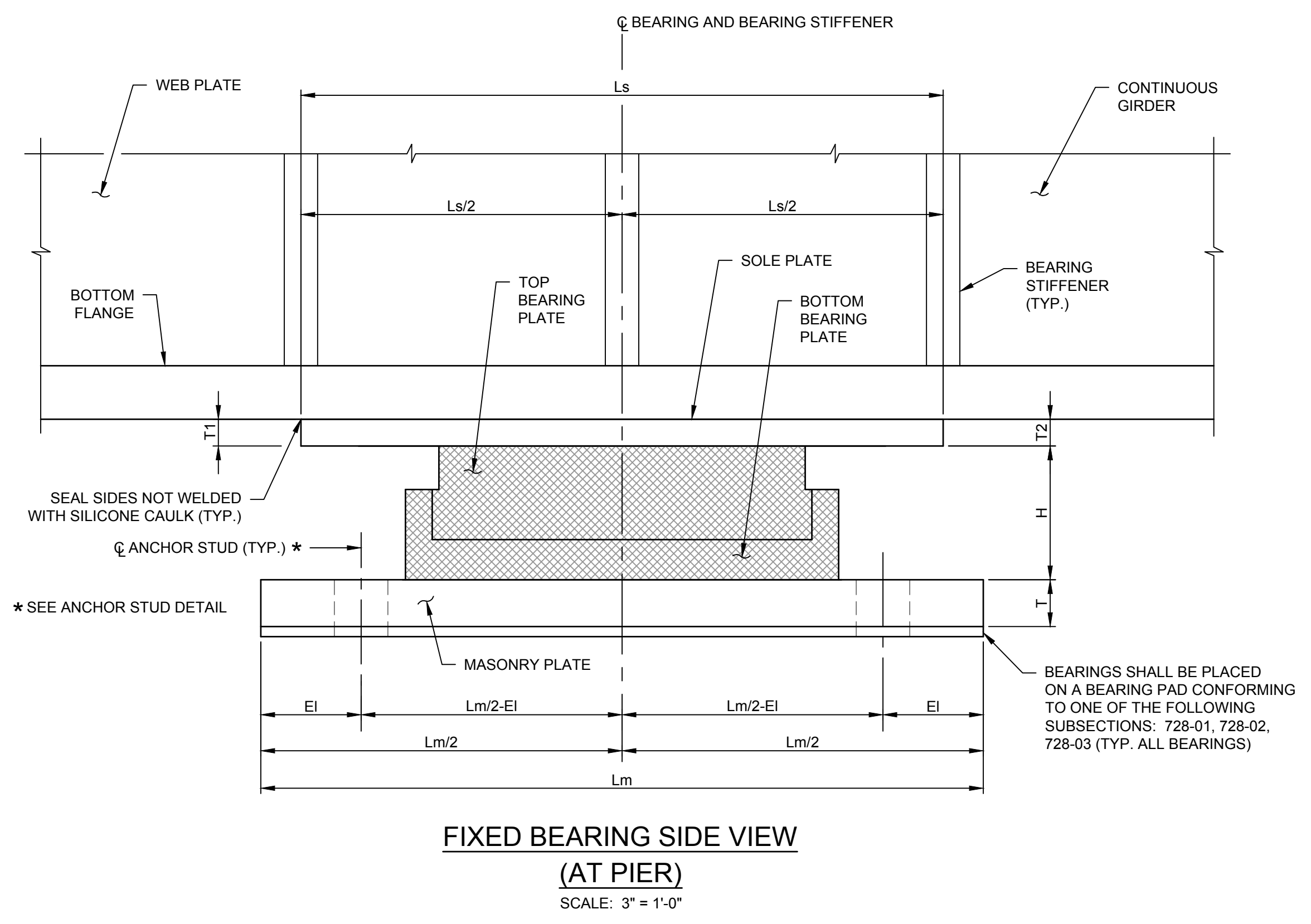


ANCHOR STUD DETAIL (AT FIXED BEARING)
 SCALE: 6" = 1'-0"

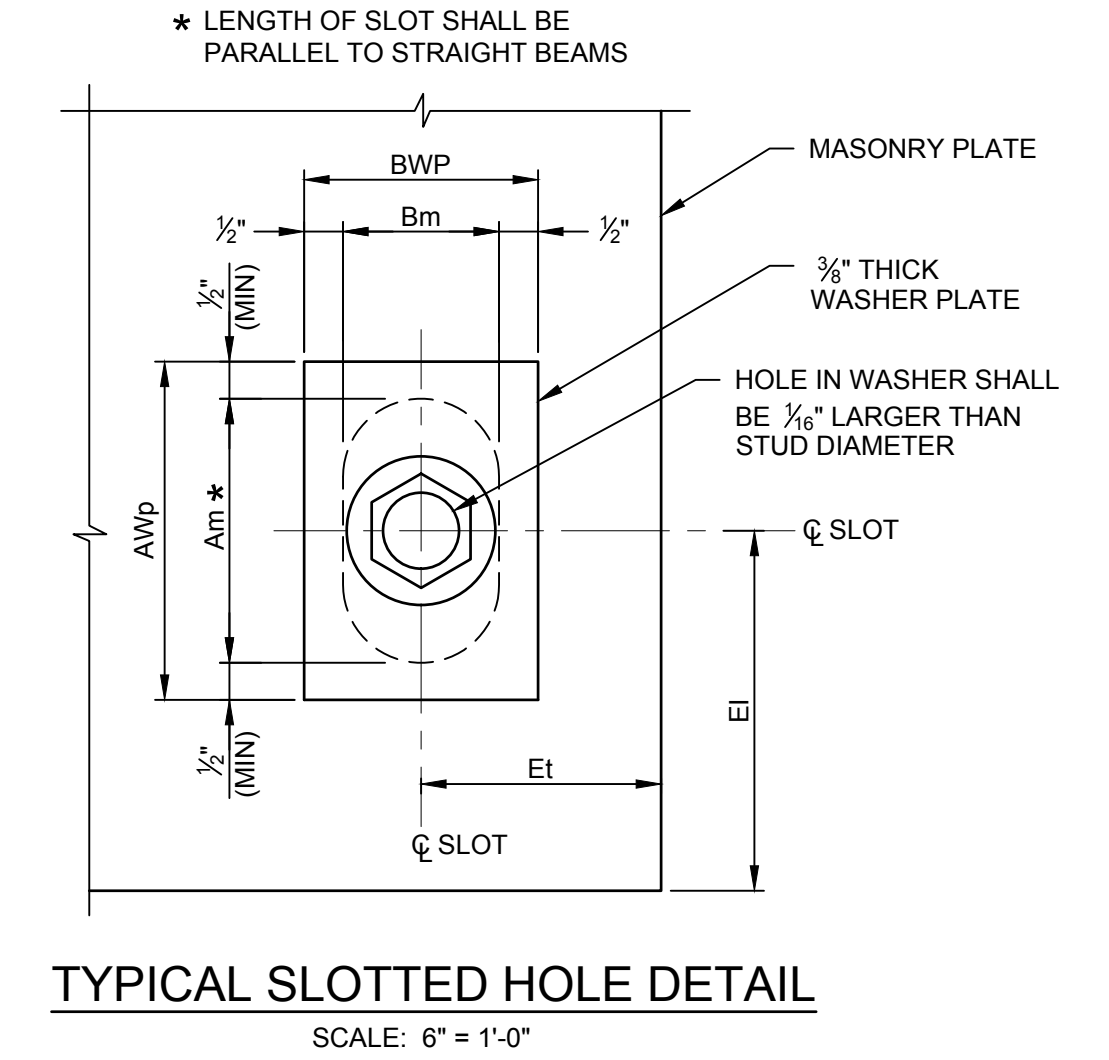
ANCHOR STUDS, WASHERS, WASHER PLATES, ANCHOR PLATES, AND NUTS SHALL MEET THE REQUIREMENTS OF SUBSECTION 723-60. THEY SHALL BE GALVANIZED IN ACCORDANCE WITH THE REQUIREMENTS OF MATERIAL SUBSECTION 719-01, "GALVANIZED COATINGS AND REPAIR METHODS". THE COST, INCLUDING GALVANIZING, SHALL BE INCLUDED IN THE UNIT PRICE BID FOR THE BEARING ITEM.



EXPANSION BEARING SIDE VIEW (AT ABUTMENT)
 SCALE: 3" = 1'-0"



FIXED BEARING SIDE VIEW (AT PIER)
 SCALE: 3" = 1'-0"



TYPICAL SLOTTED HOLE DETAIL
 SCALE: 6" = 1'-0"

LEGEND:
 INDICATES PARTS DESIGNED BY THE MANUFACTURER.

N:\18641\01 ALBANY PORT EXPANSION BEARINGS\DWG\DETAILS\BRIDGE\FINAL\ALBANY PORT EXPANSION BEARINGS\DWG\DETAILS\BRIDGE\DWG



McFarland Johnson
 60 RAILROAD PLACE
 SUITE 402
 SARATOGA SPRINGS, NEW YORK 12866
 P: 518-580-9380 F: 518-580-9383
 SaratogaROM@mjinc.com

PROJECT MILESTONE
GMP PLANS

| NO. | DATE | DESCRIPTION |
|-----|----------|-----------------------|
| 1 | 06/08/22 | GMP BID SET |
| 2 | 10/28/22 | GMP BID SET REVISIONS |
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CLIENT:
ALBANY PORT DISTRICT COMMISSION
 ALBANY, NEW YORK

PROJECT:
PORT OF ALBANY SITE INFRASTRUCTURE IMPROVEMENTS

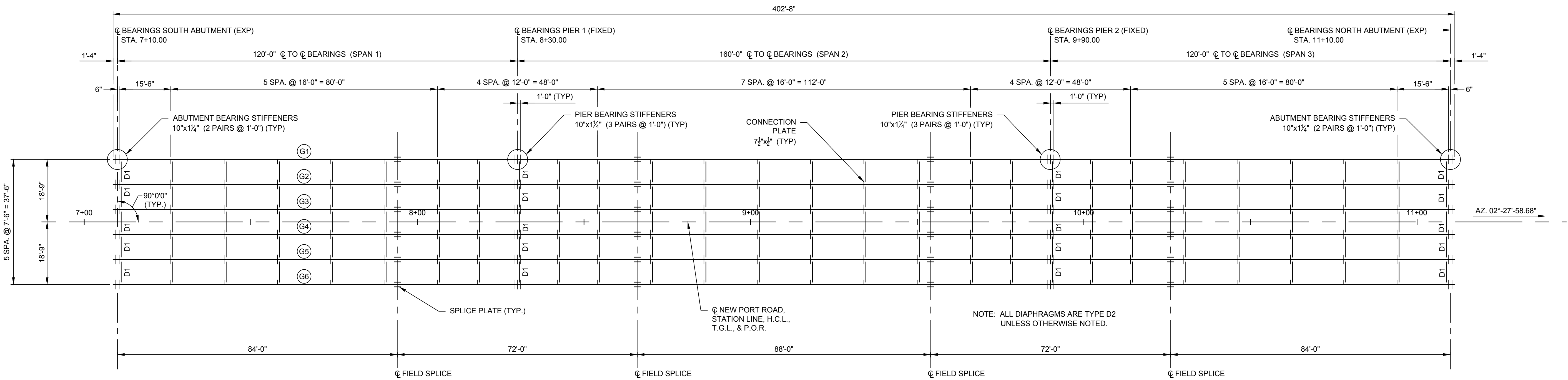
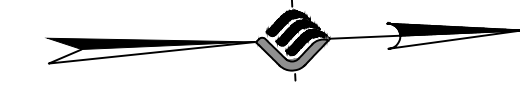
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| DRAWN | SLM |
| DESIGNED | DRW |
| CHECKED | JLR |
| SCALE | AS SHOWN |
| DATE | 6/8/2022 |
| PROJECT | 18641.00 |



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DRAWING TITLE
FRAMING PLAN

DRAWING NUMBER
ST-20



FRAMING PLAN
 SCALE: 1/16" = 1'-0"

NOTES:

1. SEE BEARING STIFFENER DETAILS ON SHEET ST-22.

NUMBER OF ALBANY PORT EXPANSION DRAWINGS SHEET FILES: BRIDGE FINAL PLANS (14) DELETED (14) FRAME EWS



McFarland Johnson
 60 RAILROAD PLACE
 SUITE 402
 SARATOGA SPRINGS, NEW YORK 12866
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 ALBANY, NEW YORK
 PROJECT: **PORT OF ALBANY SITE INFRASTRUCTURE IMPROVEMENTS**

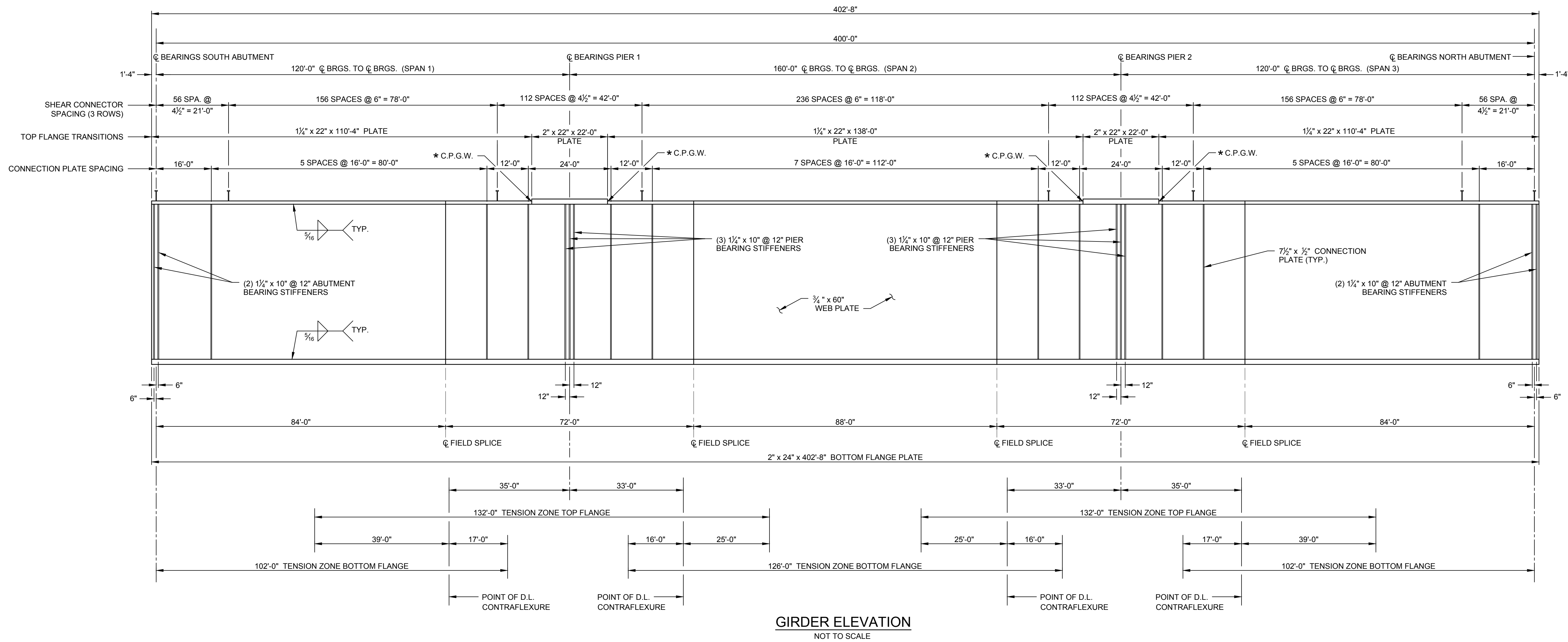
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| DRAWN | SLM |
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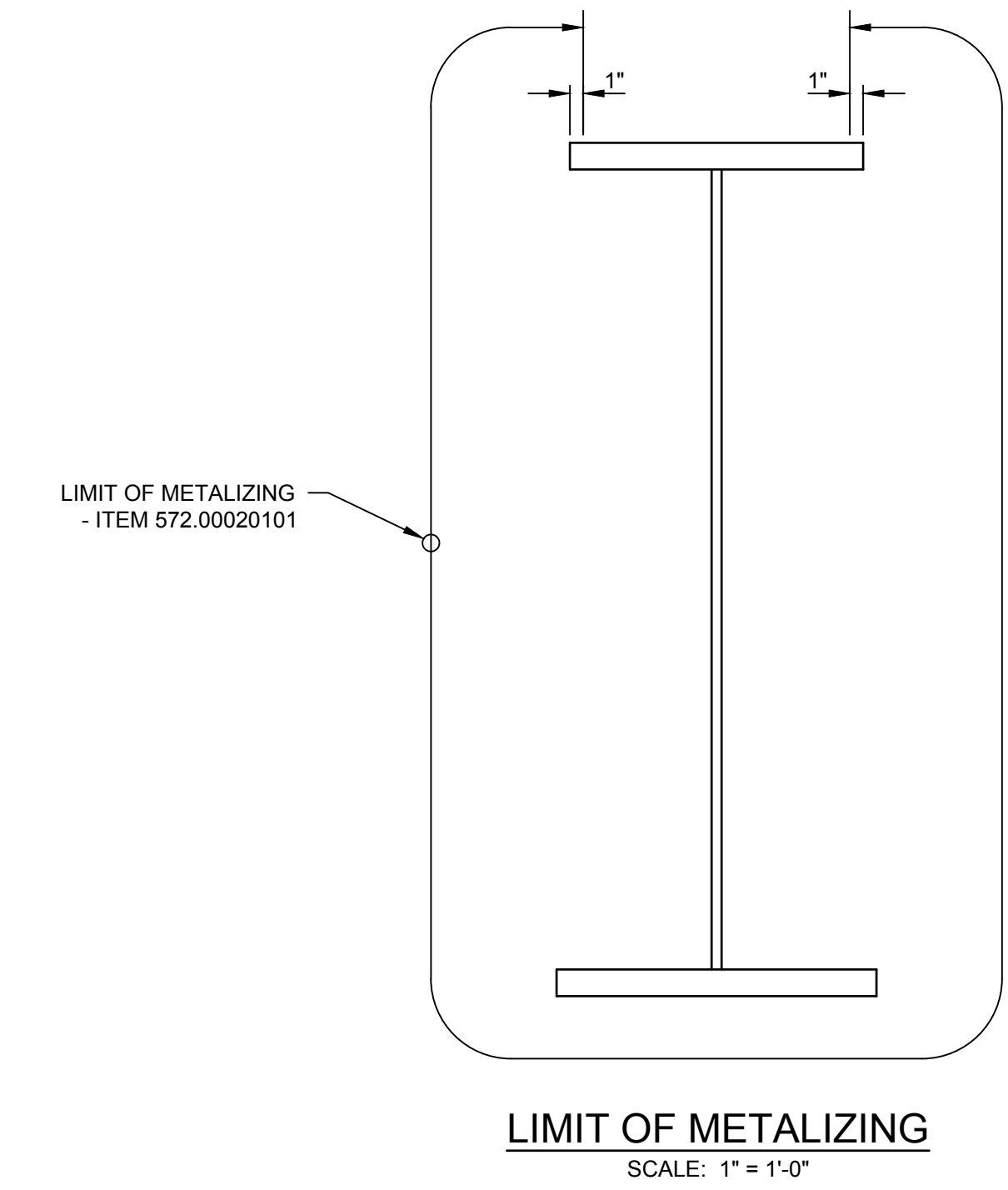
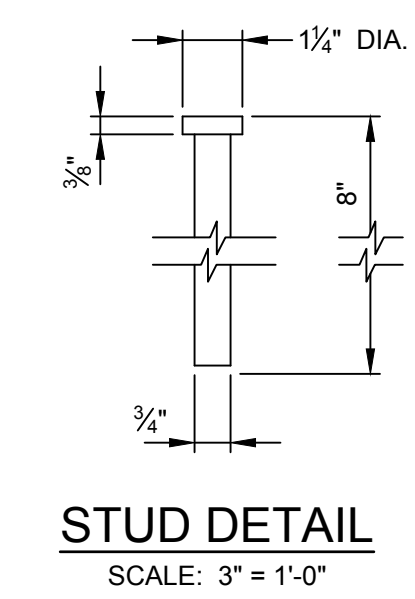
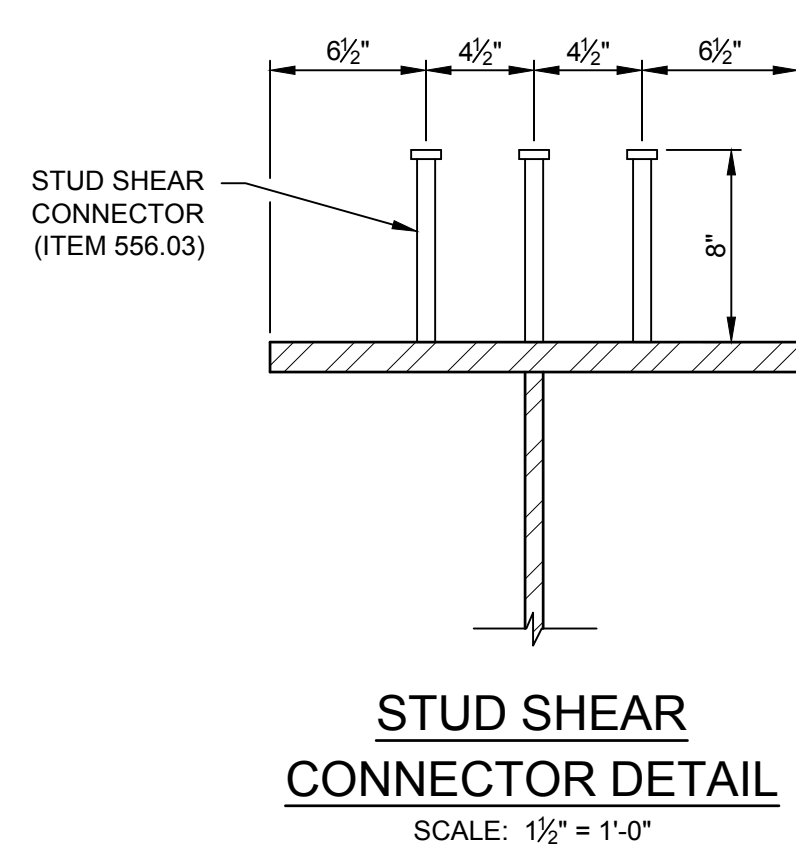
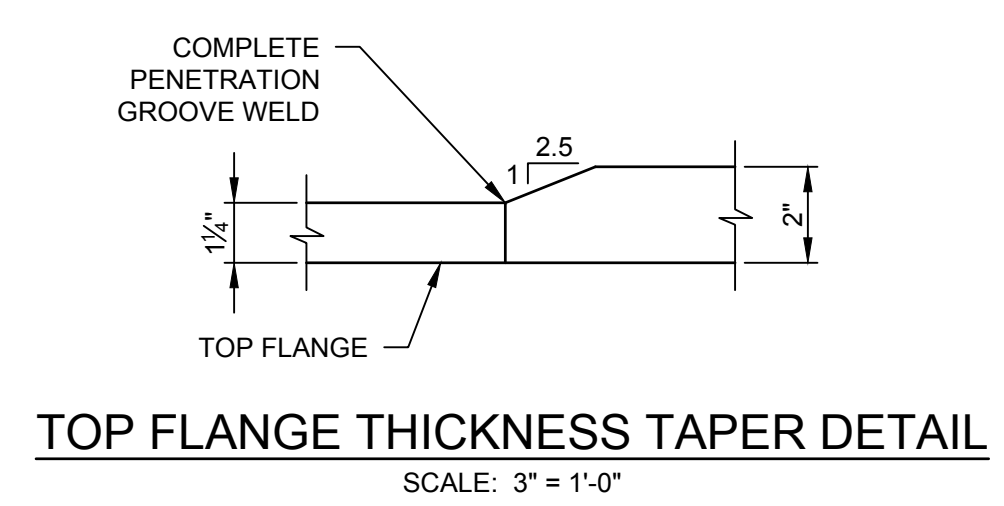
DRAWING TITLE
GIRDER ELEVATION

DRAWING NUMBER
ST-21



GIRDER ELEVATION
 NOT TO SCALE

- NOTES:**
- NO WELDING SHALL BE ALLOWED WITHIN THE TENSION ZONES SHOWN UNLESS SPECIFICALLY NOTED.
 - THERE SHALL BE NO WELDING TO METALIZED GIRDERS FOR THE ATTACHMENT OF FORMING DEVICES.
 - THE ENDS OF ALL GIRDERS AND THE BEARING STIFFENERS SHALL BE VERTICAL. ALL CONNECTION PLATES AND INTERMEDIATE STIFFENERS MAY BE PERPENDICULAR TO THE TOP FLANGES.
 - ALL GIRDERS, INCLUDING BEARING STIFFENERS AND CONNECTION PLATES, SHALL BE METALIZED FOR THEIR ENTIRE LENGTH TO THE LIMITS SHOWN IN LIMIT OF METALIZING SECTION.
 - SEE FRAMING PLAN FOR LOCATIONS OF CONNECTION PLATES.
- * C.P.G.W. = COMPLETE PENETRATION GROOVE WELD



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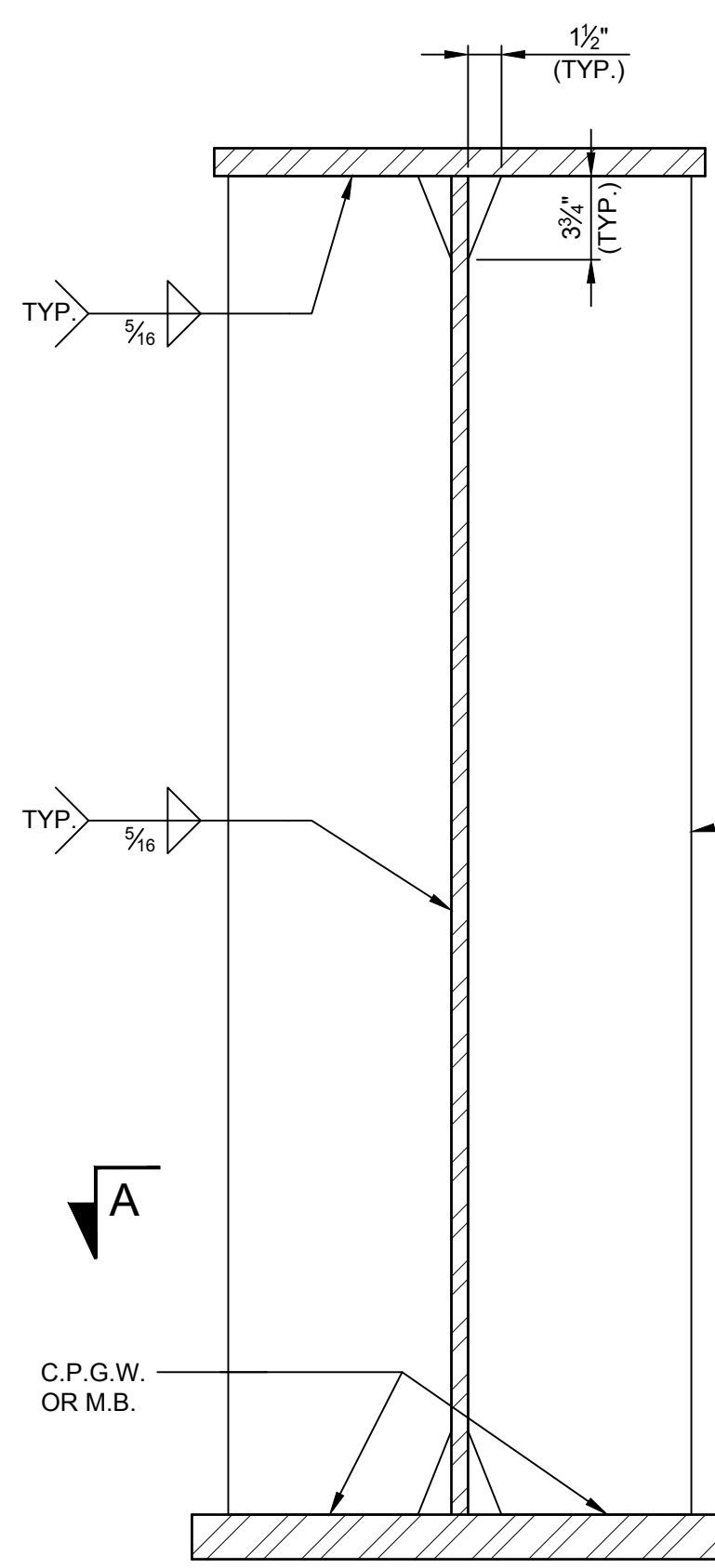
DRAWING TITLE

GIRDER AND DIAPHRAGM DETAILS

DRAWING NUMBER

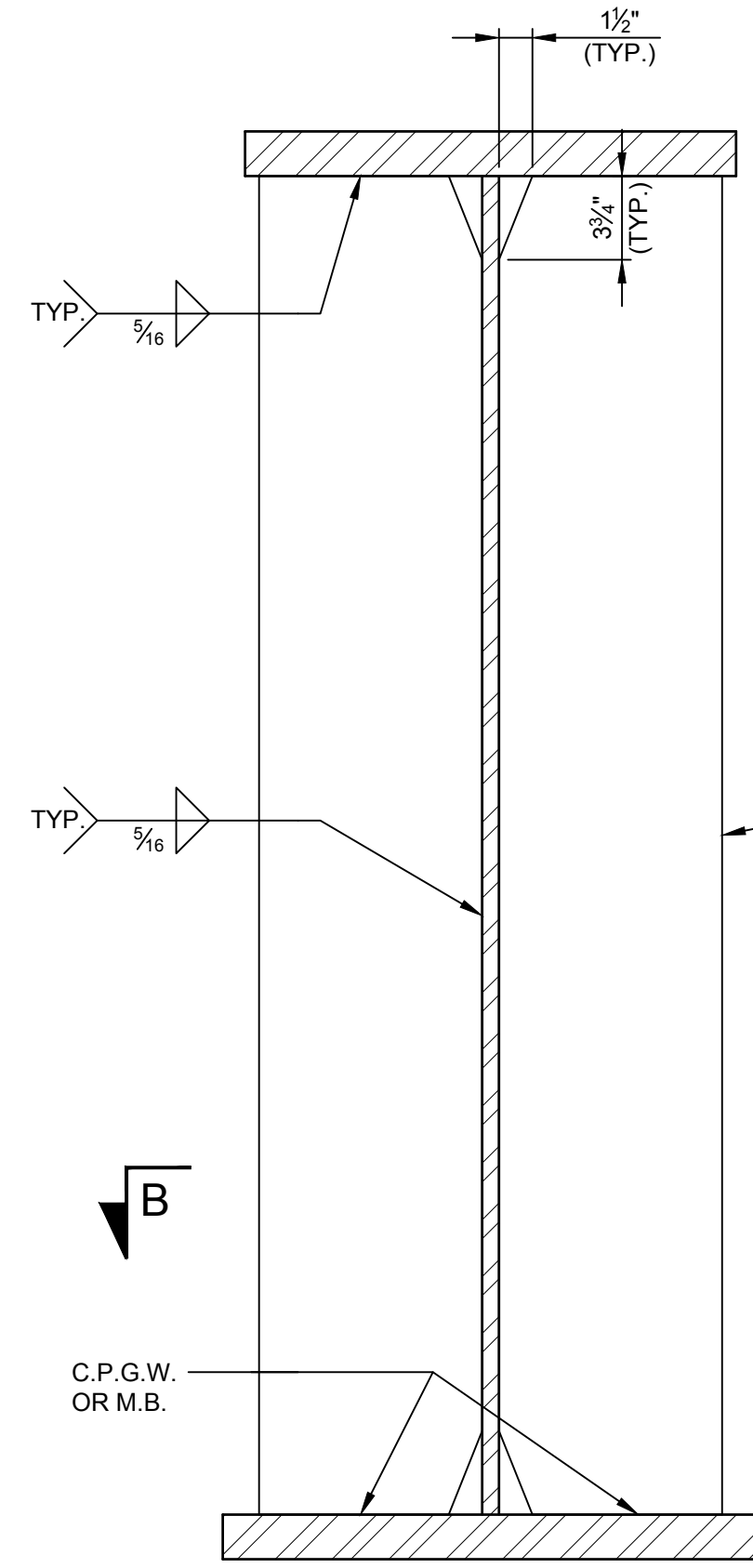
ST-22

22 OF 35



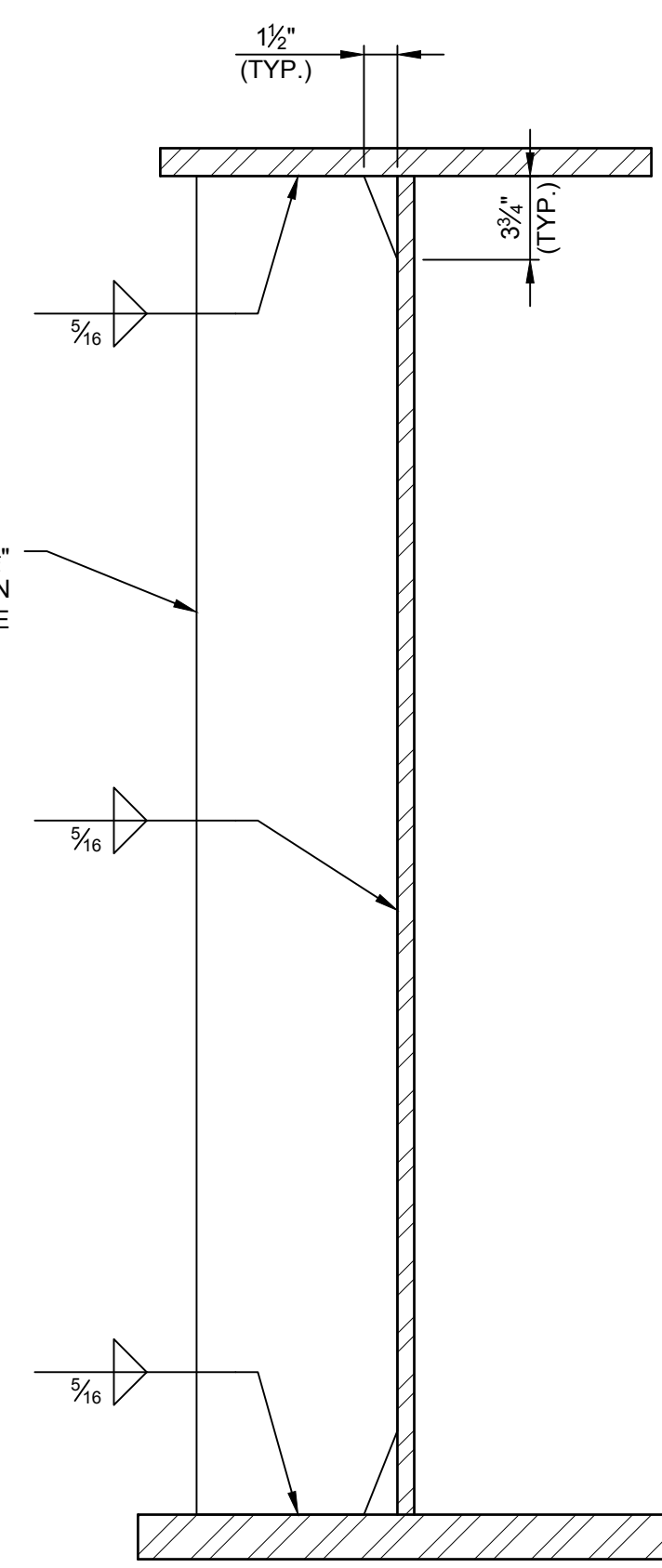
BEARING STIFFENER (AT ABUTMENT)

SCALE: 1/2" = 1'-0"



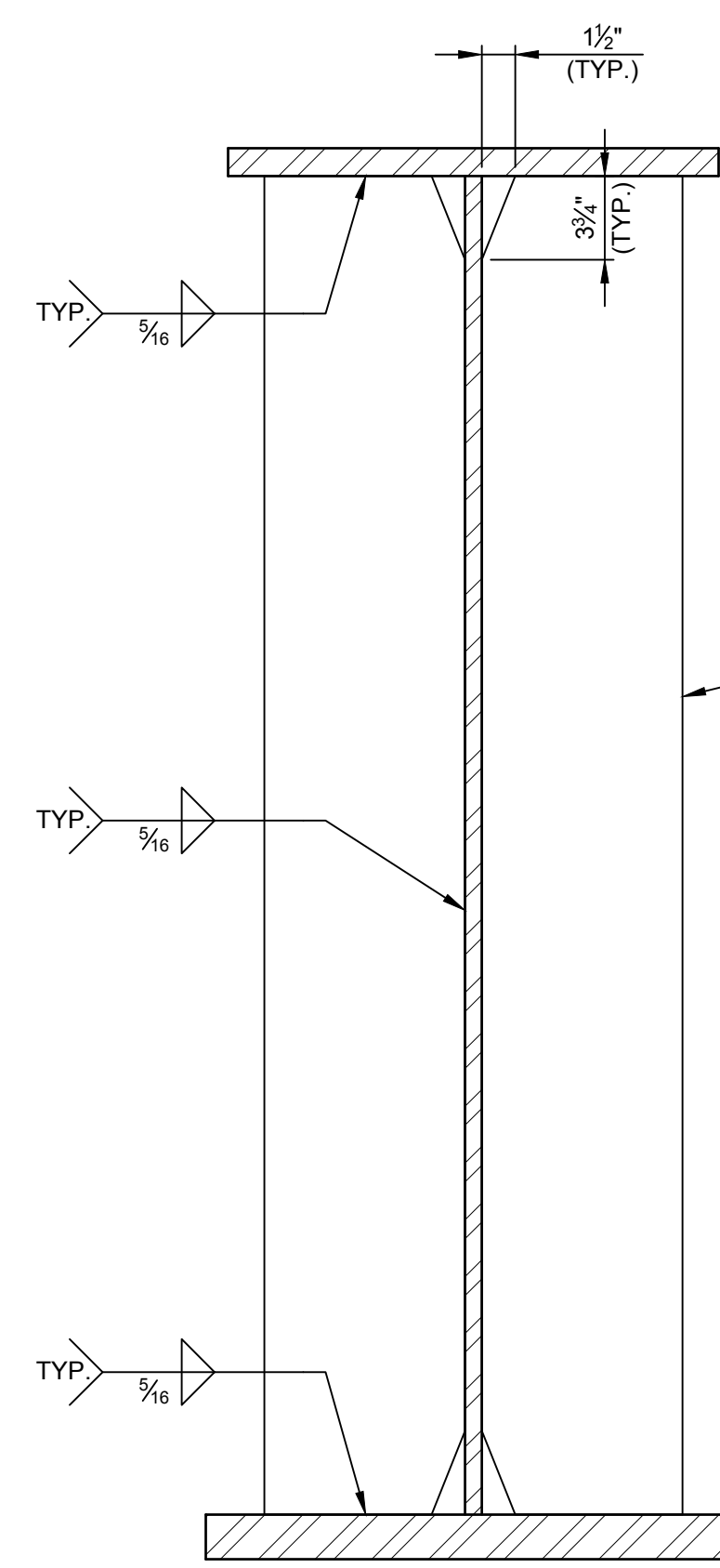
BEARING STIFFENER (AT PIER)

SCALE: 1/2" = 1'-0"



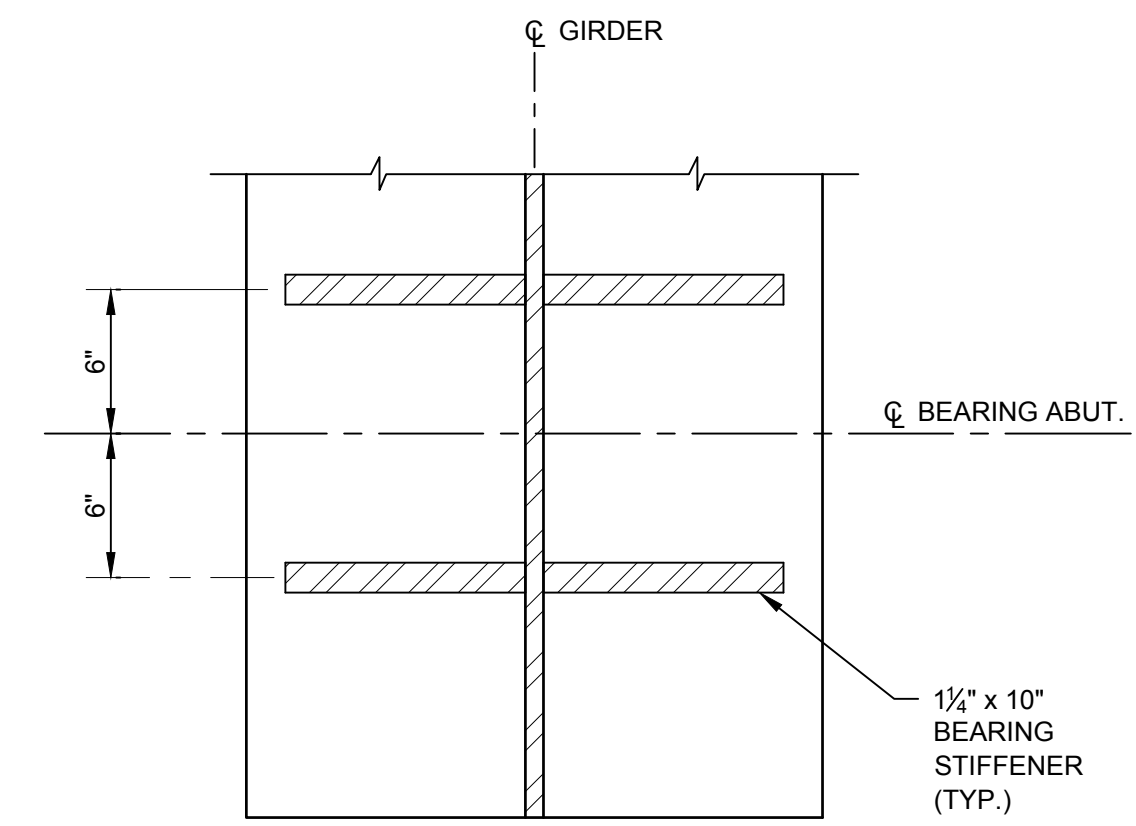
FASCIA GIRDER CONNECTION PLATE

SCALE: 1/2" = 1'-0"



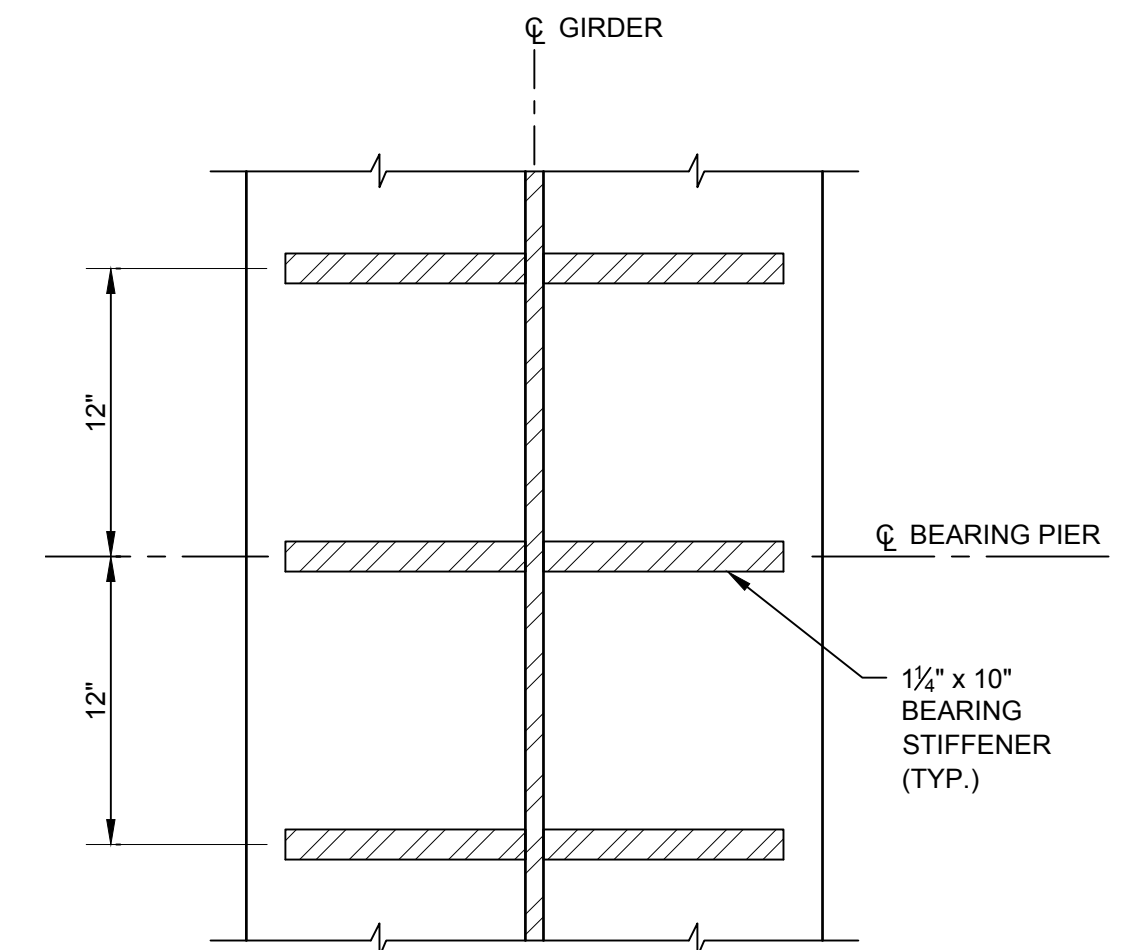
INTERIOR GIRDER CONNECTION PLATE

SCALE: 1/2" = 1'-0"



SECTION A-A

SCALE: 1/2" = 1'-0"



SECTION B-B

SCALE: 1/2" = 1'-0"

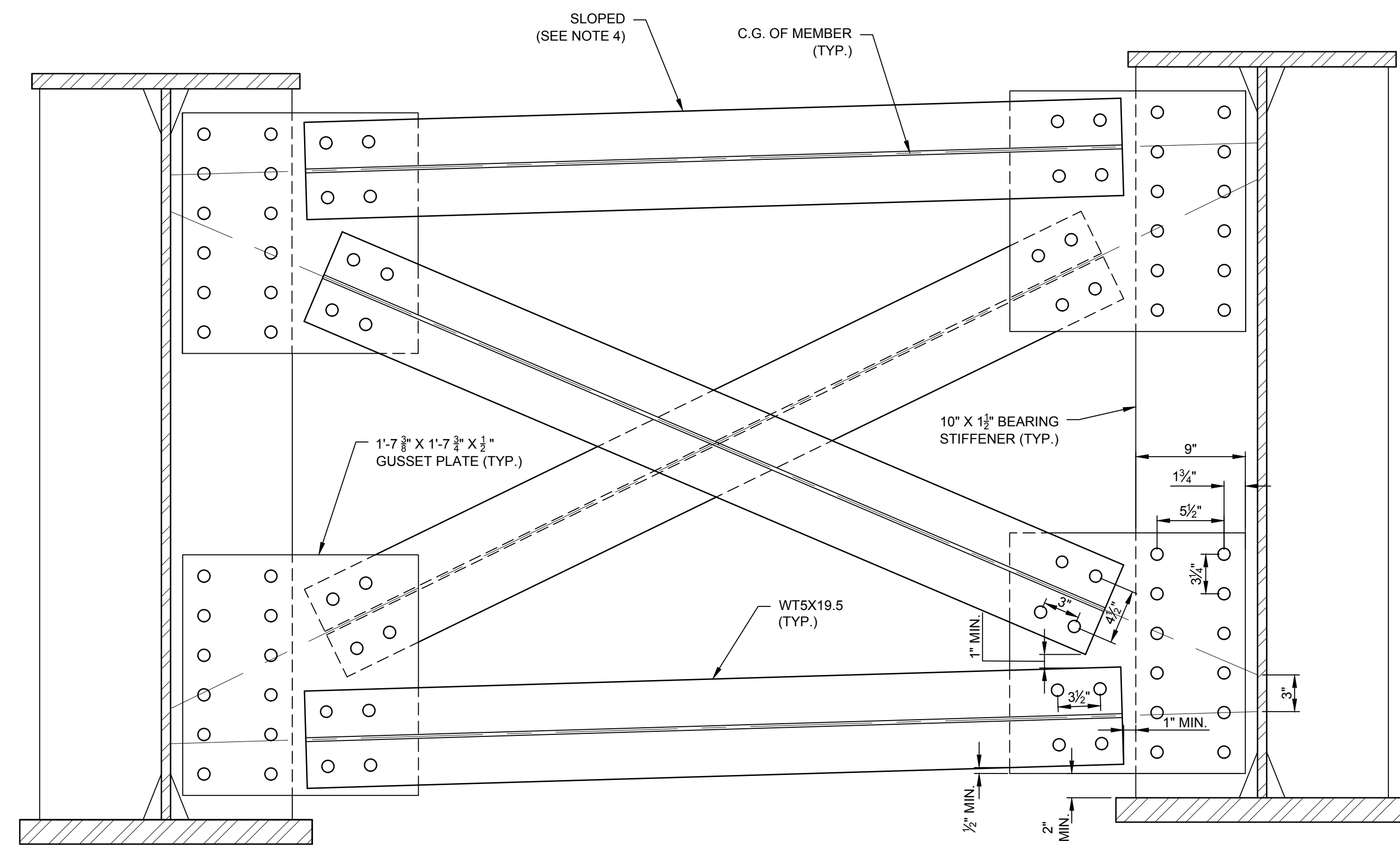
LEGEND:

C.P.G.W. = COMPLETE PENETRATION GROOVE WELD

M.B. = MILL TO BEAR THEN FILLET WELDED

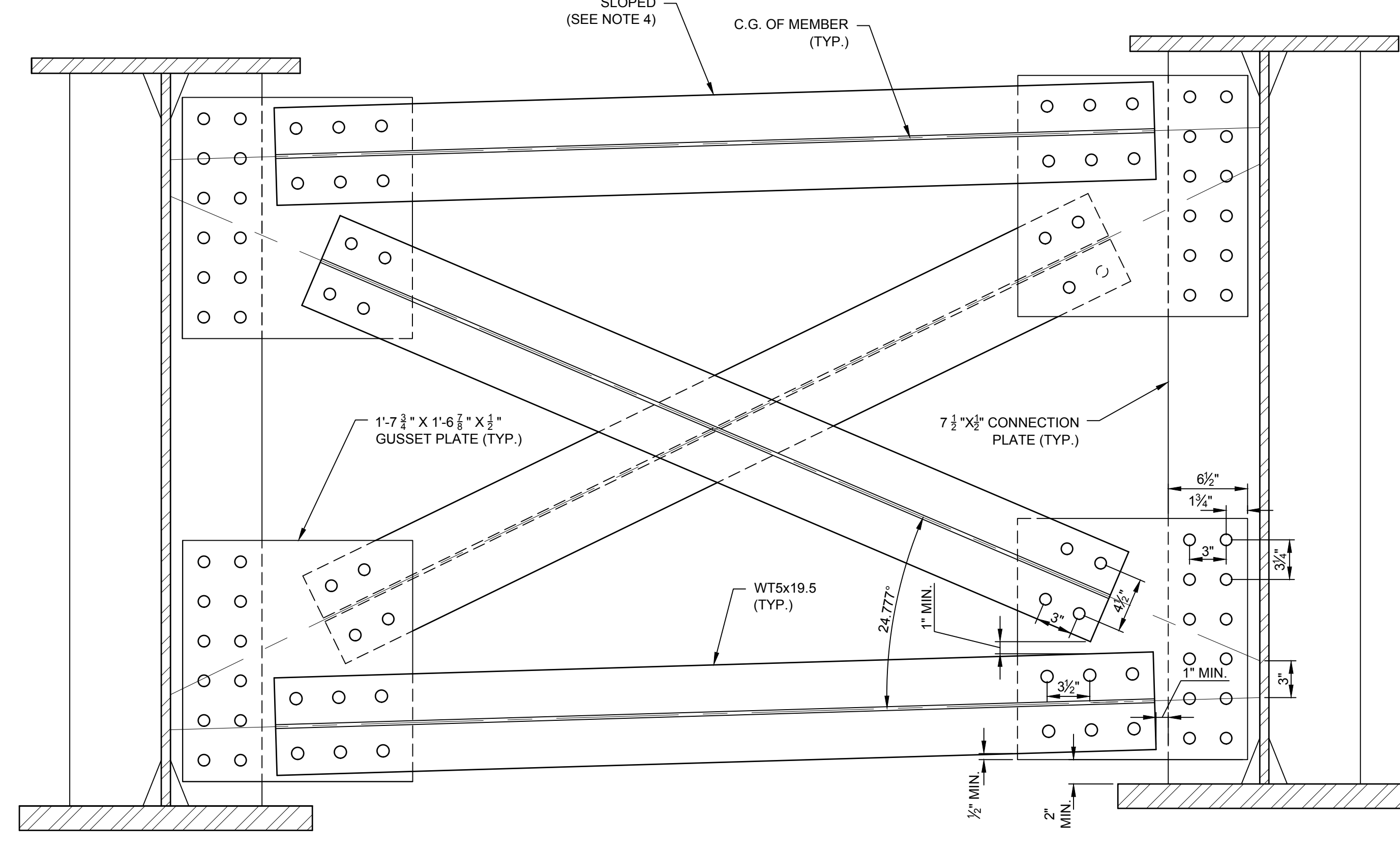
NOTES:

- THE DIAPHRAGMS, INCLUDING ALL CONNECTIONS, ARE CONSIDERED PRIMARY MEMBERS. THEY SHALL BE FABRICATED IN ACCORDANCE WITH THE NEW YORK STATE STEEL CONSTRUCTION MANUAL UNLESS NOTED OTHERWISE.
- OVERSIZED HOLES SHALL NOT BE PERMITTED FOR THE DIAPHRAGMS MEMBERS OR CONNECTIONS.
- ALL BOLTED CONNECTIONS FOR THE DIAPHRAGMS SHALL BE MADE WITH 1" DIA. ASTM F3125, GRADE A325, TYPE 1 HDG, HIGH-STRENGTH BOLTS.
- THE CONTRACTOR MAY PLACE DIAPHRAGMS ON EITHER SIDE OF BEARING STIFFENERS OR CONNECTION PLATES AS NECESSARY TO CORRECT ALIGNMENT PROVIDED THERE WILL BE NO INTERFERENCE WITH OTHER STRUCTURAL DETAILS.
- ALL STRUCTURAL STEEL CONNECTION FAYING SURFACES SHALL BE FABRICATED TO HAVE CLASS B SURFACE CONDITION (KS = 0.50 MIN.).
- FOR DIAPHRAGM CONNECTIONS, A CERTIFICATION OF CLASS B SLIP COEFFICIENT AND CREEP RESISTANCE IS REQUIRED TO BE SUBMITTED TO THE E.I.C. FOR APPROVAL. THE CERTIFICATION SHALL INCLUDE THE WRITTEN TEST RESULTS, INCLUDING THE THICKNESS RANGE REQUIRED TO MEET THE CERTIFICATION. THIS IS REQUIRED TO BE SUBMITTED A MINIMUM OF 10 WORK DAYS PRIOR TO THE SCHEDULED START OF THE STEEL ERECTION.
- ALL EDGE DISTANCES SHALL BE A MINIMUM OF 1 3/4" FROM CENTER OF BOLT.



DIAPHRAGM - TYPE D1

SCALE: 1/2" = 1'-0"



DIAPHRAGM - TYPE D2

SCALE: 1/2" = 1'-0"



McFarland Johnson
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 ALBANY, NEW YORK
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| DRAWN | SLM |
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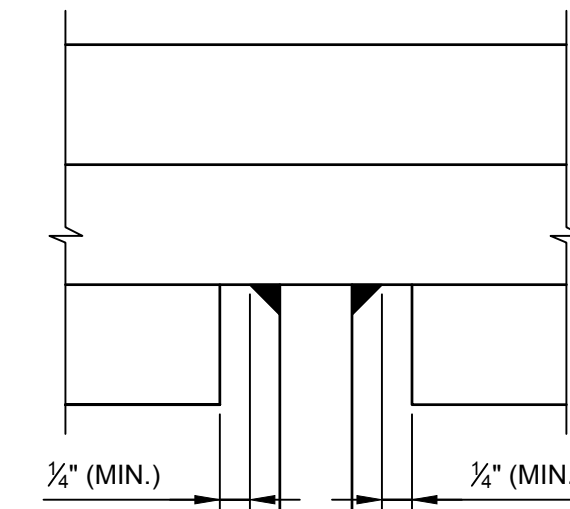
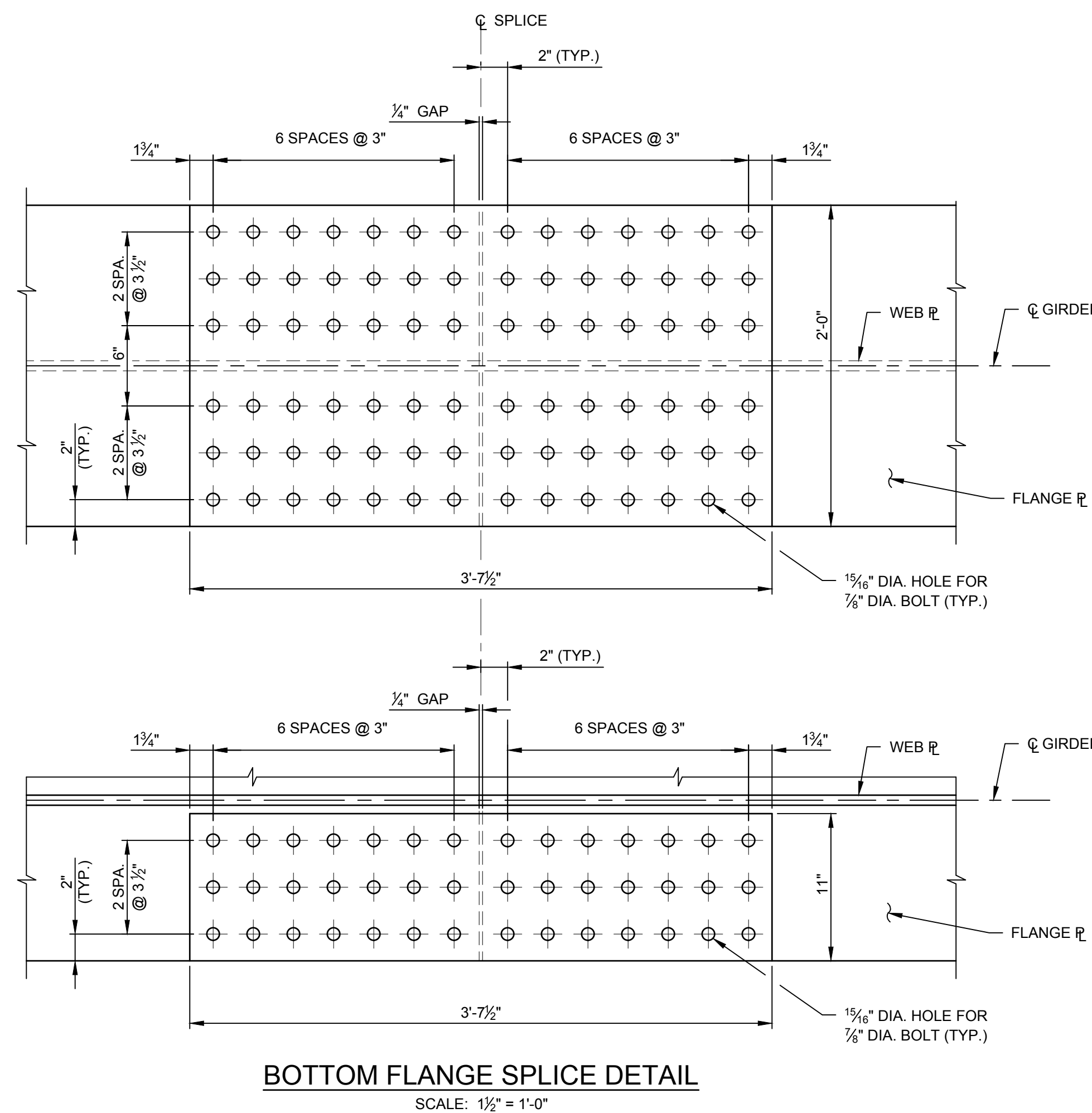
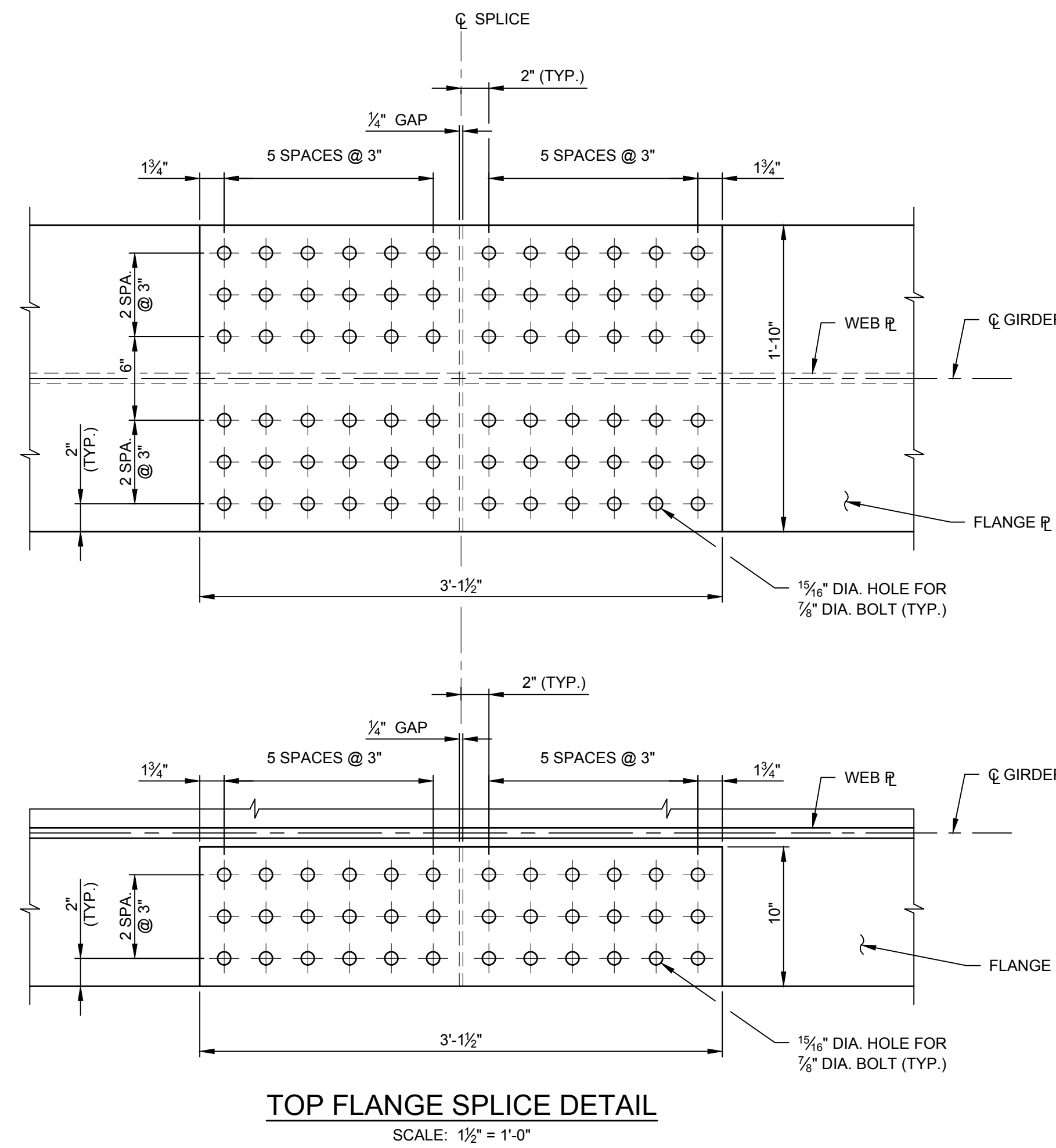
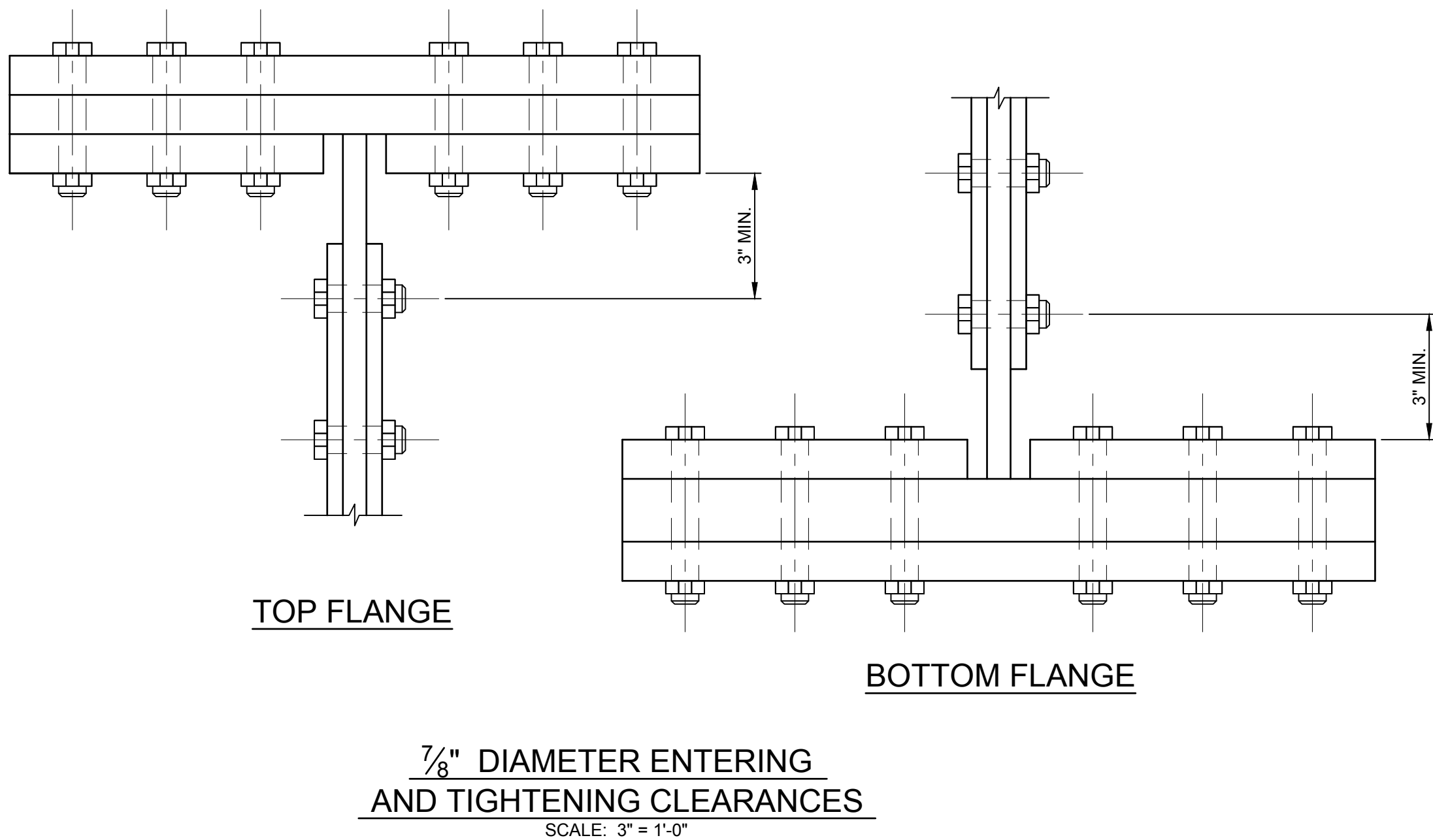
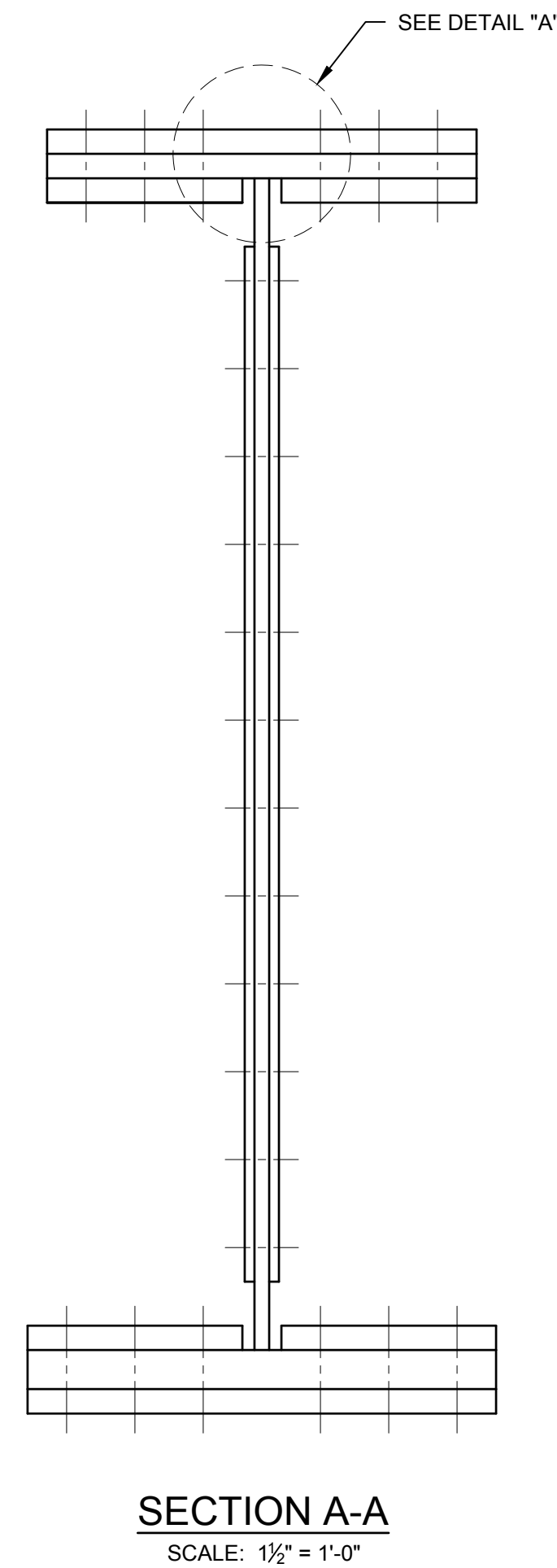
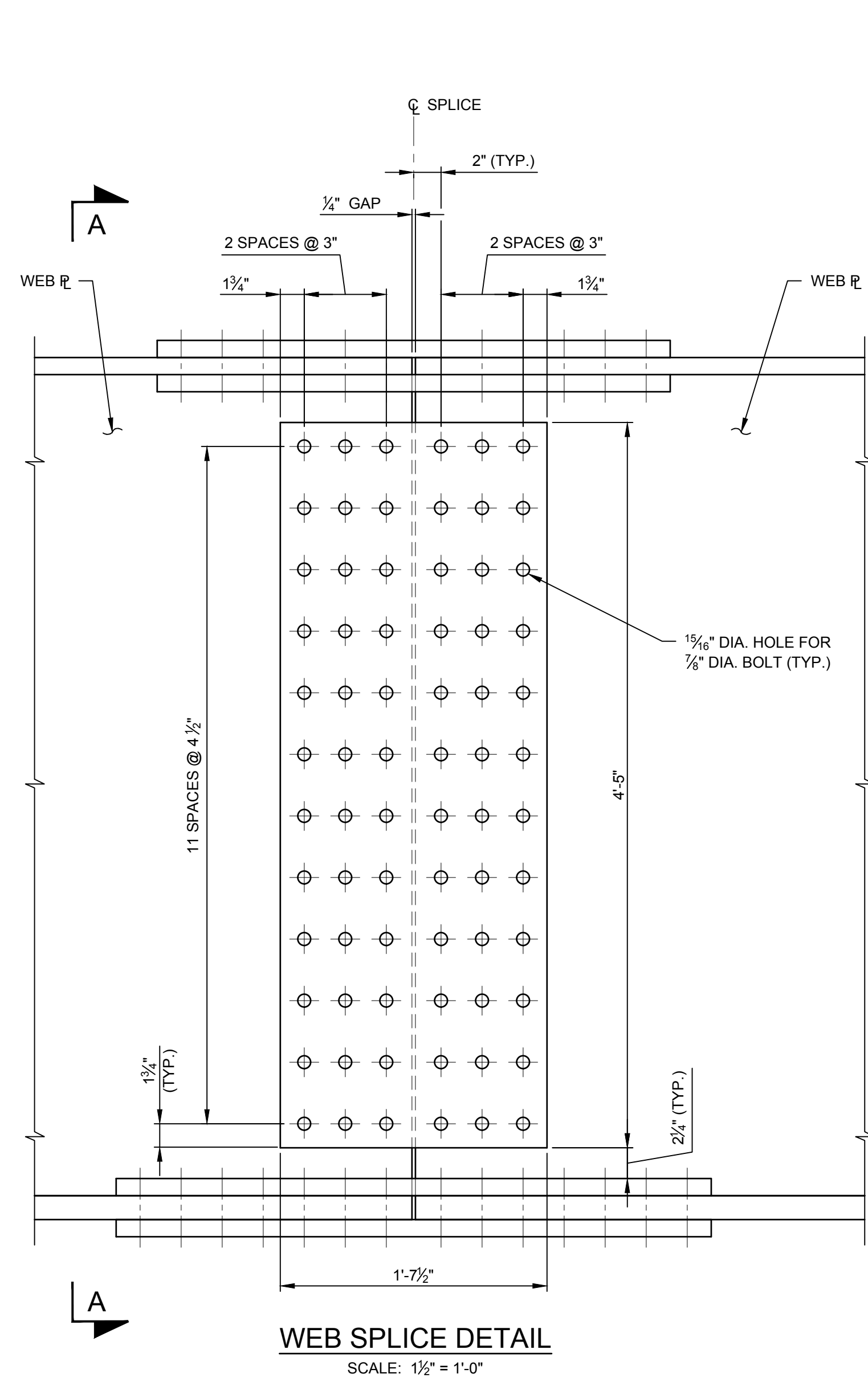
DRAWING TITLE

SPLICE DETAILS

DRAWING NUMBER

ST-23

23 OF 35



| | TOP FLANGE SPLICE | BOTTOM FLANGE SPLICE | WEB SPLICE |
|--------------|---|--|---------------------------------|
| SPLICE PLATE | 1 - PL 1'-10" x 1 1/2" x 3'-1 1/2" 2 - PL 10" x 1 1/2" x 3'-1 1/2" | 1 - PL 2'-0" x 1 1/2" x 3'-7 1/2" 2 - PL 11" x 1 1/2" x 3'-7 1/2" | 2 - PL 1'-7 1/2" x 1/2" x 4'-5" |
| NO. OF BOLTS | 36 - EACH SIDE | 42 - EACH SIDE | 36 - EACH SIDE |

GIRDER SPLICE NOTES:

- ALL COSTS FOR BOLTS, NUTS, AND WASHERS SHALL BE INCLUDED IN THE PRICE BID FOR STRUCTURAL STEEL.
- SPLICE DESIGNS ARE BASED ON THE LOCATIONS INDICATED. THE CONTRACTOR HAS THE OPTION OF USING ALTERNATE SPLICE LOCATIONS. HOWEVER, RELOCATION REQUESTS MUST BE SUBMITTED TO THE E.I.C. FOR APPROVAL. ALL REVISED SPLICE LOCATION SUBMITTALS SHALL BE SEALED BY A LICENSED PROFESSIONAL ENGINEER. NO ADDITIONAL COMPENSATION WILL BE MADE TO THE CONTRACTOR FOR RELOCATING THE SPLICE. FABRICATION SHALL CONFORM TO THE CURRENT NEW YORK STATE STEEL CONSTRUCTION MANUAL.
- ALL SPLICE PLATES SHALL CONSIST OF THE SAME GRADE OF STEEL AS THE MEMBER(S) BEING SPLICED AND THEIR COST INCLUDED IN THE PRICE BID FOR THE STRUCTURAL STEEL.
- SPLICE PLATES SHALL HAVE OXYGEN CUT EDGES, AS PER SECTION 609 OF THE NYS STEEL CONSTRUCTION MANUAL.
- SUBJECT TO E.I.C. APPROVAL, THE CONTRACTOR CAN PROPOSE THE USE OF A WELDED SPLICE. HOWEVER, ALL COSTS ASSOCIATED WITH THIS CHANGE WILL BE AT THE CONTRACTOR'S EXPENSE. ADDITIONALLY, A DETAILED WELDING PROCEDURE, AS PER NYS STEEL CONSTRUCTION MANUAL 203.5 (B), OF THE NYS STEEL CONSTRUCTION MANUAL SHALL BE SUBMITTED TO THE E.I.C. FOR APPROVAL.
- BOLT LOCATIONS SHOWN MAY HAVE TO BE MOVED LATERALLY ON THEIR FLANGE TO MEET SEALING REQUIREMENTS AS DESCRIBED IN SECTION 203.14 OF THE NYS STEEL CONSTRUCTION MANUAL.
- ALL BOLTS SHALL BE 7/8" DIA. ASTM F3125, GRADE A325, TYPE 1 (HOT DIPPED GALVANIZED). NUTS SHALL BE A563, GRADE DH (HOT DIPPED GALVANIZED), OR A194, GRADE 2H (HOT DIPPED GALVANIZED). WASHERS SHALL BE F436 (HOT DIPPED GALVANIZED).
- ALL EXPOSED STEEL SURFACES ON THE SPLICE PLATES SHALL BE METALIZED (ITEM 572.00020101).
- ALL STRUCTURAL STEEL CONNECTION FAYING SURFACES SHALL BE FABRICATED TO HAVE CLASS B SURFACE CONDITION (Ks = 0.50 MIN.).



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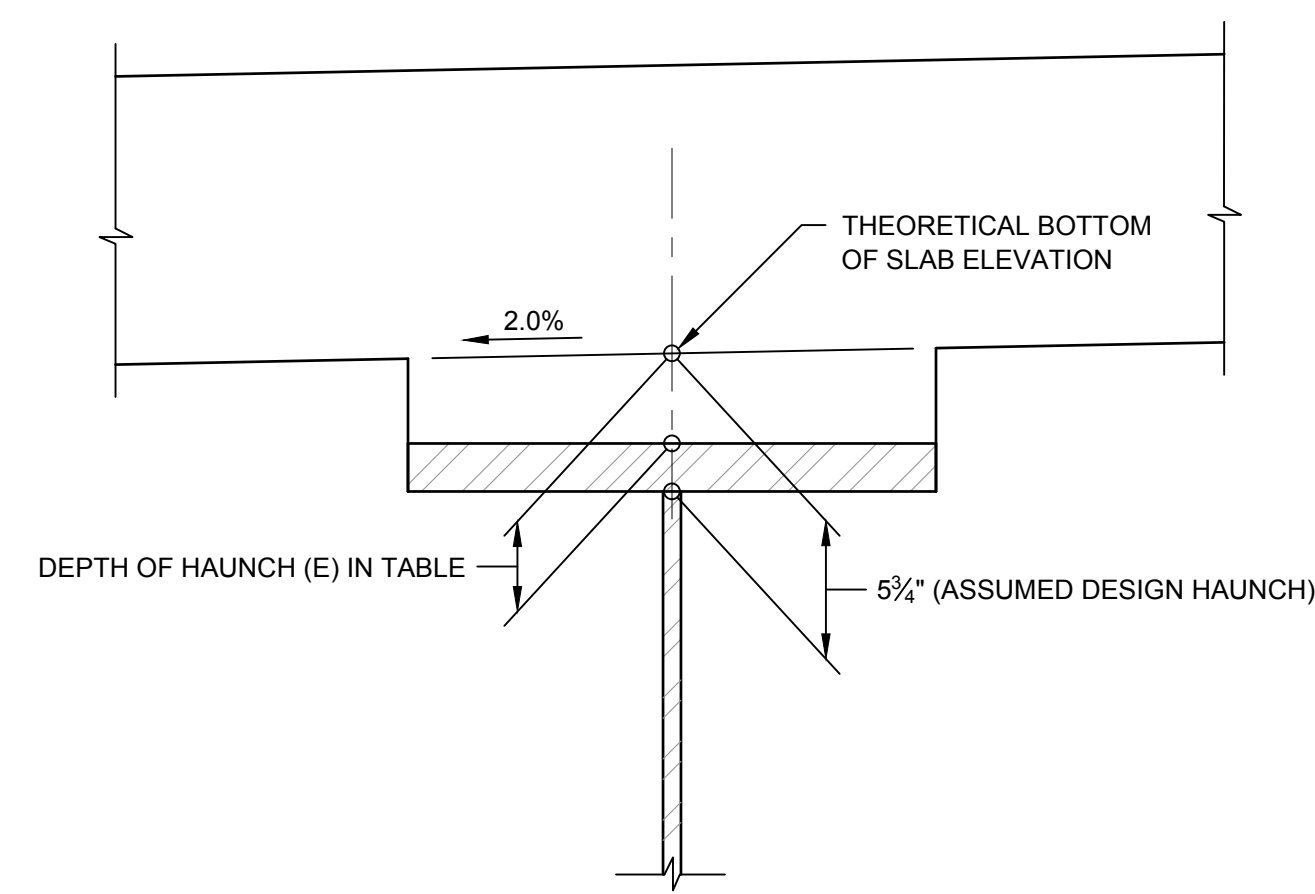
PROJECT MILESTONE

GMP PLANS

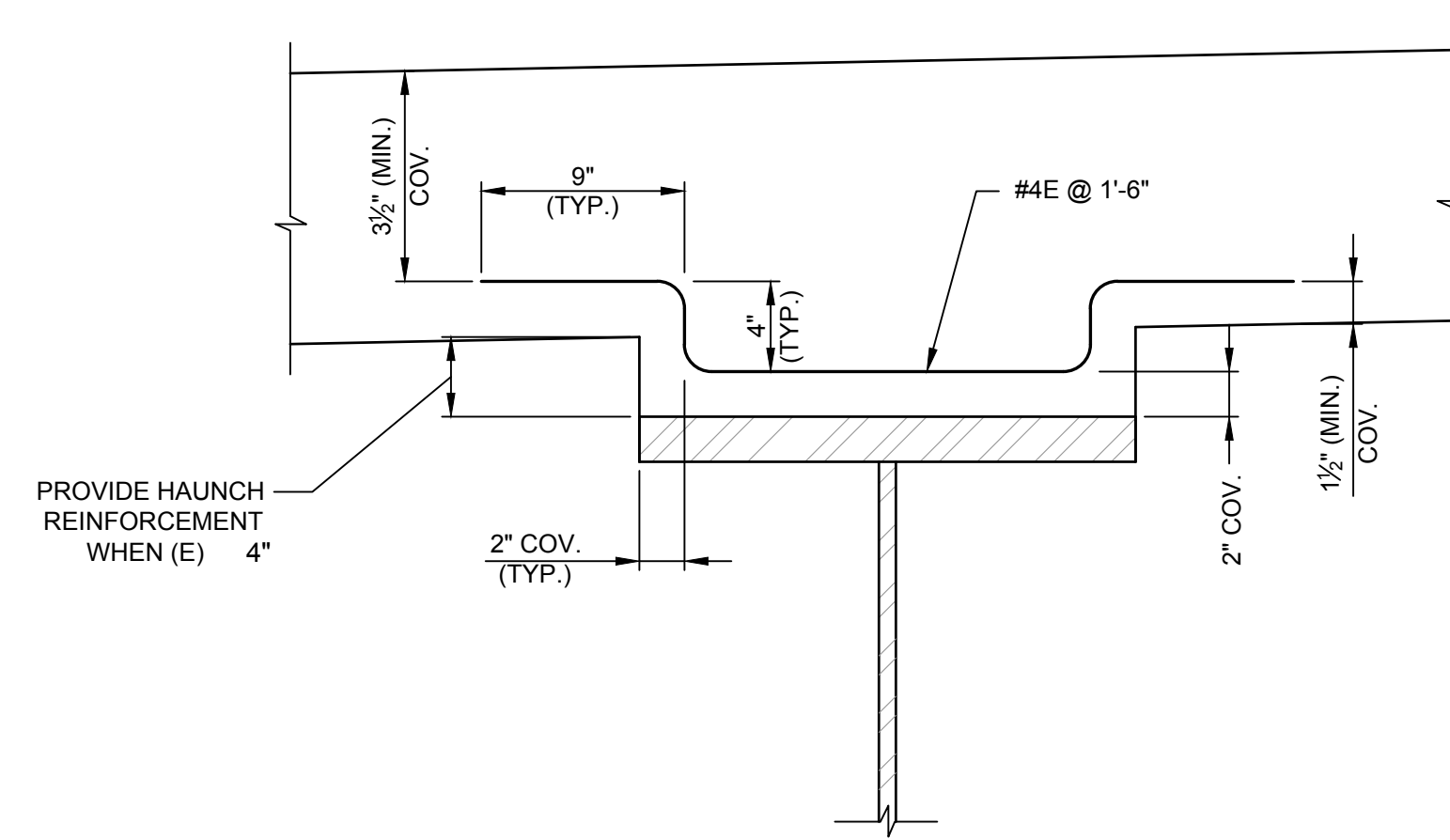
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| HAUNCH TABLE (FT) | | CL. OF BRGS. SOUTH ABUT. | 0.1 L1 | 0.2 L1 | 0.3 L1 | 0.4 L1 | 0.5 L1 | 0.6 L1 | 0.7 L1 | 0.8 L1 | 0.9 L1 | CL. OF BRGS. PIER 1 | 0.1 L2 | 0.2 L2 | 0.3 L2 | 0.4 L2 | 0.5 L2 | 0.6 L2 | 0.7 L2 | 0.8 L2 | 0.9 L2 | CL. OF BRGS. PIER 2 | 0.1 L3 | 0.2 L3 | 0.3 L3 | 0.4 L3 | 0.5 L3 | 0.6 L3 | 0.7 L3 | 0.8 L3 | 0.9 L3 | CL. OF BRGS. NORTH ABUT. | | |
|-------------------|---------------------------------------|--------------------------|--------|--------|--------|--------|--------|--------|--------|--------|--------|---------------------|--------|--------|--------|--------|--------|--------|--------|--------|--------|---------------------|--------|--------|--------|--------|--------|--------|--------|--------|--------|--------------------------|--|--|
| GIRDER 1 | (A) REQ'D BOTTOM OF SLAB ELEVATIONS | 22.31 | 22.78 | 23.23 | 23.64 | 24.03 | 24.38 | 24.71 | 25.00 | 25.27 | 25.51 | 25.72 | 25.95 | 26.14 | 26.27 | 26.34 | 26.37 | 26.34 | 26.27 | 26.14 | 25.95 | 25.72 | 25.51 | 25.27 | 25.00 | 24.71 | 24.38 | 24.03 | 23.64 | 23.23 | 22.78 | 22.31 | | |
| | (B) TOP OF STEEL EL. (FIELD MEASURE) | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| | (C) = (A) - (B) | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| | (D) CONCRETE + S.D.L. DEFLECTION | 0.00 | 0.03 | 0.06 | 0.08 | 0.09 | 0.08 | 0.07 | 0.04 | 0.02 | 0.00 | 0.00 | 0.03 | 0.08 | 0.13 | 0.17 | 0.18 | 0.17 | 0.13 | 0.08 | 0.03 | 0.00 | 0.00 | 0.02 | 0.04 | 0.07 | 0.08 | 0.09 | 0.08 | 0.06 | 0.03 | 0.00 | | |
| | (E) DEPTH OF HAUNCH REQ'D = (C) + (D) | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| GIRDER 2 | (A) REQ'D BOTTOM OF SLAB ELEVATIONS | 22.46 | 22.93 | 23.38 | 23.79 | 24.18 | 24.53 | 24.86 | 25.15 | 25.42 | 25.66 | 25.87 | 26.10 | 26.29 | 26.42 | 26.49 | 26.52 | 26.49 | 26.42 | 26.29 | 26.10 | 25.87 | 25.66 | 25.42 | 25.15 | 24.86 | 24.53 | 24.18 | 23.79 | 23.38 | 22.93 | 22.46 | | |
| | (B) TOP OF STEEL EL. (FIELD MEASURE) | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| | (C) = (A) - (B) | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| | (D) CONCRETE + S.D.L. DEFLECTION | 0.00 | 0.03 | 0.06 | 0.08 | 0.09 | 0.08 | 0.06 | 0.04 | 0.02 | 0.00 | 0.00 | 0.03 | 0.08 | 0.13 | 0.17 | 0.18 | 0.17 | 0.13 | 0.08 | 0.03 | 0.00 | 0.00 | 0.02 | 0.04 | 0.06 | 0.08 | 0.09 | 0.08 | 0.06 | 0.03 | 0.00 | | |
| | (E) DEPTH OF HAUNCH REQ'D = (C) + (D) | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| GIRDER 3 | (A) REQ'D BOTTOM OF SLAB ELEVATIONS | 22.61 | 23.08 | 23.53 | 23.94 | 24.33 | 24.68 | 25.01 | 25.30 | 25.57 | 25.81 | 26.02 | 26.25 | 26.44 | 26.57 | 26.64 | 26.67 | 26.64 | 26.57 | 26.44 | 26.25 | 26.02 | 25.81 | 25.57 | 25.30 | 25.01 | 24.68 | 24.33 | 23.94 | 23.53 | 23.08 | 22.61 | | |
| | (B) TOP OF STEEL EL. (FIELD MEASURE) | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| | (C) = (A) - (B) | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| | (D) CONCRETE + S.D.L. DEFLECTION | 0.00 | 0.03 | 0.06 | 0.08 | 0.09 | 0.08 | 0.06 | 0.04 | 0.02 | 0.00 | 0.00 | 0.03 | 0.08 | 0.13 | 0.17 | 0.18 | 0.17 | 0.13 | 0.08 | 0.03 | 0.00 | 0.00 | 0.02 | 0.04 | 0.06 | 0.08 | 0.09 | 0.08 | 0.06 | 0.03 | 0.00 | | |
| | (E) DEPTH OF HAUNCH REQ'D = (C) + (D) | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| GIRDER 4 | (A) REQ'D BOTTOM OF SLAB ELEVATIONS | 22.61 | 23.08 | 23.53 | 23.94 | 24.33 | 24.68 | 25.01 | 25.30 | 25.57 | 25.81 | 26.02 | 26.25 | 26.44 | 26.57 | 26.64 | 26.67 | 26.64 | 26.57 | 26.44 | 26.25 | 26.02 | 25.81 | 25.57 | 25.30 | 25.01 | 24.68 | 24.33 | 23.94 | 23.53 | 23.08 | 22.61 | | |
| | (B) TOP OF STEEL EL. (FIELD MEASURE) | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| | (C) = (A) - (B) | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| | (D) CONCRETE + S.D.L. DEFLECTION | 0.00 | 0.03 | 0.06 | 0.08 | 0.09 | 0.08 | 0.06 | 0.04 | 0.02 | 0.00 | 0.00 | 0.03 | 0.08 | 0.13 | 0.17 | 0.18 | 0.17 | 0.13 | 0.08 | 0.03 | 0.00 | 0.00 | 0.02 | 0.04 | 0.06 | 0.08 | 0.09 | 0.08 | 0.06 | 0.03 | 0.00 | | |
| | (E) DEPTH OF HAUNCH REQ'D = (C) + (D) | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| GIRDER 5 | (A) REQ'D BOTTOM OF SLAB ELEVATIONS | 22.46 | 22.93 | 23.38 | 23.79 | 24.18 | 24.53 | 24.86 | 25.15 | 25.42 | 25.66 | 25.87 | 26.10 | 26.29 | 26.42 | 26.49 | 26.52 | 26.49 | 26.42 | 26.29 | 26.10 | 25.87 | 25.66 | 25.42 | 25.15 | 24.86 | 24.53 | 24.18 | 23.79 | 23.38 | 22.93 | 22.46 | | |
| | (B) TOP OF STEEL EL. (FIELD MEASURE) | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| | (C) = (A) - (B) | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| | (D) CONCRETE + S.D.L. DEFLECTION | 0.00 | 0.03 | 0.06 | 0.08 | 0.09 | 0.08 | 0.06 | 0.04 | 0.02 | 0.00 | 0.00 | 0.03 | 0.08 | 0.13 | 0.17 | 0.18 | 0.17 | 0.13 | 0.08 | 0.03 | 0.00 | 0.00 | 0.02 | 0.04 | 0.06 | 0.08 | 0.09 | 0.08 | 0.06 | 0.03 | 0.00 | | |
| | (E) DEPTH OF HAUNCH REQ'D = (C) + (D) | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| GIRDER 6 | (A) REQ'D BOTTOM OF SLAB ELEVATIONS | 22.31 | 22.78 | 23.23 | 23.64 | 24.03 | 24.38 | 24.71 | 25.00 | 25.27 | 25.51 | 25.72 | 25.95 | 26.14 | 26.27 | 26.34 | 26.37 | 26.34 | 26.27 | 26.14 | 25.95 | 25.72 | 25.51 | 25.27 | 25.00 | 24.71 | 24.38 | 24.03 | 23.64 | 23.23 | 22.78 | 22.31 | | |
| | (B) TOP OF STEEL EL. (FIELD MEASURE) | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| | (C) = (A) - (B) | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| | (D) CONCRETE + S.D.L. DEFLECTION | 0.00 | 0.03 | 0.06 | 0.08 | 0.09 | 0.08 | 0.07 | 0.04 | 0.02 | 0.00 | 0.00 | 0.03 | 0.08 | 0.13 | 0.17 | 0.18 | 0.17 | 0.13 | 0.08 | 0.03 | 0.00 | 0.00 | 0.02 | 0.04 | 0.07 | 0.08 | 0.09 | 0.08 | 0.06 | 0.03 | 0.00 | | |
| | (E) DEPTH OF HAUNCH REQ'D = (C) + (D) | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |

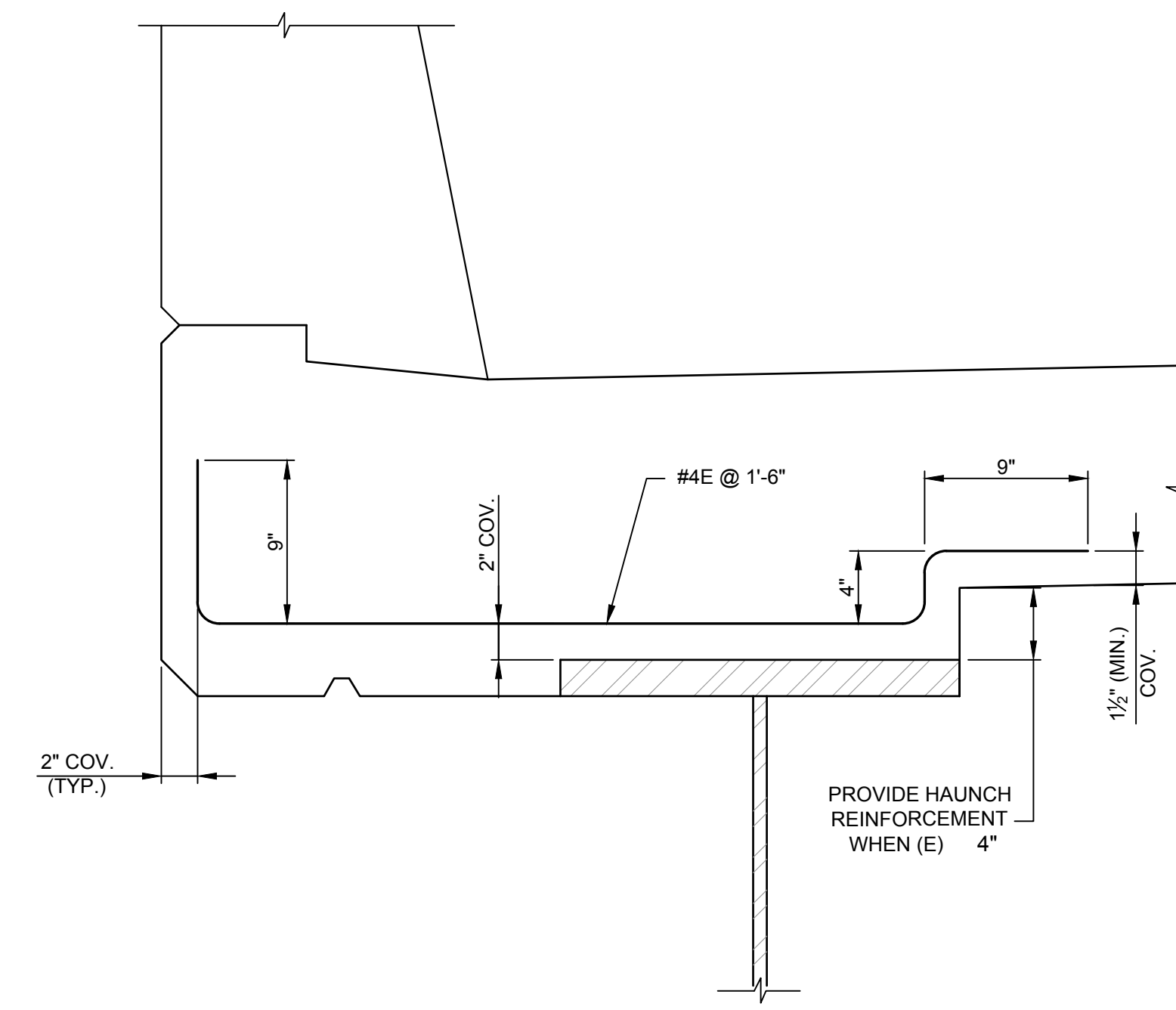
HAUNCH TABLE



GIRDER HAUNCH DETAIL
SCALE: 1/2" = 1'-0"



REINFORCED GIRDER HAUNCH DETAIL - INTERIOR GIRDER
SCALE: 1/2" = 1'-0"



REINFORCED GIRDER HAUNCH DETAIL - EXTERIOR GIRDER
SCALE: 1/2" = 1'-0"

NOTE:

1. THE CONTRACTOR SHALL PROVIDE THE ENGINEER WITH THE COMPLETED HAUNCH TABLE PRIOR TO SETTING THE BOTTOM FORMWORK OF THE DECK.

CLIENT: ALBANY PORT DISTRICT COMMISSION
 ALBANY, NEW YORK
 PROJECT: PORT OF ALBANY SITE INFRASTRUCTURE IMPROVEMENTS

| | |
|----------|----------|
| DRAWN | SLM |
| DESIGNED | DRW |
| CHECKED | JLR |
| SCALE | AS SHOWN |
| DATE | 6/8/2022 |
| PROJECT | 18641.00 |



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DRAWING TITLE

HAUNCH TABLE AND DECK DETAILS

DRAWING NUMBER

ST-25



McFarland Johnson
 60 RAILROAD PLACE
 SUITE 402
 SARATOGA SPRINGS, NEW YORK 12866
 P: 518-580-9380 F: 518-580-9383
 SaratogaROM@mjinc.com

PROJECT MILESTONE
GMP PLANS

| NO. | DATE | DESCRIPTION |
|-----|----------|-----------------------|
| 1 | 06/08/22 | GMP BID SET |
| 2 | 10/28/22 | GMP BID SET REVISIONS |

CLIENT:
ALBANY PORT DISTRICT COMMISSION
 ALBANY, NEW YORK

PROJECT:
PORT OF ALBANY SITE INFRASTRUCTURE IMPROVEMENTS

| | |
|----------|----------|
| DRAWN | SLM |
| DESIGNED | DRW |
| CHECKED | JLR |
| SCALE | AS SHOWN |
| DATE | 6/8/2022 |
| PROJECT | 18641.00 |



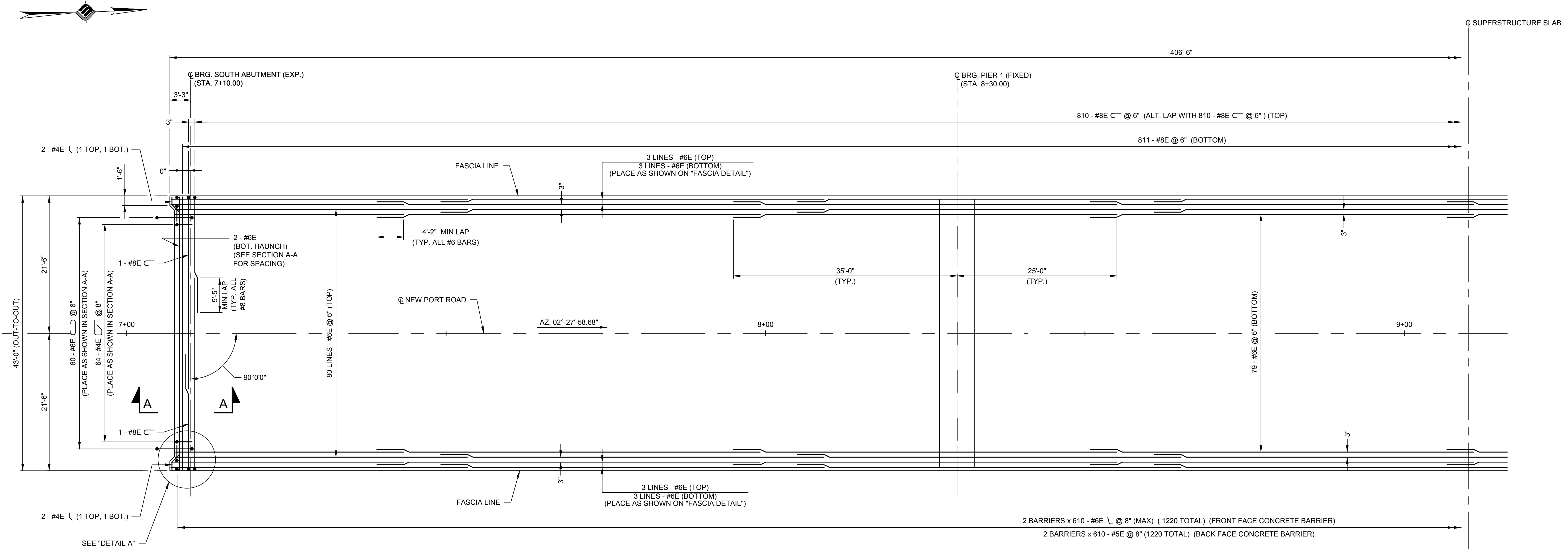
IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECT DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.

DRAWING TITLE
SUPERSTRUCTURE SLAB (1 OF 2)

DRAWING NUMBER

ST-26

26 OF 35

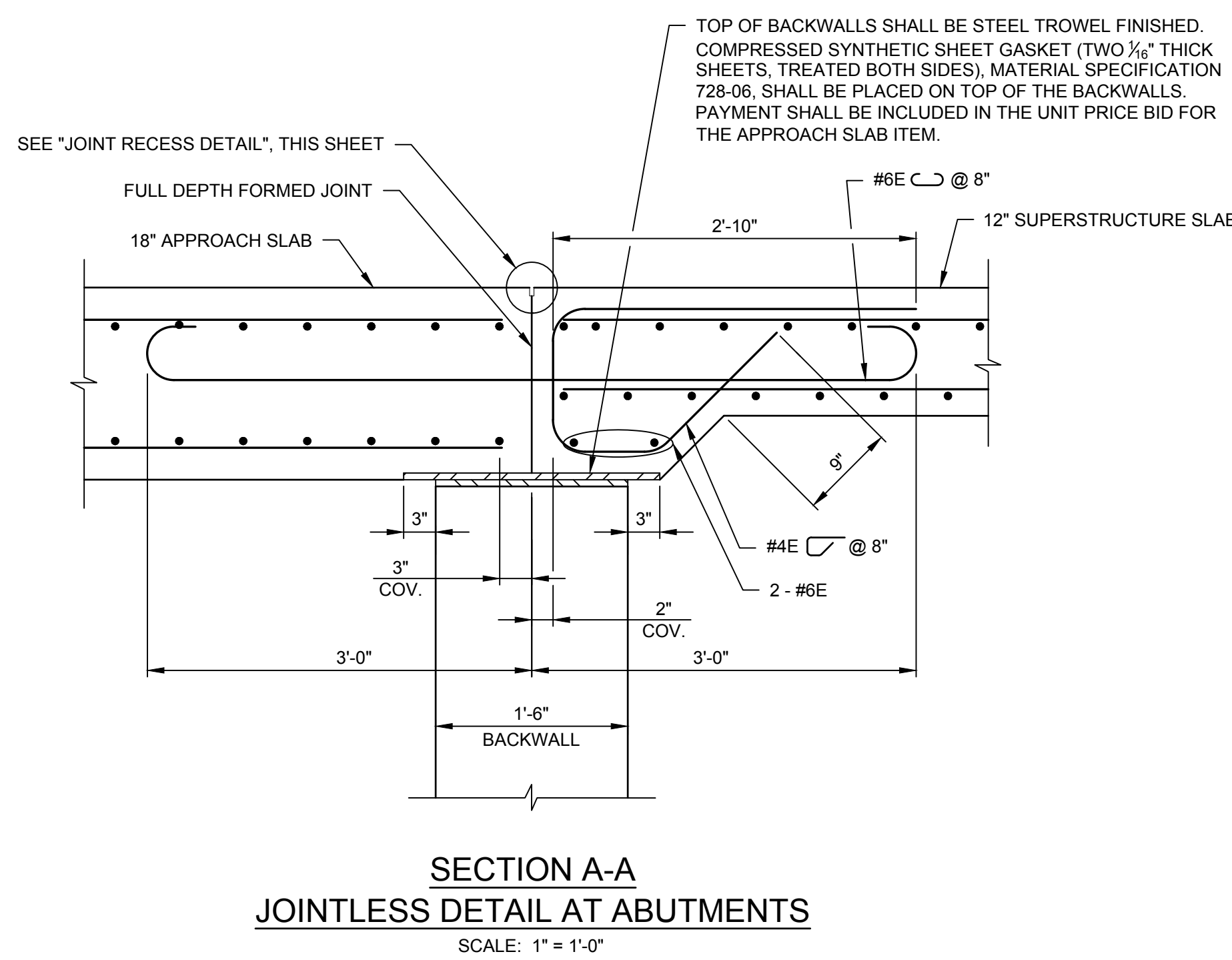


SUPERSTRUCTURE SLAB REINFORCEMENT PLAN

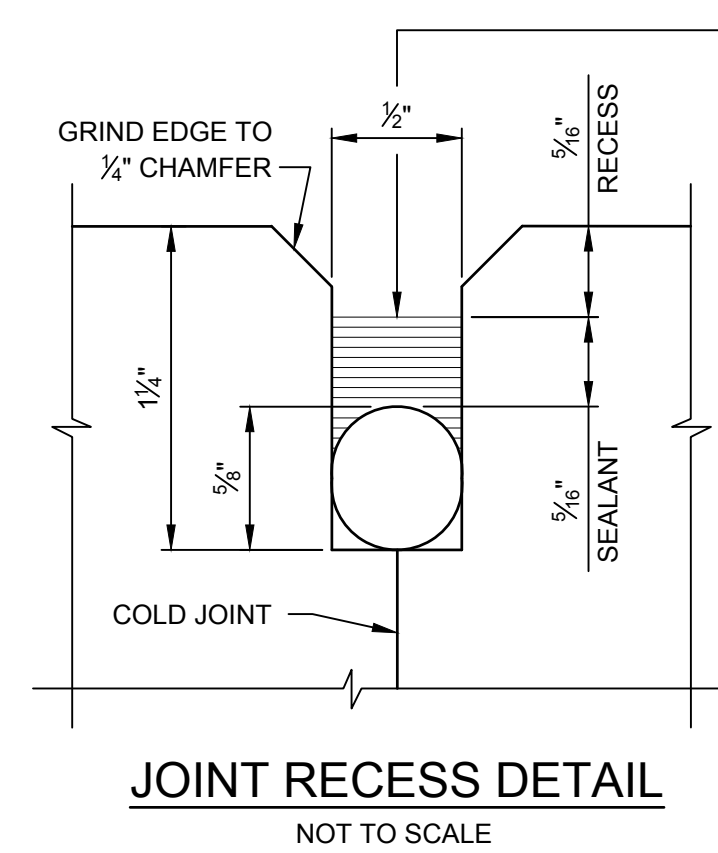
SCALE: 1/8" = 1'-0"

| SUPERSTRUCTURE SLAB TABLE | | | |
|---------------------------|---|--|---|
| PLACEMENT | SUPERSTRUCTURE SLAB (SY) ITEM 557.4103 AND WINTER SURFACE TREATMENT (SY) ITEM 557.29 | LONGITUDINAL SAWCUT GROOVING (SY) ITEM 558.02 | PROTECTIVE SEALING OF NEW DECKS (SF) ITEM 559.01 * |
| 1 | 382.22 SY | 355.55 SY | 4067.00 SF |
| 2 | 764.44 SY | 711.11 SY | 8134.00 SF |
| 3 | 764.40 SY | 711.11 SY | 8134.00 SF |
| TOTAL | 1911.06 SY | 1777.77 SY | 20335.00 SF |

* THIS QUANTITY INCLUDES THE AREA FOR PROTECTIVE SEALING OF THE CONCRETE BARRIERS

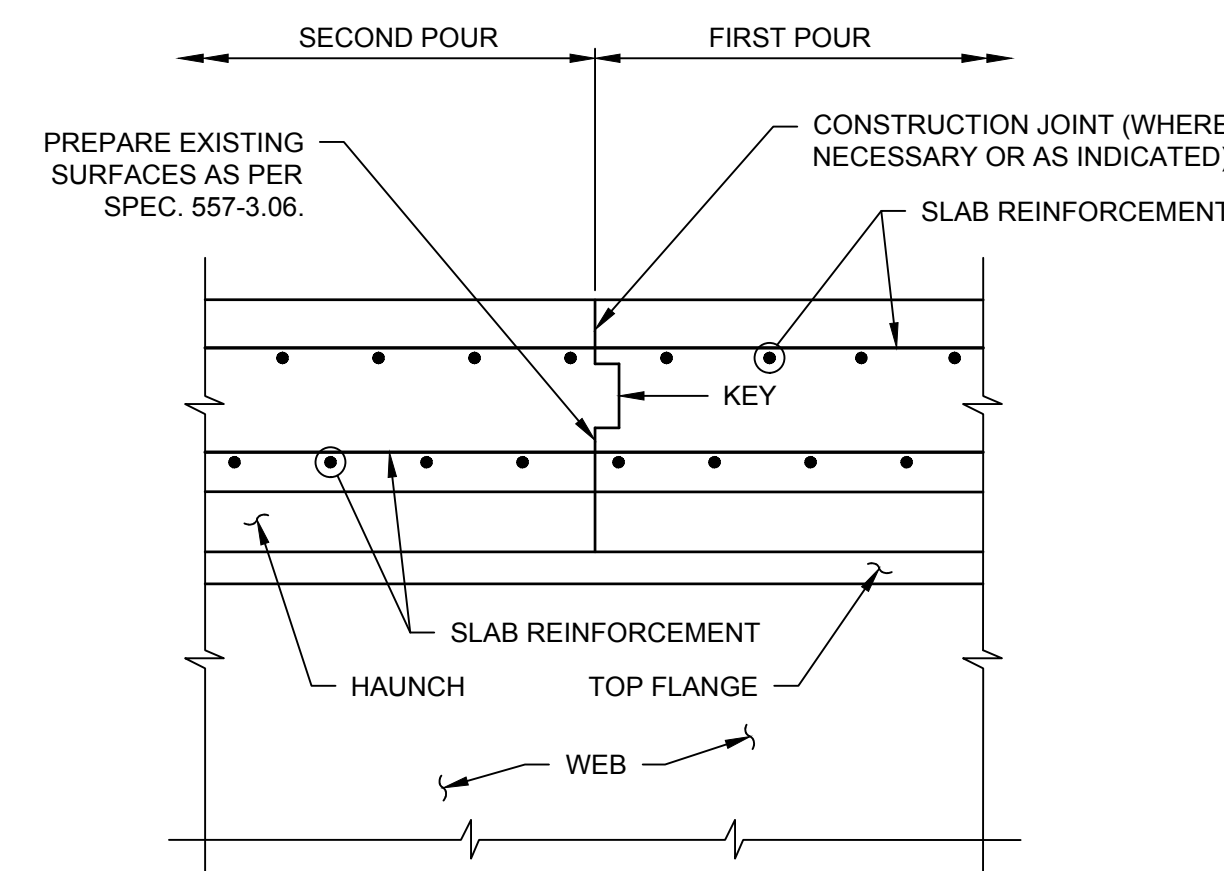


SECTION A-A
JOINTLESS DETAIL AT ABUTMENTS
 SCALE: 1" = 1'-0"

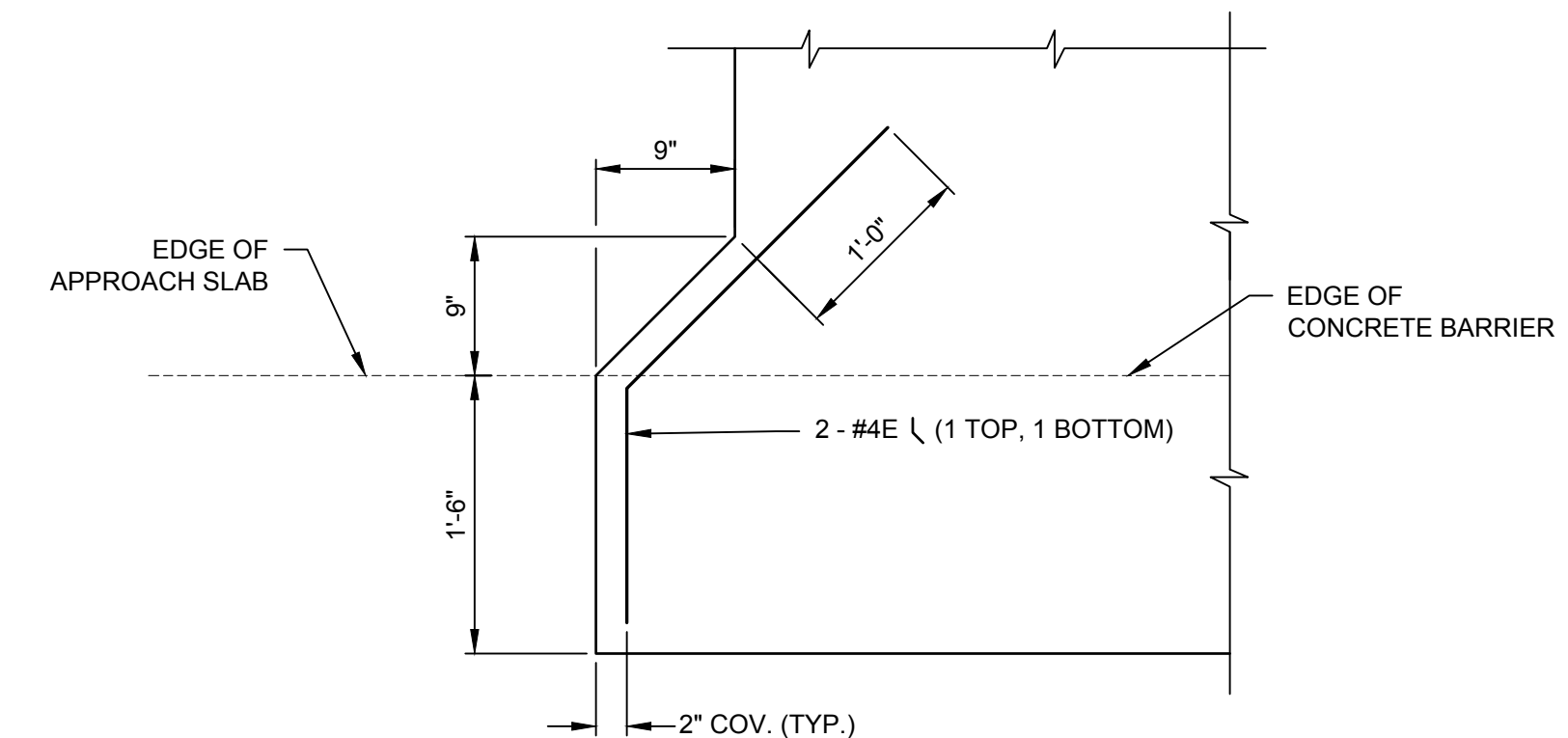


JOINT RECESS DETAIL
 NOT TO SCALE

FILL THE RECESS WITH A STRUCTURAL JOINT MATERIAL, SILICONE SEALANT, FROM THE NYSDOT APPROVED LIST FOR ITEM 557.51000016. IF THE RECESS IS SAW CUT, WATER BLAST IMMEDIATELY FOLLOWING CUTTING TO REMOVE ANY RESIDUAL SLURRY BEFORE IT DRIES. CLEAN THE VERTICAL FACES OF THE RECESS BY ABRASIVE BLAST, AND AIR BLOW THE RESIDUE FROM THE RECESS. PRIME THE VERTICAL FACES WITH THE MANUFACTURER'S RECOMMENDED PRIMER, AND ALLOW TO DRY. PLACE A 3/8" DIA. SOFT CLOSED CELL BACKER ROD IN THE BOTTOM OF THE RECESS. POUR THE SILICONE SEALANT TO A DEPTH OF APPROX. 3/8". PAYMENT TO BE INCLUDED IN THE UNIT PRICE BID FOR THE APPROACH SLAB.



TRANSVERSE CONSTRUCTION JOINT IN SLAB
 SCALE: 1" = 1'-0"



DETAIL A
 SCALE: 1" = 1'-0"

NOTES:

- (E) DENOTES EPOXY COATED BARS.
- ALL COVER SHALL BE 2" UNLESS OTHERWISE NOTED.
- ALL REINFORCING LAP LOCATIONS SHALL ALTERNATE.



McFarland Johnson
 60 RAILROAD PLACE
 SUITE 402
 SARATOGA SPRINGS, NEW YORK 12866
 P: 518-580-9380 F: 518-580-9383
 SaratogaROM@mjinc.com

PROJECT MILESTONE
GMP PLANS

| NO. | DATE | DESCRIPTION |
|-----|----------|-----------------------|
| 1 | 06/08/22 | GMP BID SET |
| 2 | 10/28/22 | GMP BID SET REVISIONS |
| | | |
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| | | |

CLIENT:
ALBANY PORT DISTRICT COMMISSION
 ALBANY, NEW YORK

PROJECT:
PORT OF ALBANY SITE INFRASTRUCTURE IMPROVEMENTS

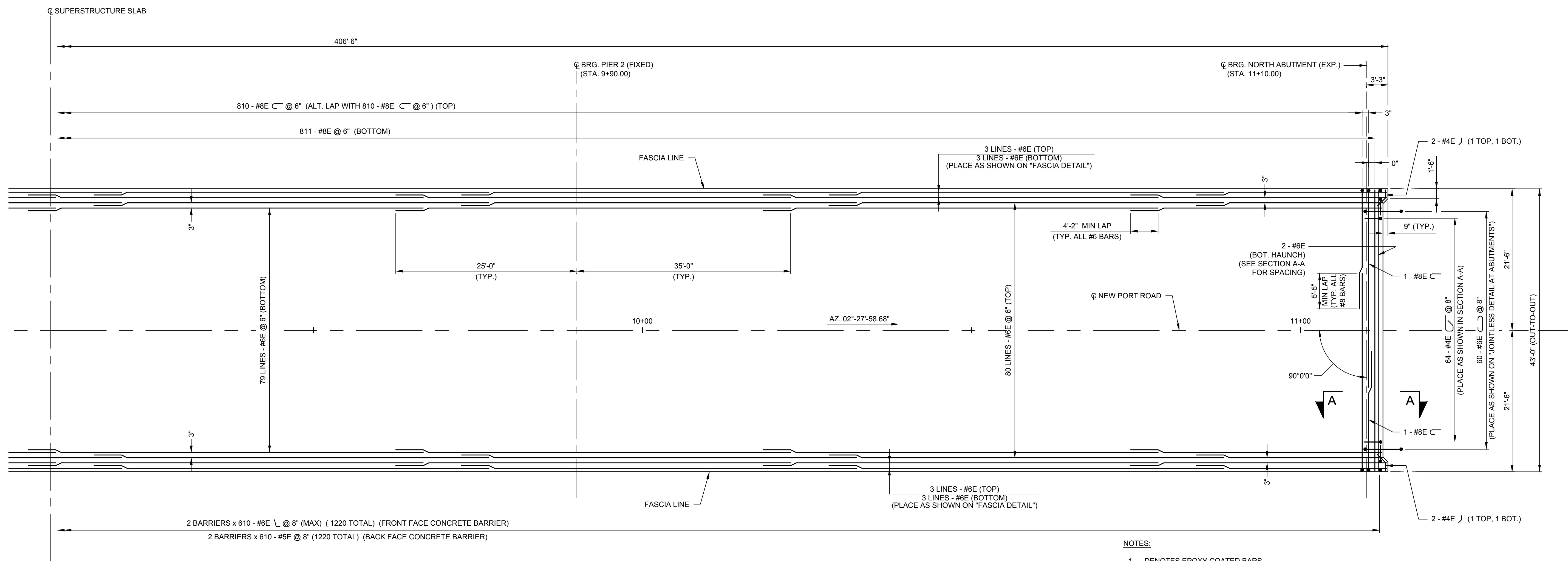
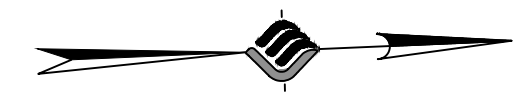
| | |
|----------|----------|
| DRAWN | SLM |
| DESIGNED | DRW |
| CHECKED | JLR |
| SCALE | AS SHOWN |
| DATE | 6/8/2022 |
| PROJECT | 18641.00 |



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DRAWING TITLE
SUPERSTRUCTURE SLAB (2 OF 2)

DRAWING NUMBER
ST-27



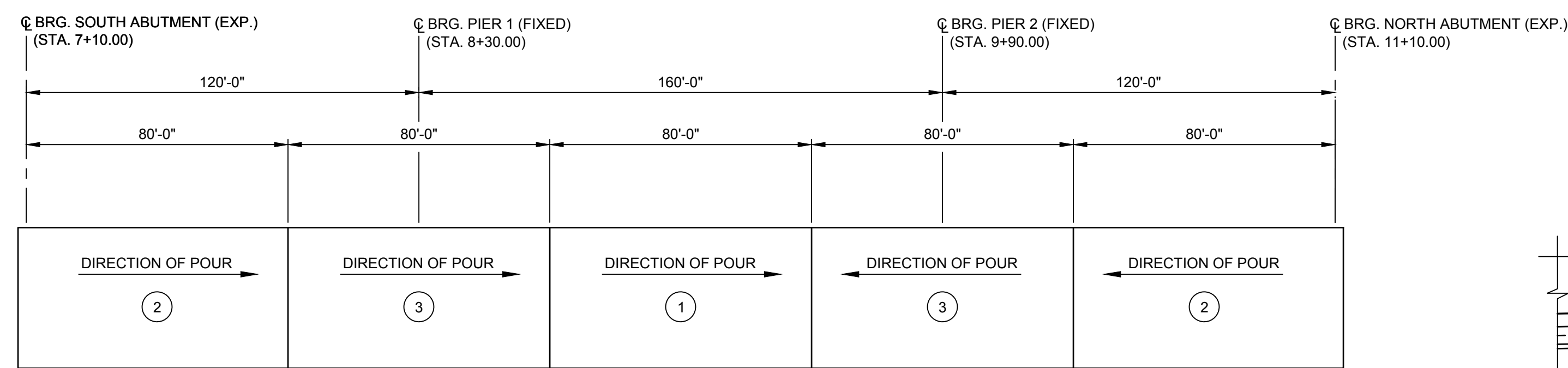
SUPERSTRUCTURE SLAB REINFORCEMENT PLAN
 SCALE: 1/8" = 1'-0"

NOTES:

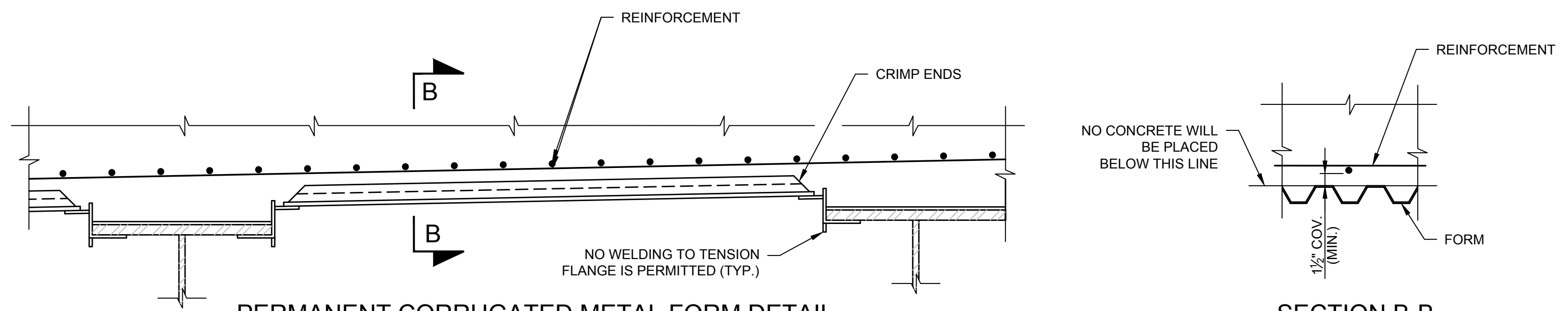
- DENOTES EPOXY COATED BARS.
- ALL COVER SHALL BE 2" UNLESS OTHERWISE NOTED.
- ALL REINFORCING LAP LOCATIONS SHALL ALTERNATE.
- SEE PREVIOUS SHEET FOR SECTION A-A.

FORM UNIT NOTES:

- THE COST OF THE FORMING SYSTEM SHOWN ON THIS DRAWING SHALL BE INCLUDED IN THE UNIT PRICE BID FOR SUPERSTRUCTURE SLAB CONCRETE ITEM.
- THERE SHALL BE NO WELDING TO METALIZED GIRDER SURFACES FOR THE ATTACHMENT OF FORMING DEVICES.
- THE SUPPORT ANGLES SHALL BE GALVANIZED IN ACCORDANCE WITH MATERIAL SPECIFICATION 719-01.



SUPERSTRUCTURE SLAB PLACEMENT SEQUENCE
 SCALE: 1/32" = 1'-0"



PERMANENT CORRUGATED METAL FORM DETAIL
 SCALE: 1" = 1'-0"

SECTION B-B
 SCALE: 1" = 1'-0"

NUMBER OF ALBANY PORT DISTRICT COMMISSION SHEETS: BRIDGE FINAL PLANS (WITH DOUBLE LANDING)



McFarland Johnson
 60 RAILROAD PLACE
 SUITE 402
 SARATOGA SPRINGS, NEW YORK 12866
 P: 518-580-9380 F: 518-580-9383
 SaratogaROM@mjinc.com

PROJECT MILESTONE

GMP PLANS

| NO. | DATE | DESCRIPTION |
|-----|----------|-----------------------|
| 1 | 06/08/22 | GMP BID SET |
| 2 | 10/28/22 | GMP BID SET REVISIONS |
| | | |
| | | |
| | | |
| | | |

CLIENT:
ALBANY PORT DISTRICT COMMISSION

ALBANY, NEW YORK

PROJECT:
PORT OF ALBANY SITE INFRASTRUCTURE IMPROVEMENTS

| | |
|----------|----------|
| DRAWN | SLM |
| DESIGNED | TEPA |
| CHECKED | JLR |
| SCALE | AS SHOWN |
| DATE | 6/8/2022 |
| PROJECT | 18641.00 |



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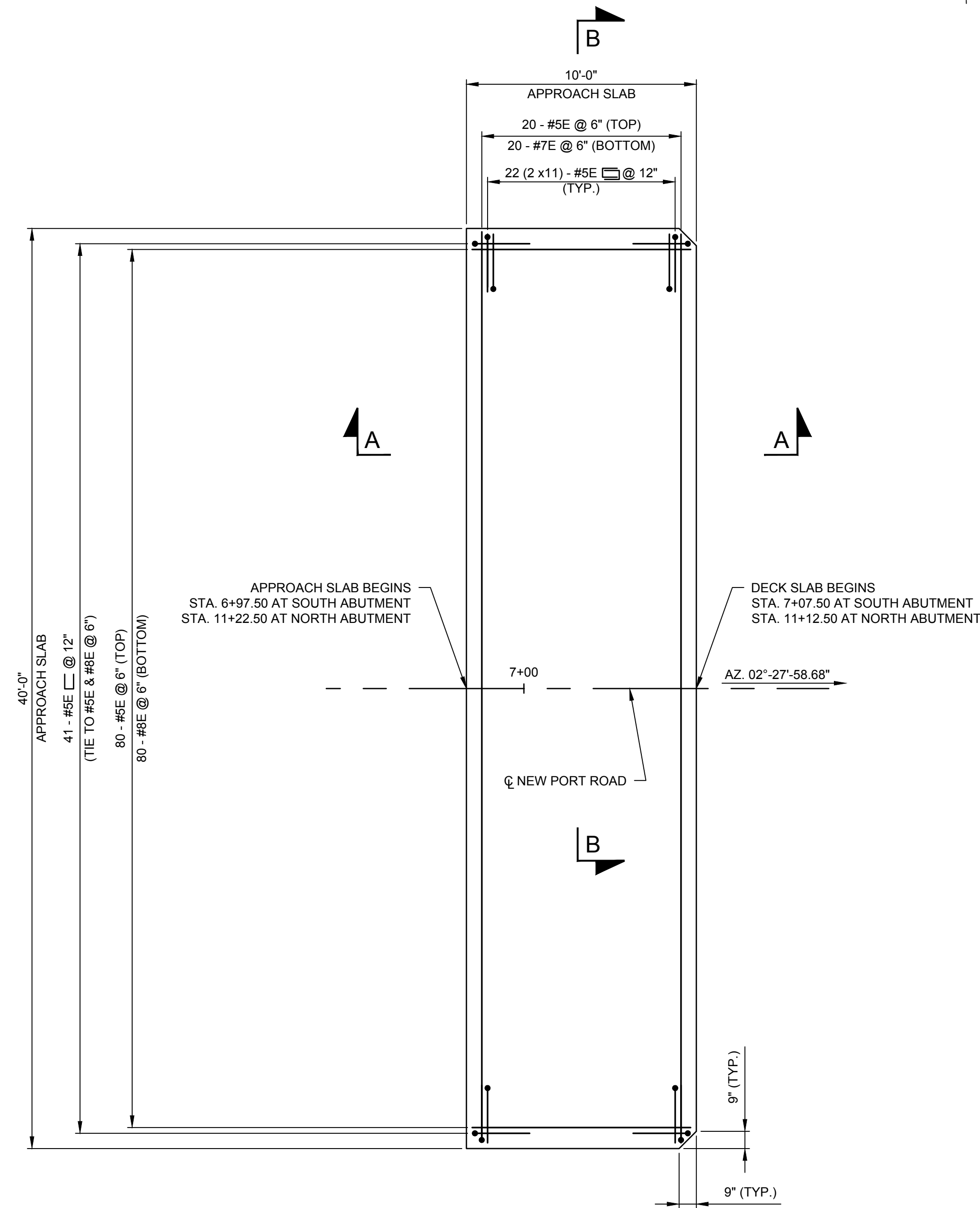
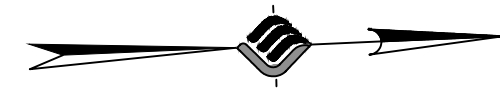
DRAWING TITLE

APPROACH SLAB DETAILS

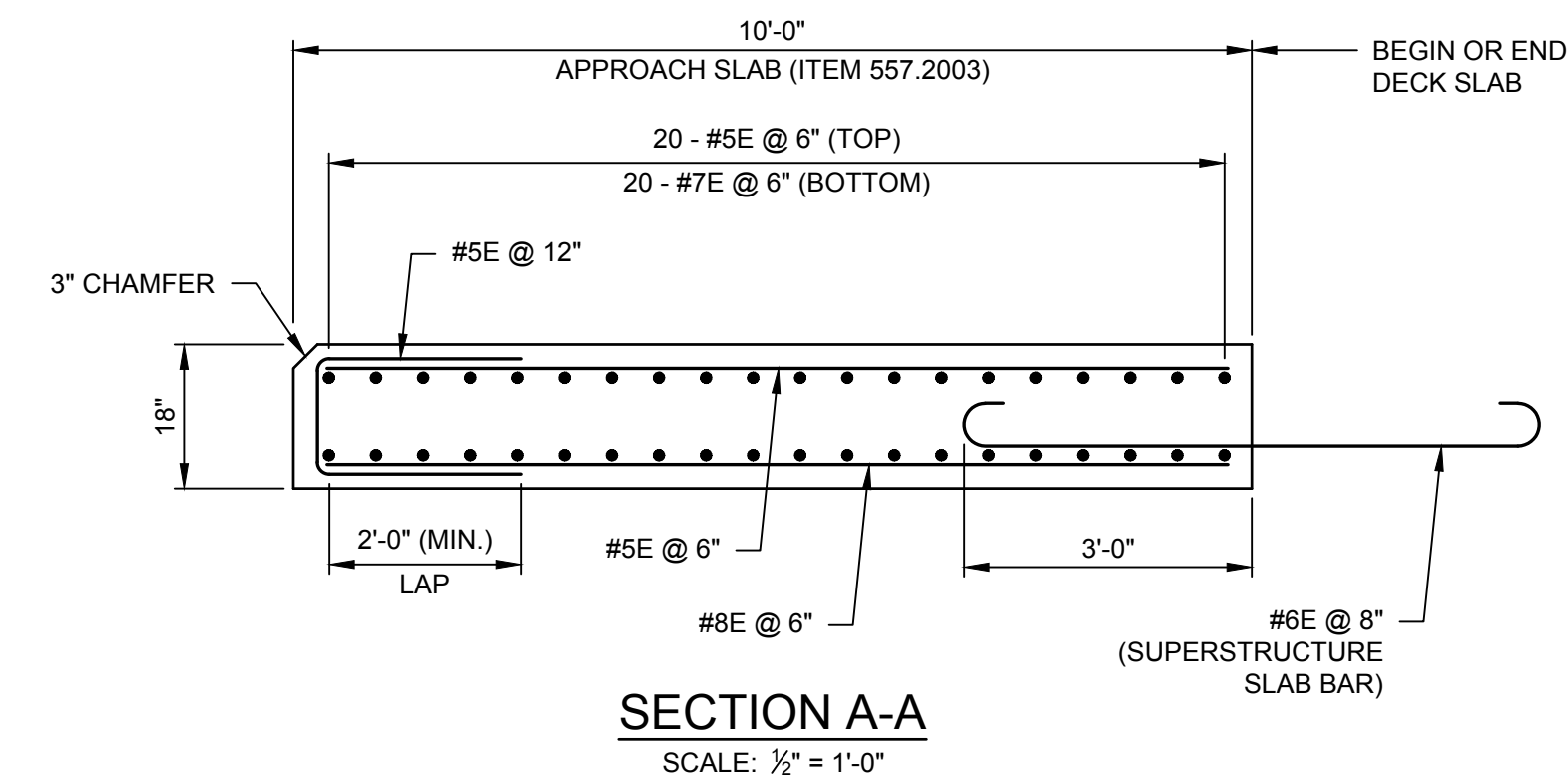
DRAWING NUMBER

ST-28

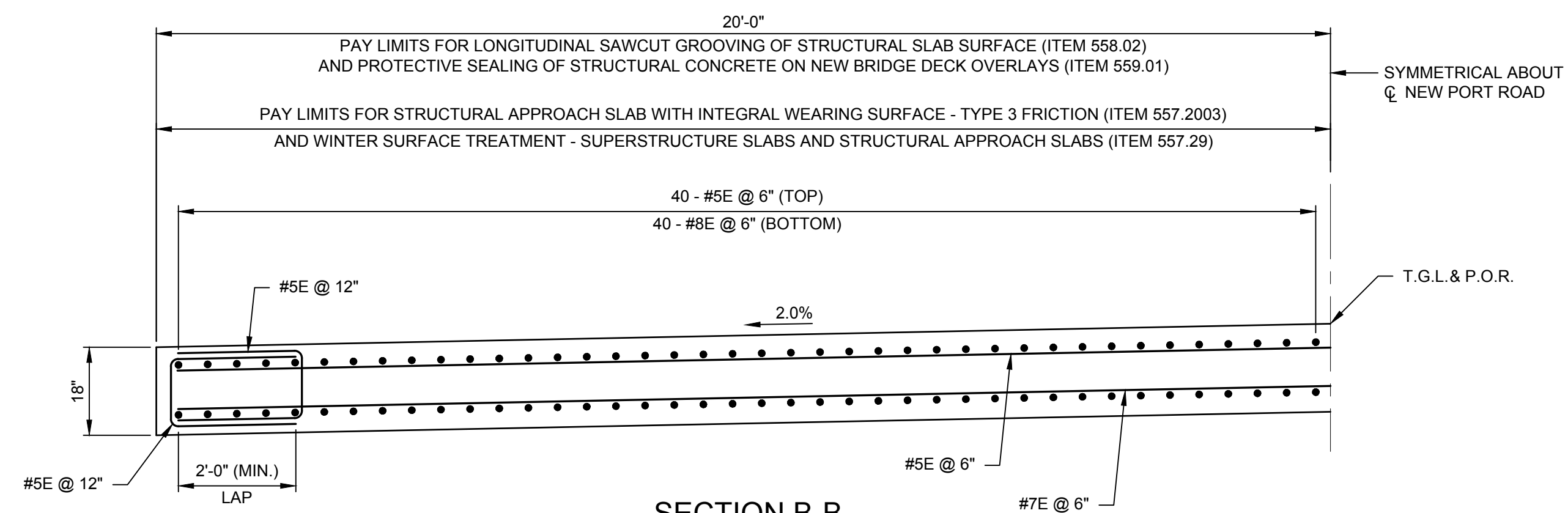
28 OF 35



APPROACH SLAB REINFORCEMENT PLAN
 (SOUTH APPROACH SLAB SHOWN, NORTH APPROACH SLAB OPPOSITE HAND)
 SCALE: 1/2" = 1'-0"



SECTION A-A
 SCALE: 1/2" = 1'-0"



SECTION B-B
 SCALE: 1/2" = 1'-0"

| APPROACH SLAB TABLE | | | |
|---------------------|---|---|--|
| LOCATION | APPROACH SLAB ITEM 557.2003 AND WINTER SURFACE TREATMENT (SY) ITEM 557.29 | LONGITUDINAL SAWCUT GROOVING (SY) ITEM 558.02 | PROTECTIVE SEALING OF NEW DECKS (SF) ITEM 559.01 |
| BEGIN APPROACH SLAB | 44.4 SY | 44.4 SY | 400 SF |
| END APPROACH SLAB | 44.4 SY | 44.4 SY | 400 SF |

NOTES:

- ALL REINFORCEMENT SHALL BE EPOXY COATED.
- ALL REINFORCEMENT SHALL HAVE A 3" COVER UNLESS OTHERWISE NOTED.
- TOP SURFACES OF STRUCTURAL SLABS AND APPROACH SLABS SHALL BE GROOVED UNDER THE SAWCUT GROOVING OF STRUCTURAL SLAB SURFACE ITEM.



McFarland Johnson
 60 RAILROAD PLACE
 SUITE 402
 SARATOGA SPRINGS, NEW YORK 12866
 P: 518-580-9380 F: 518-580-9383
 SaratogaROM@mjinc.com

PROJECT MILESTONE
GMP PLANS

| NO. | DATE | DESCRIPTION |
|-----|----------|-----------------------|
| 1 | 06/08/22 | GMP BID SET |
| 2 | 10/28/22 | GMP BID SET REVISIONS |
| | | |
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| | | |
| | | |
| | | |
| | | |

CLIENT:
ALBANY PORT DISTRICT COMMISSION
 ALBANY, NEW YORK

PROJECT:
PORT OF ALBANY SITE INFRASTRUCTURE IMPROVEMENTS

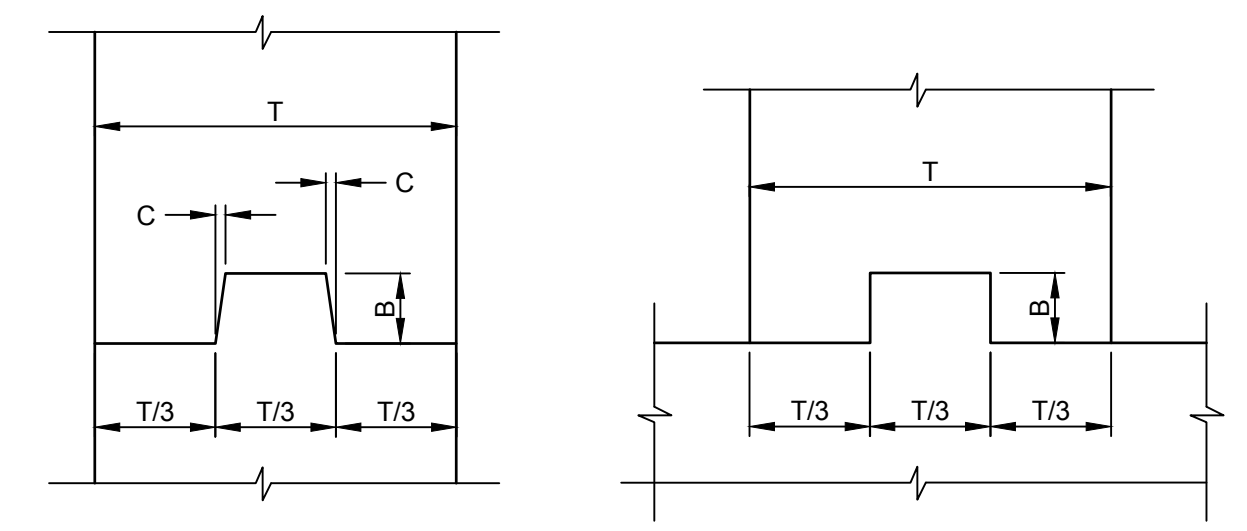
| | |
|----------|----------|
| DRAWN | SLM |
| DESIGNED | RLJ |
| CHECKED | JLR |
| SCALE | AS SHOWN |
| DATE | 6/8/2022 |
| PROJECT | 18641.00 |



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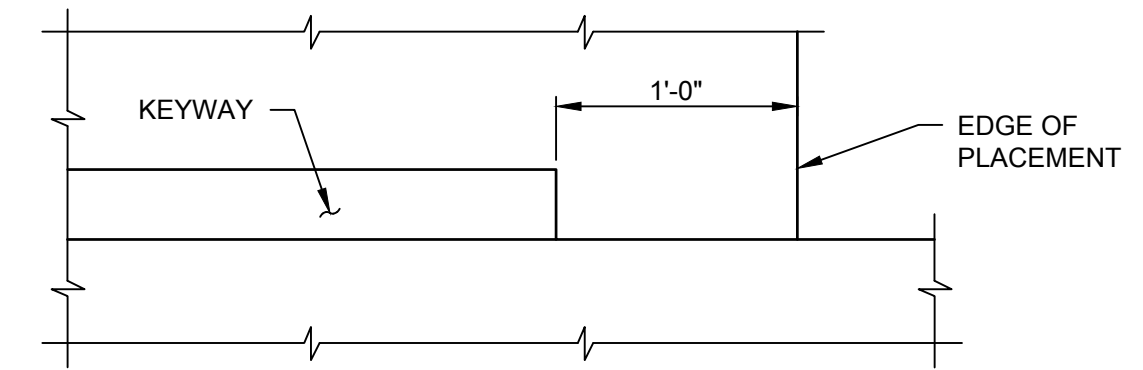
DRAWING TITLE
MISCELLANEOUS DETAILS

DRAWING NUMBER
ST-29



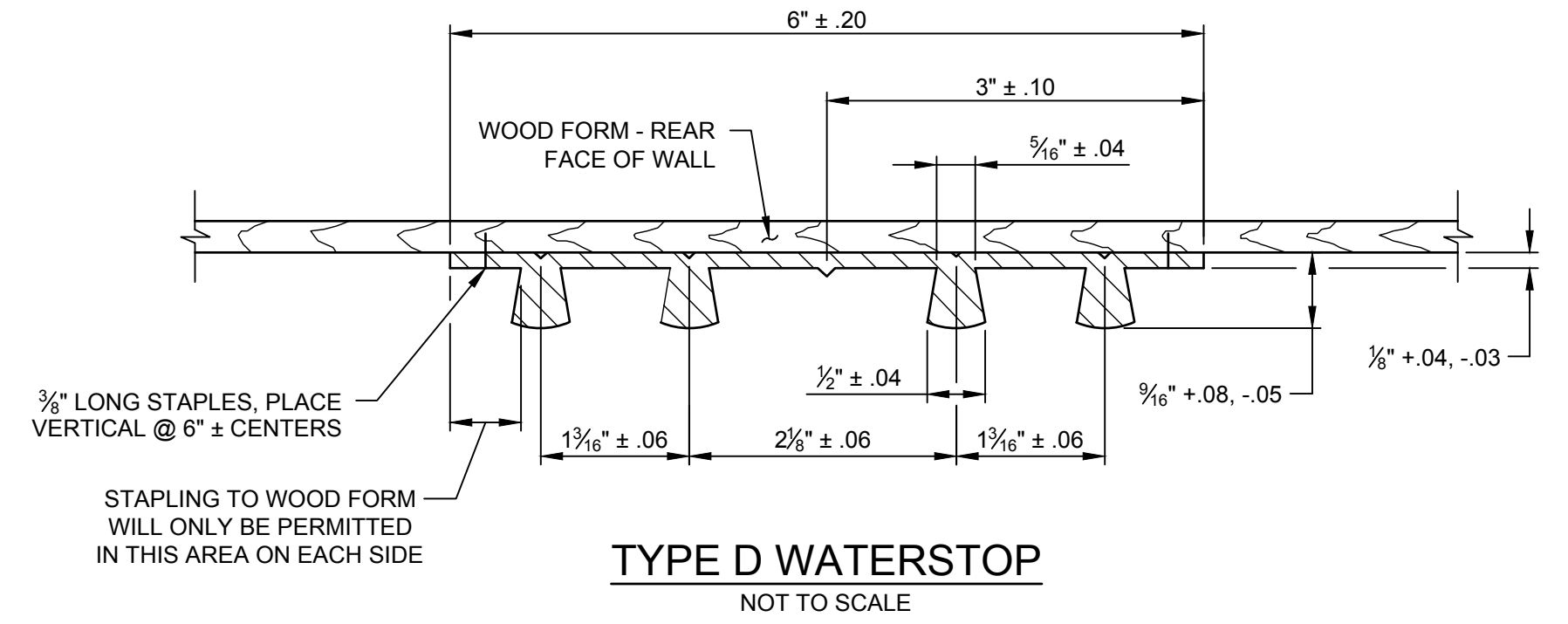
NOTE: WATERSTOP NOT SHOWN.

VERTICAL **HORIZONTAL**

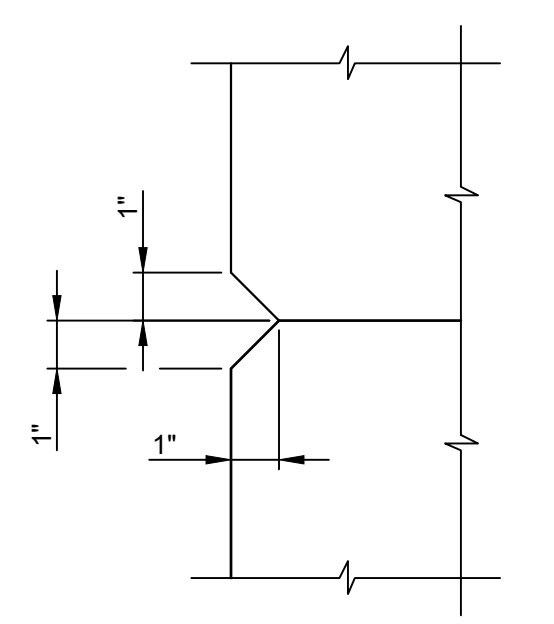


| CONSTRUCTION AND CONTRACTION JOINTS | | |
|-------------------------------------|--------|--------------|
| C | B | T/3 |
| 3/16" | 1 1/2" | 0 TO 6" |
| 3/8" | 3 1/2" | 6" TO 10" |
| 3/4" | 5 1/2" | 10" AND OVER |

KEYWAY DETAILS
 NOT TO SCALE



TYPE D WATERSTOP
 NOT TO SCALE



CHAMFER DETAIL
 SCALE: 3" = 1'-0"

WATERSTOP NOTES:

PVC USED IN WATERSTOPS SHALL CONFORM TO THE REQUIREMENTS OF N.Y.S. STANDARD SPECIFICATIONS SUBSECTION 705.11.

THE COST OF FURNISHING AND PLACING WATERSTOPS SHALL BE INCLUDED IN THE UNIT PRICES BID FOR THE CONCRETE ITEMS.

FIELD SPLICES SHOULD BE AVOIDED IF POSSIBLE. HOWEVER, HEAT WELDED BUTT SPLICES WILL BE PERMITTED ON LONG STRAIGHT RUNS (GENERALLY IN EXCESS OF 50 FEET) AT POINTS APPROVED BY THE ENGINEER.

WATERSTOP SHALL BE SHIPPED IN STRAIGHT SECTIONS HAVING A MINIMUM LENGTH OF 10 FEET UNLESS SHORTER LENGTHS ARE REQUIRED.

REMOULDED RESILIENT JOINT FILLER SHALL CONFORM TO THE REQUIREMENTS OF N.Y.S. STANDARD SPECIFICATION SUBSECTION 705-07.

DETAILS ON THE DRAWINGS LABELED AS "NOT TO SCALE" ARE INTENTIONALLY DRAWN NOT TO SCALE FOR VISUAL CLARITY. ALL OTHER DETAILS, FOR WHICH NO SCALE IS SHOWN, ARE DRAWN PROPORTIONAL AND ARE FULLY DIMENSIONED.

TYPE D WATERSTOP SHALL BE LIGHT GRAY IN COLOR.

HOLES MUST NOT BE MADE IN WATERSTOP FOR ANY PURPOSE EXCEPT AS REQUIRED FOR STAPLING TO FORMS.

NOTE:
 1. FOR LOCATION OF TYPE D WATERSTOP, SEE SHEET ST-13.

N:\18641.00\ALBANY PORT EXPANSION\DRAWINGS\CONCRETE\BRIDGE PANEL PLANS\ST-29\ST-29.DWG



McFarland Johnson
 60 RAILROAD PLACE
 SUITE 402
 SARATOGA SPRINGS, NEW YORK 12866
 P:518-580-9380 F:518-580-9383
 SaratogaROM@mjinc.com

PROJECT MILESTONE
GMP PLANS

| NO. | DATE | DESCRIPTION |
|-----|----------|-----------------------|
| 1 | 06/08/22 | GMP BID SET |
| 2 | 10/28/22 | GMP BID SET REVISIONS |
| | | |
| | | |
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CLIENT:
ALBANY PORT DISTRICT COMMISSION
 ALBANY, NEW YORK

PROJECT:
PORT OF ALBANY SITE INFRASTRUCTURE IMPROVEMENTS

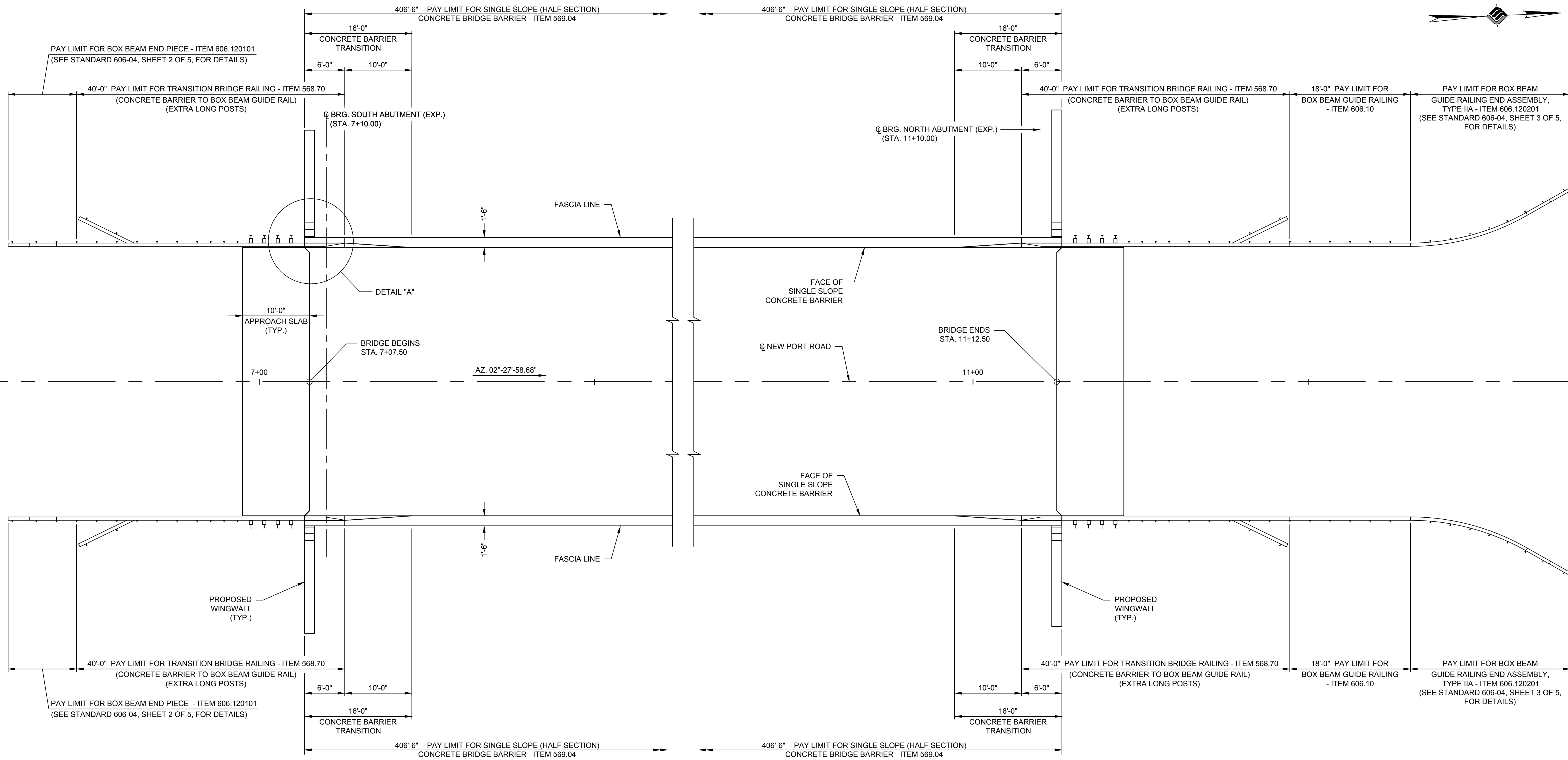
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|----------|----------|
| DRAWN | SLM |
| DESIGNED | RLJ |
| CHECKED | JLR |
| SCALE | AS SHOWN |
| DATE | 6/8/2022 |
| PROJECT | 18641.00 |



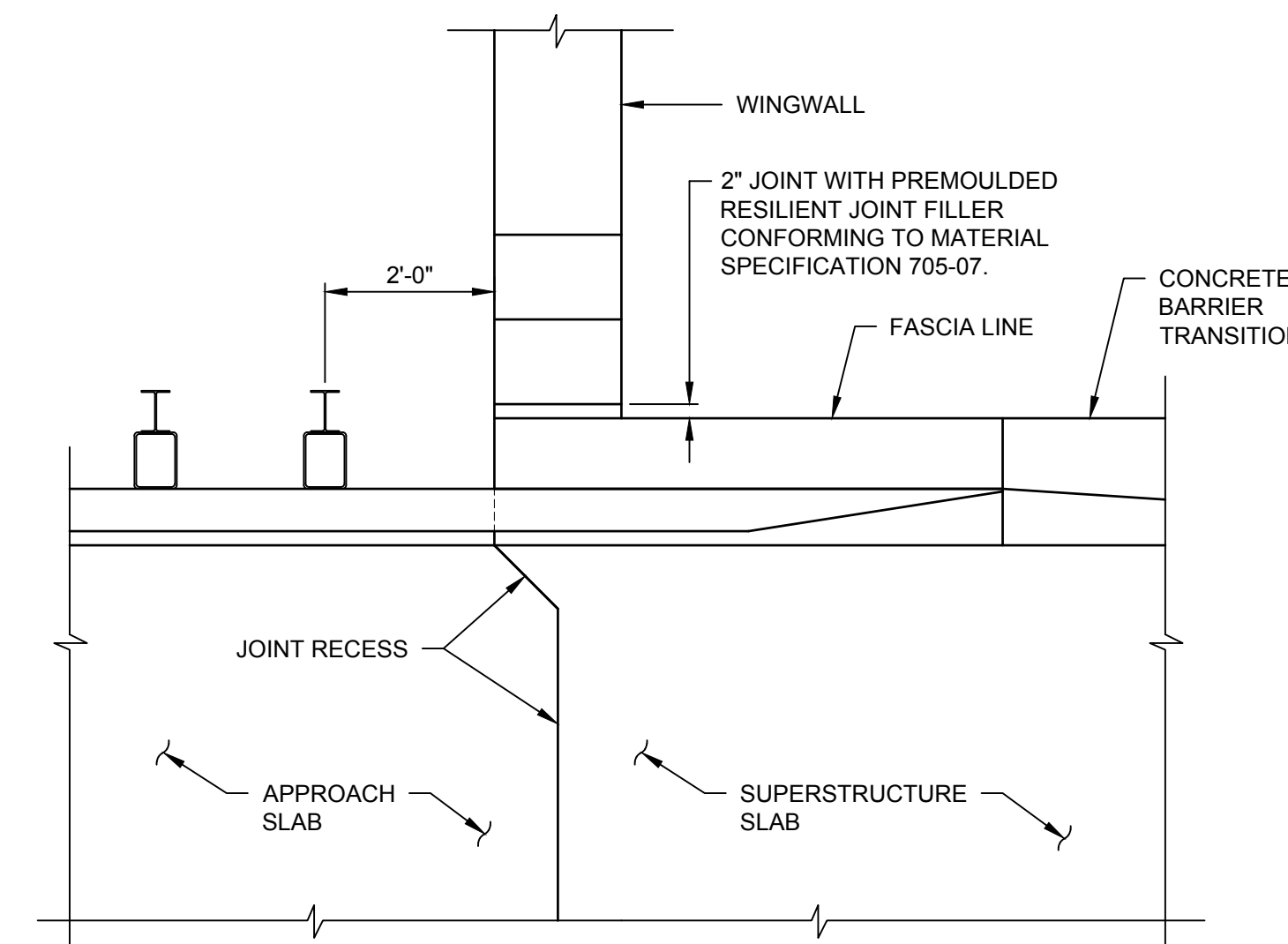
IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECT DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.

DRAWING TITLE
BARRIER AND RAILING LAYOUT PLAN

DRAWING NUMBER
ST-30



BARRIER AND RAILING LAYOUT PLAN
 SCALE: 1/8" = 1'-0"



DETAIL "A"
 (TYP. AT ALL 4 CORNERS OF BRIDGE)
 SCALE: 1/2" = 1'-0"

NOTE:
 SEE SUPERSTRUCTURE SLAB REINFORCEMENT, SHEETS ST-26 AND ST-27, AND APPROACH SLAB DETAILS, SHEET ST-28, FOR REINFORCEMENT DETAILS

N:\18641\01 ALBANY PORT EXPANSION\DRAWINGS\SHEET FILES\BRIDGE FINAL PLANS\ST-30-BARRIER AND RAILING.DWG



McFarland Johnson

60 RAILROAD PLACE
SUITE 402
SARATOGA SPRINGS, NEW YORK 12866
P: 518-580-9380 F: 518-580-9383
SaratogaROM@mjinc.com

PROJECT MILESTONE

GMP PLANS

| NO. | DATE | DESCRIPTION |
|-----|----------|-----------------------|
| 1 | 06/08/22 | GMP BID SET |
| 2 | 10/28/22 | GMP BID SET REVISIONS |
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| DESIGNED | DRW |
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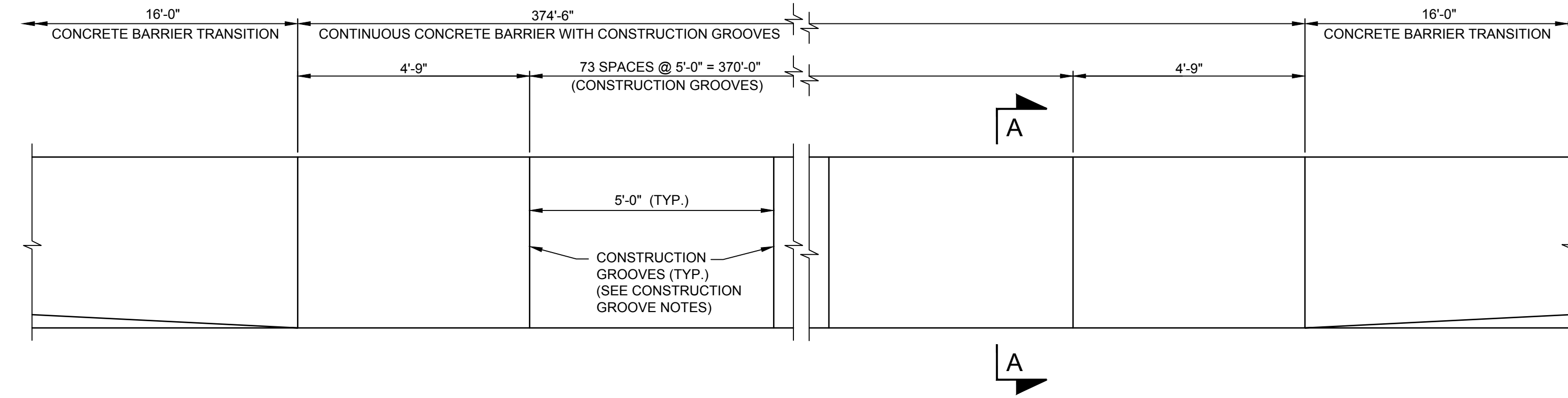
DRAWING TITLE

CONCRETE SINGLE SLOPE BARRIER DETAILS (1 OF 2)

DRAWING NUMBER

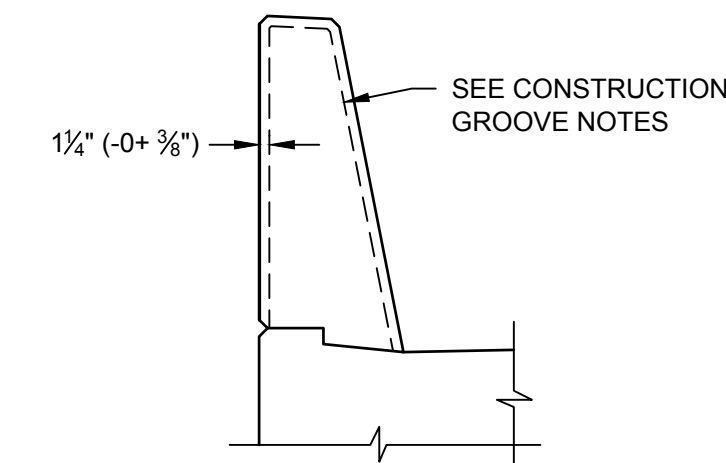
ST-31

31 OF 35



CONCRETE SINGLE SLOPE BARRIER GROOVE SPACING - ELEVATION

SCALE: 1/2" = 1'-0"

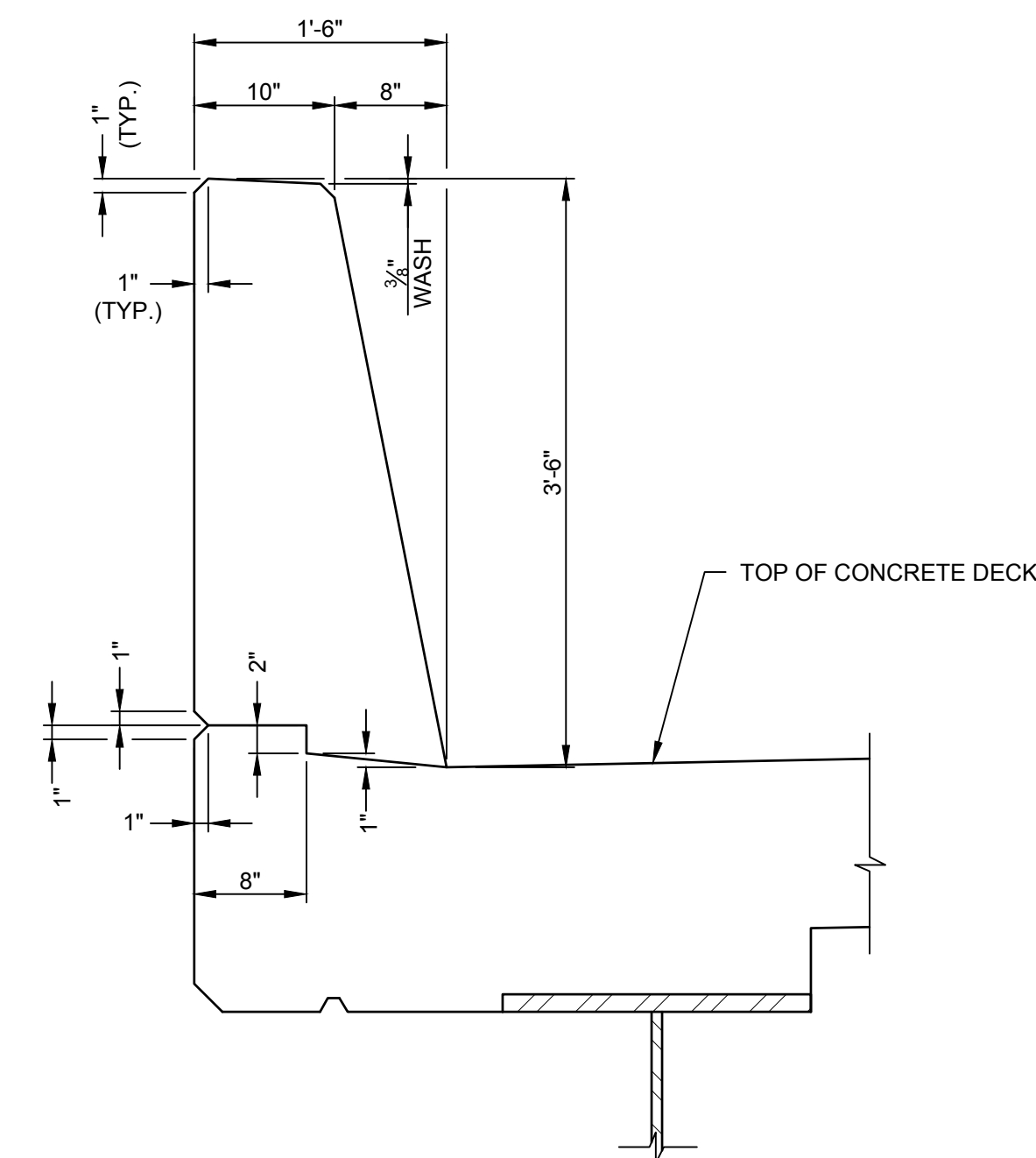


SECTION A-A

SCALE: 1/2" = 1'-0"

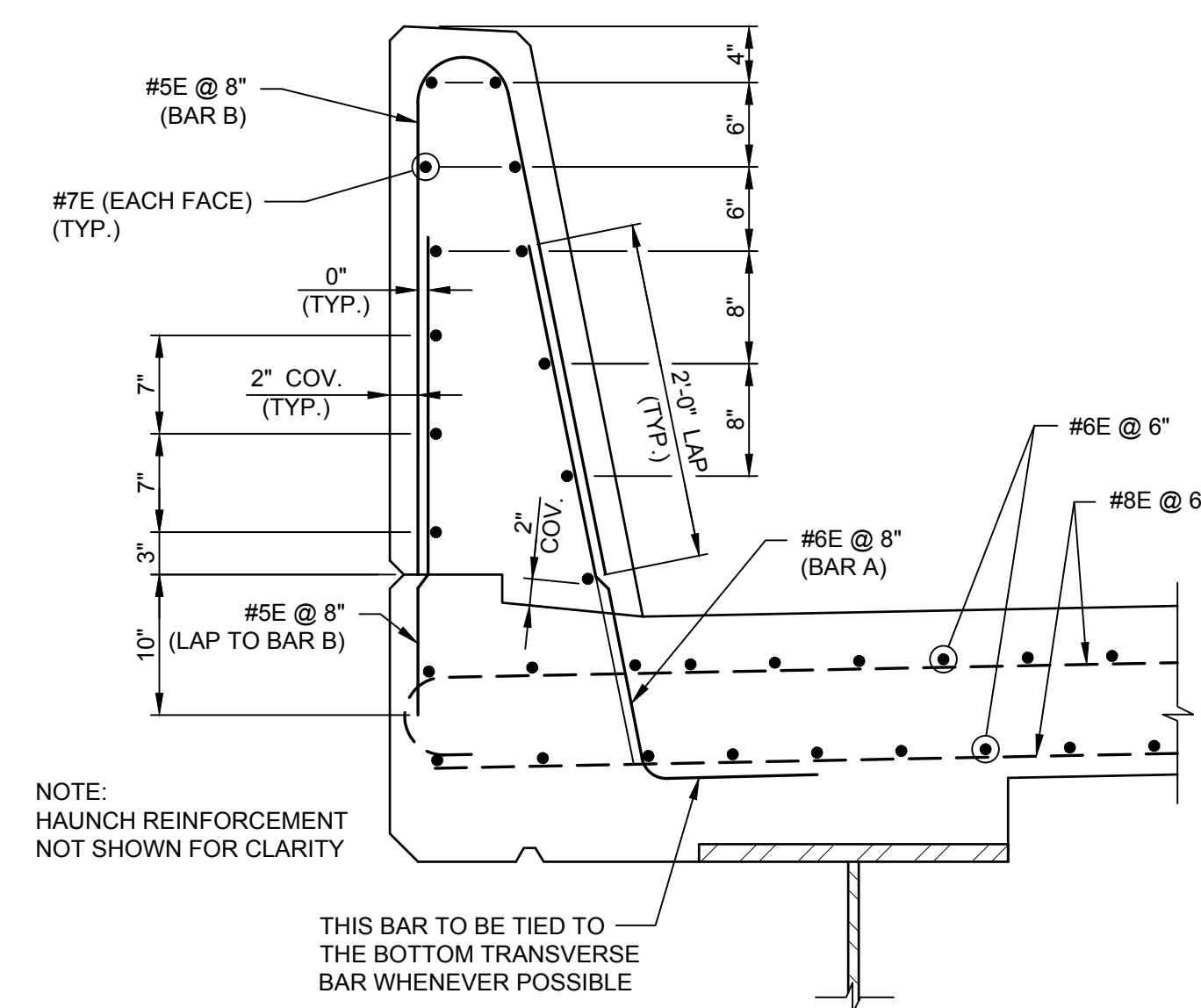
CONSTRUCTION GROOVE NOTES:

1. FORMING FOR CAST-IN-PLACE CONSTRUCTED FORMS.
 2. SAWING THE SET CONCRETE WITHIN 8 HOURS OF PLACEMENT FOR CAST-IN-PLACE SLIP FORMED.
 3. CUTTING THE PLASTIC CONCRETE.
- COMBINATION OF THESE METHODS MAY BE EMPLOYED. THE DEPTH OF THE GROOVES SHALL BE 1 1/2" (-0+ 3/8"). FORMED GROOVES SHALL MAKE A 60° ANGLE WITH THE SURFACE. CUT GROOVES SHALL BE FINISHED WITH SHALLOW (3/4"± 1/4") CONSTANT DEPTH 45° CHAMFERS AT THE SURFACE.



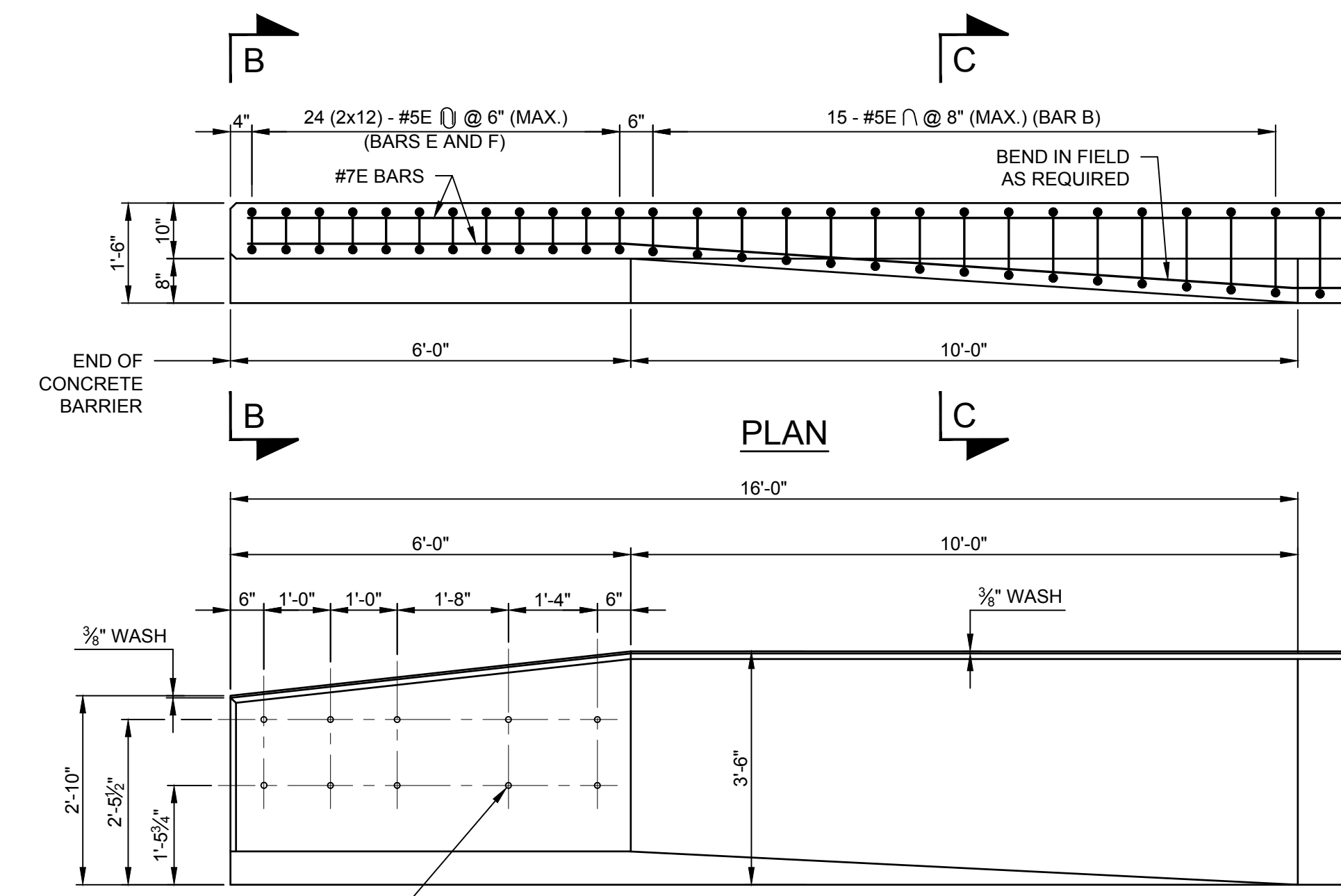
CONCRETE SINGLE SLOPE BARRIER SECTION

SCALE: 1" = 1'-0"



CONCRETE SINGLE SLOPE BARRIER SECTION (REINFORCEMENT)

SCALE: 1" = 1'-0"



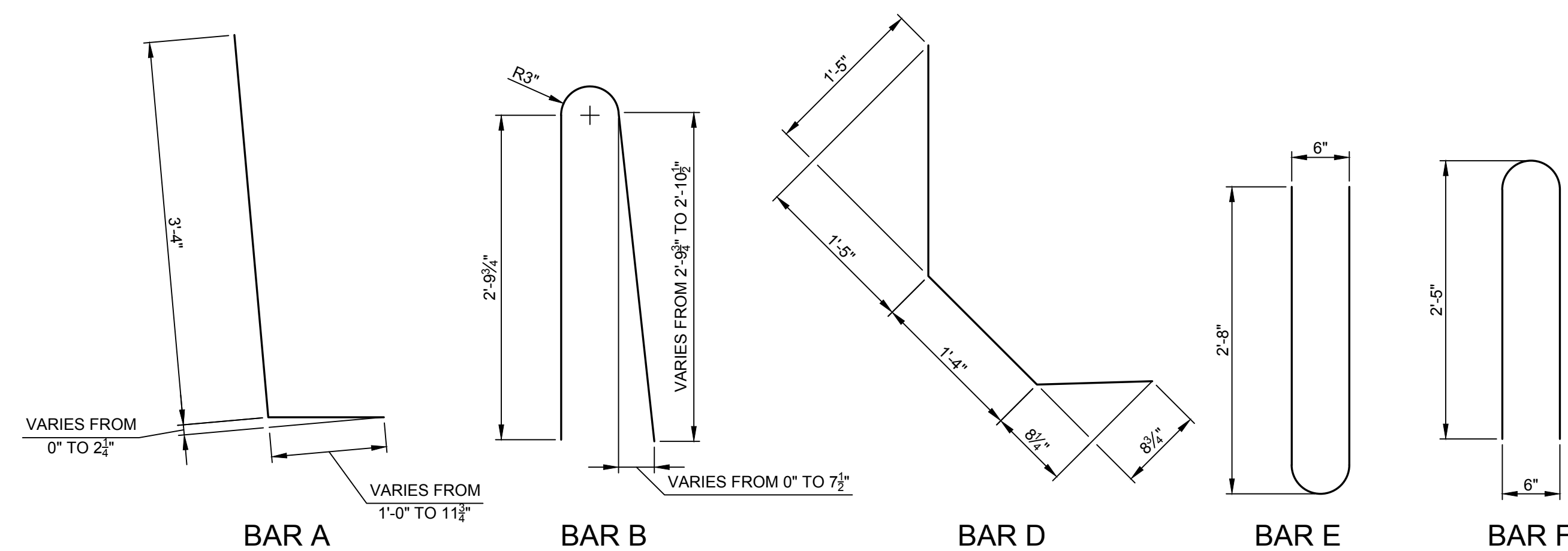
CONCRETE BARRIER TRANSITION

SCALE: 1/2" = 1'-0"

1" DIA. P.V.C. PIPE OR 1" DIA. GALVANIZED STEEL PIPE (ASTM A53, NPS 1" DIA.). THE COST OF FURNISHING AND PLACING SHALL BE INCLUDED IN THE CONCRETE SINGLE SLOPE BARRIER ITEM.

NOTE:

FOR SECTIONS B-B AND C-C SEE SHEET ST-32.



BAR TYPES

NOTE:
ALL BAR DIMENSIONS ARE OUT TO OUT.
ALL RADII ARE TO OUTSIDE OF BAR.



McFarland Johnson
 60 RAILROAD PLACE
 SUITE 402
 SARATOGA SPRINGS, NEW YORK 12866
 P: 518-580-9380 F: 518-580-9383
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PROJECT MILESTONE
GMP PLANS

| NO. | DATE | DESCRIPTION |
|-----|----------|-----------------------|
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CLIENT: **ALBANY PORT DISTRICT COMMISSION**
 ALBANY, NEW YORK
 PROJECT: **PORT OF ALBANY SITE INFRASTRUCTURE IMPROVEMENTS**

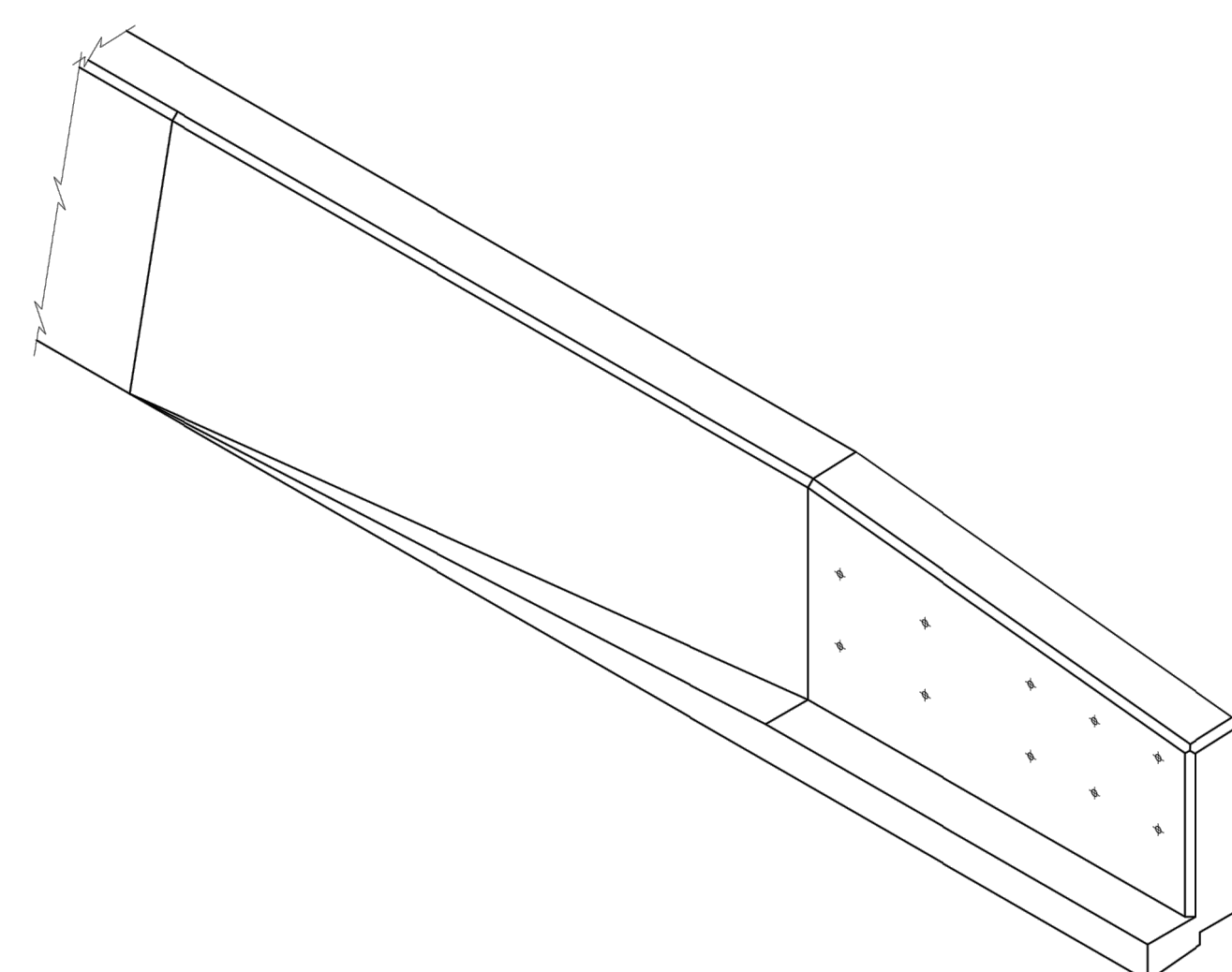
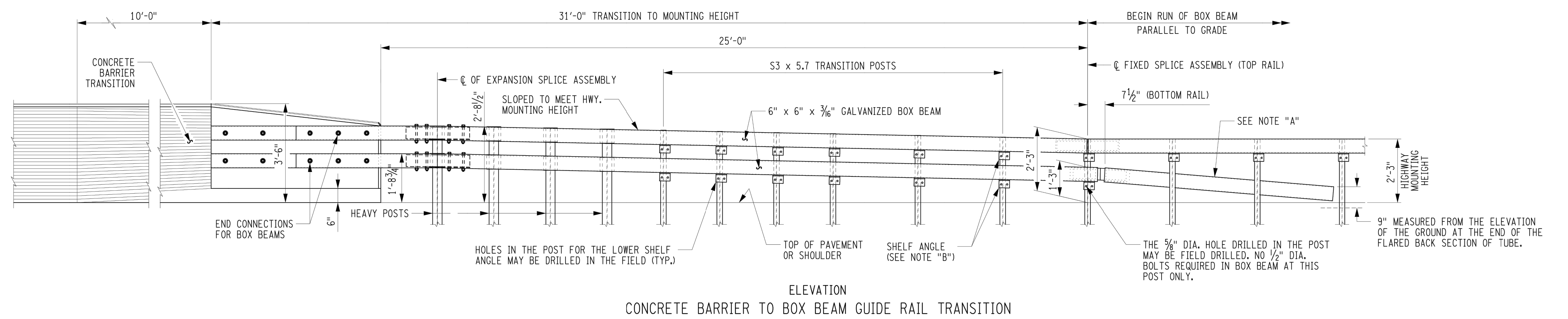
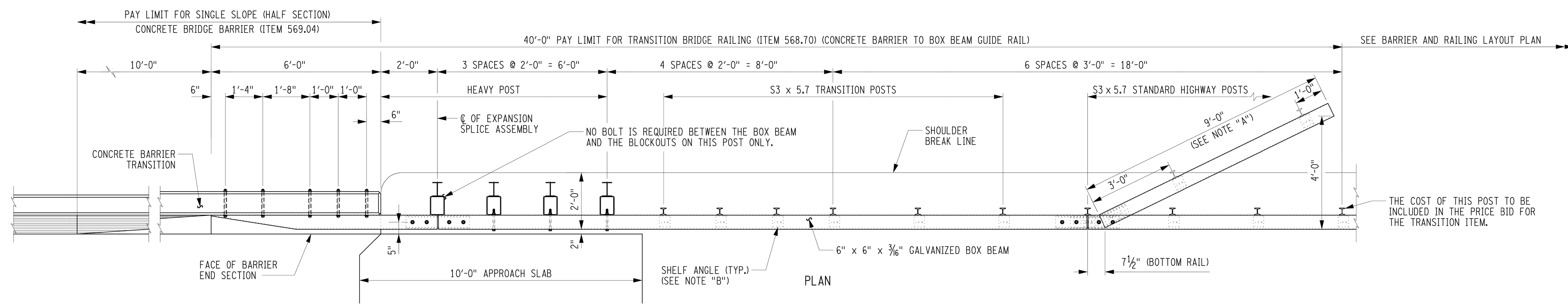
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| DRAWN | SLM |
| DESIGNED | RLJ |
| CHECKED | JLR |
| SCALE | AS SHOWN |
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| PROJECT | 18641.00 |



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DRAWING TITLE
RAILING DETAILS (1 OF 3)

DRAWING NUMBER
ST-33



ISOMETRIC OF CONCRETE SINGLE SLOPE BARRIER END SECTION

- NOTES:
- NOTE "A":
 THE COST OF THE POSTS, SPLICE TUBE AND RAIL FOR THE LOWER TUBE FLARE SECTION IS INCLUDED IN THE PRICE BID FOR THE TRANSITION ITEM.
- NOTE "B":
 SEE TYPICAL RAIL TO POST CONNECTION DETAIL ON CURRENT HIGHWAY STANDARD SHEET TITLE "BOX BEAM GUIDE RAIL".

N:\18641\01 ALBANY PORT EXPANSION\DRAWINGS\SHEET FILES\BRIDGE FINAL PLANS\RAILING\DETAILS\RAILING.DWG



McFarland Johnson
 60 RAILROAD PLACE
 SUITE 402
 SARATOGA SPRINGS, NEW YORK 12866
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PROJECT MILESTONE

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ALBANY, NEW YORK

PROJECT: PORT OF ALBANY SITE INFRASTRUCTURE IMPROVEMENTS

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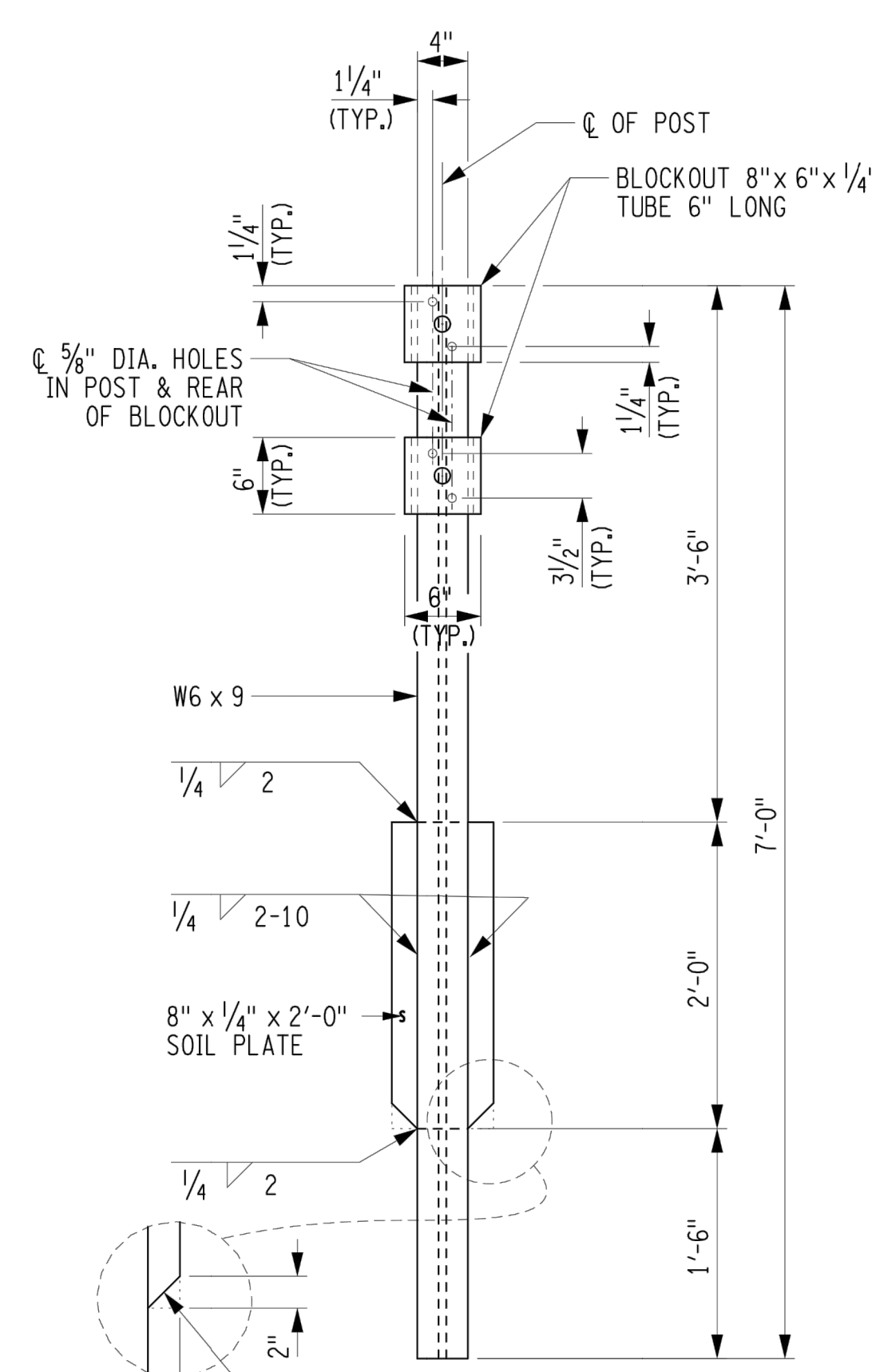
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DRAWING TITLE

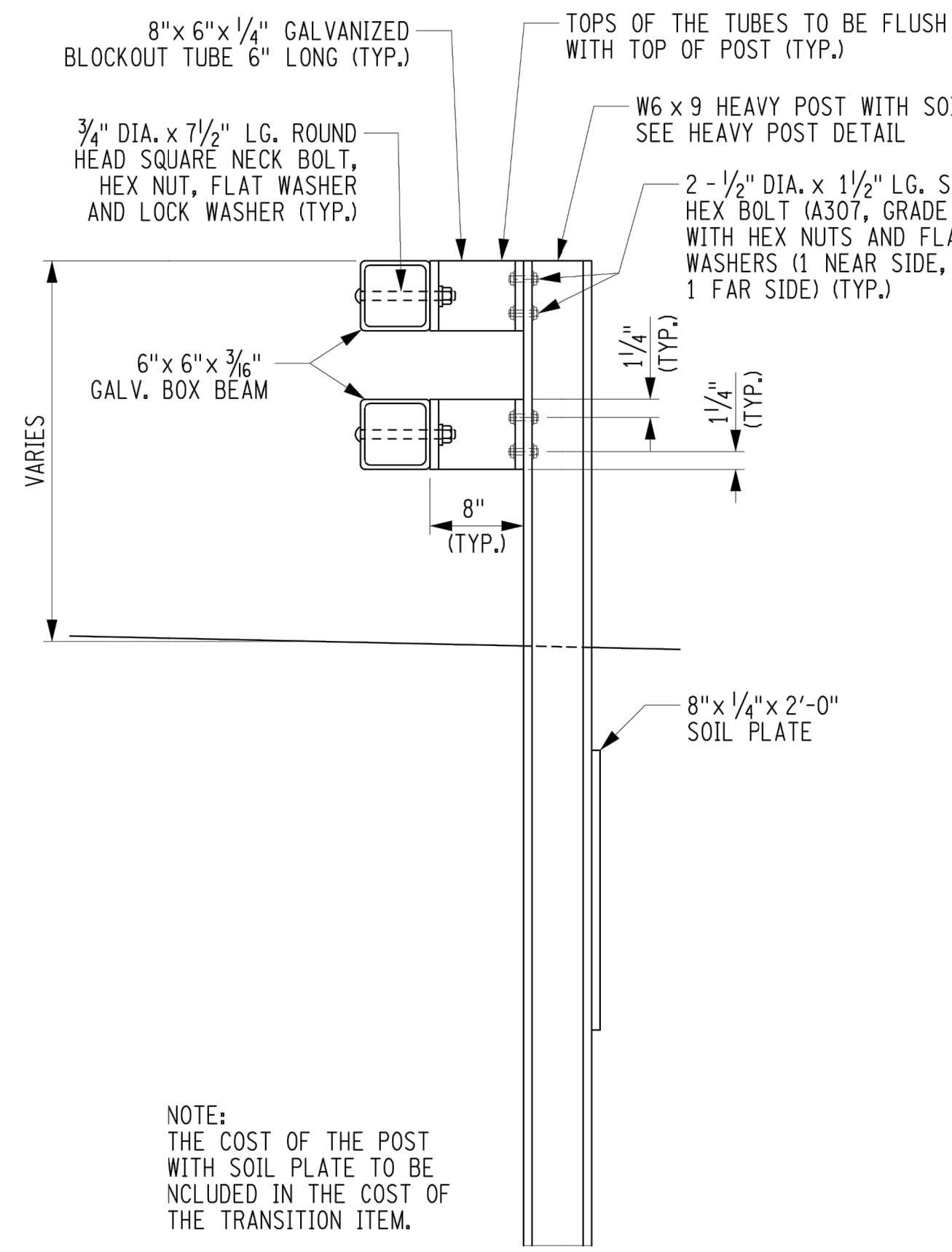
RAILING DETAILS
(2 OF 3)

DRAWING NUMBER

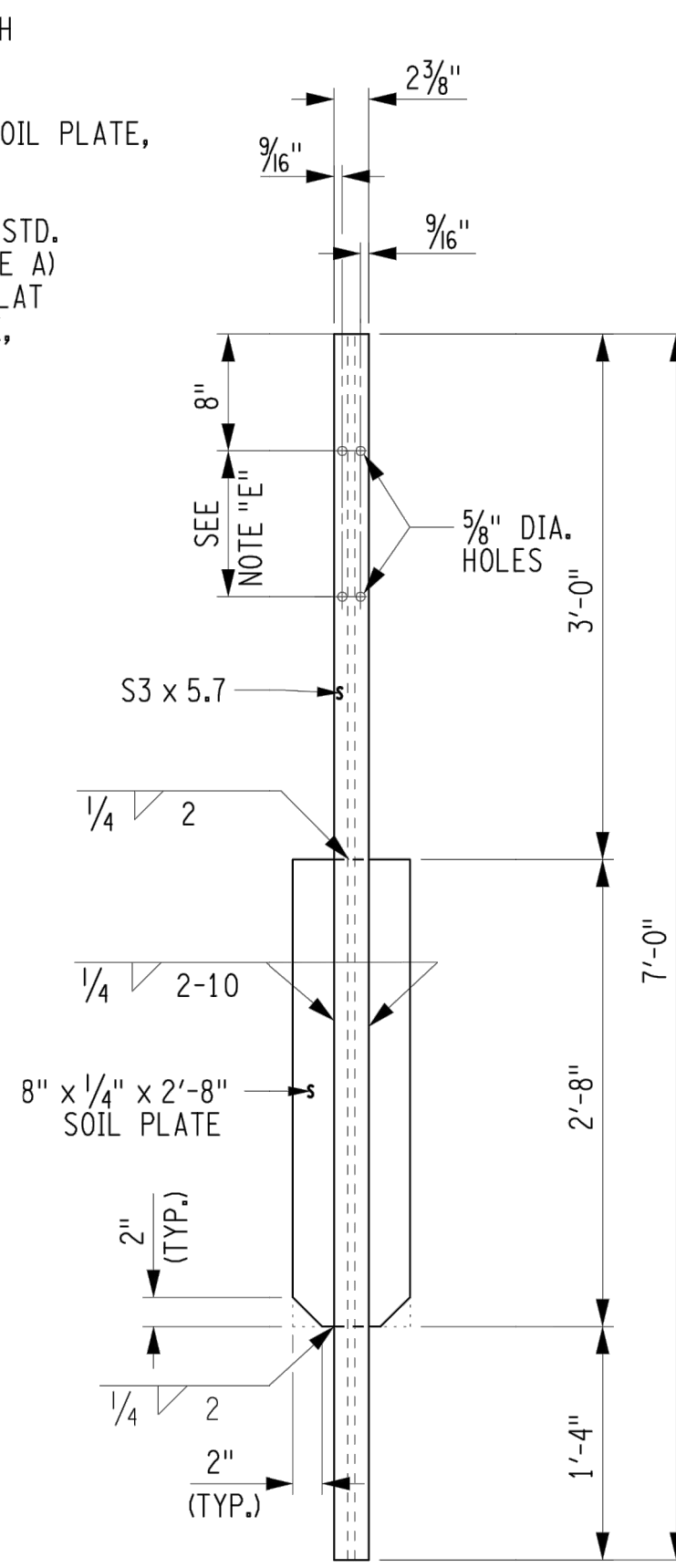
ST-34



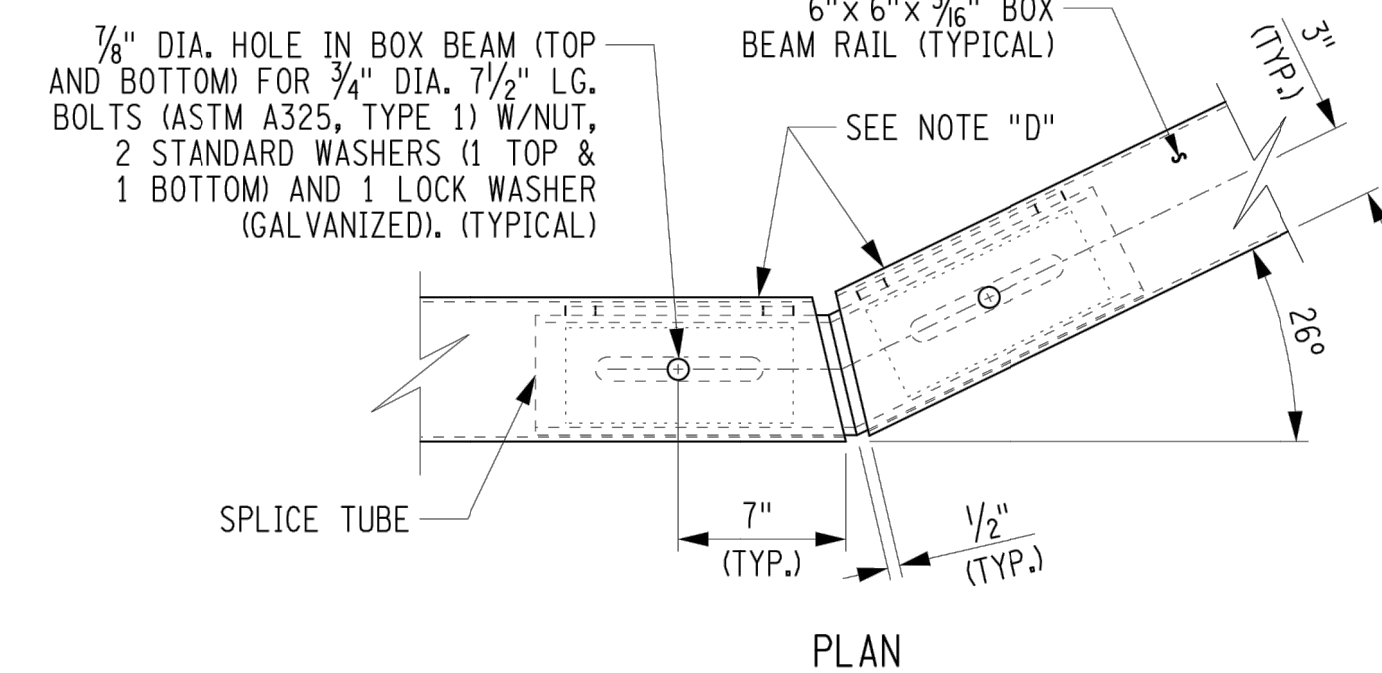
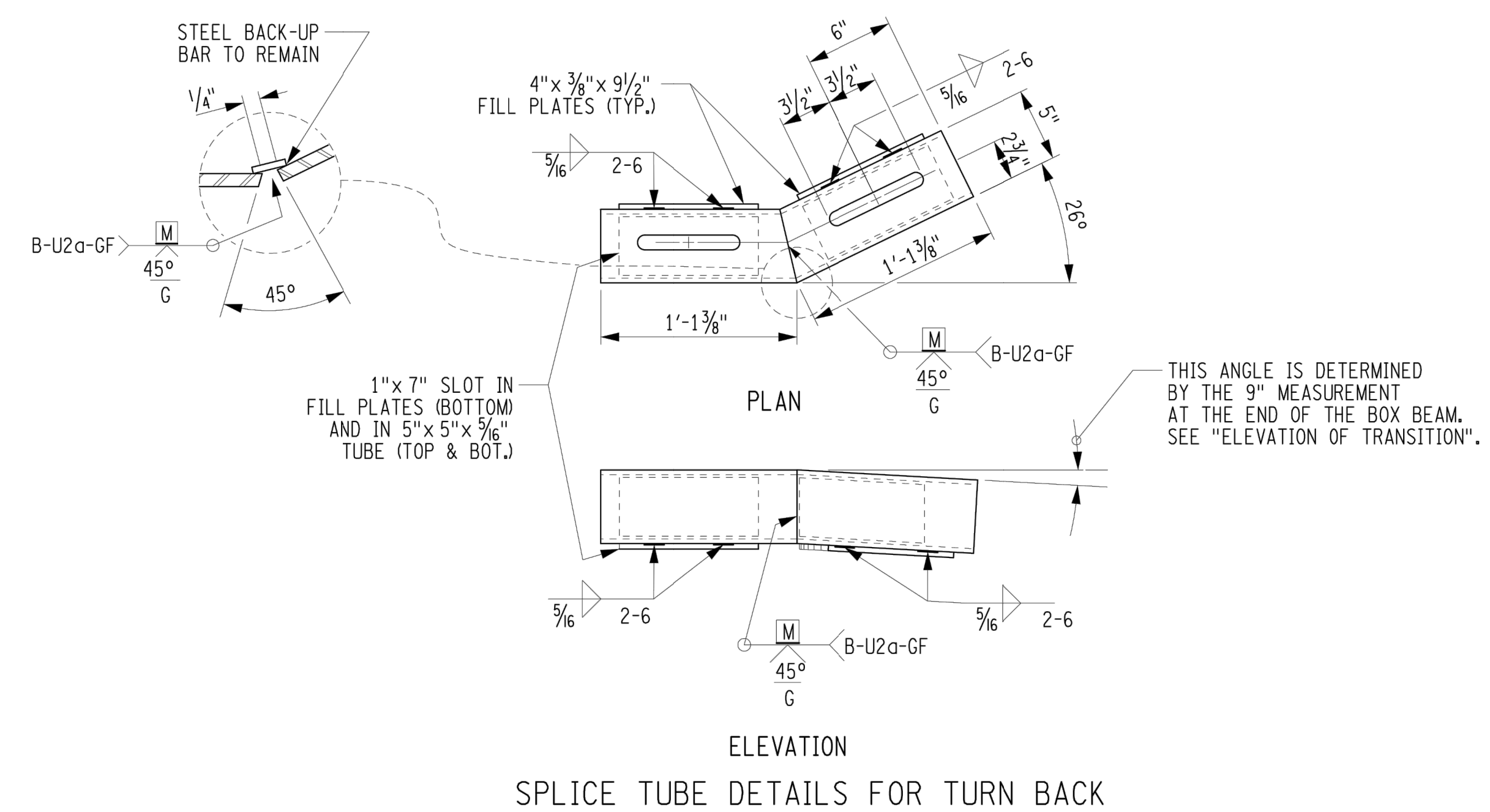
HEAVY POST DETAIL



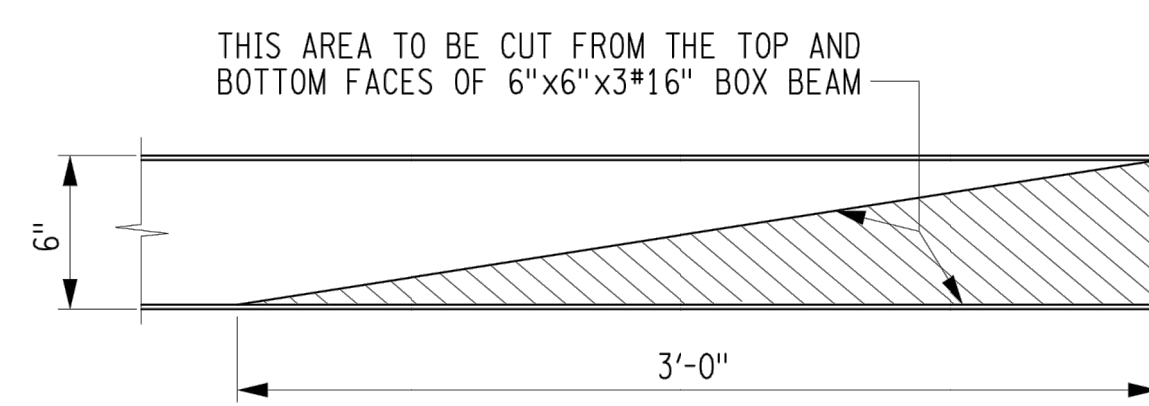
HEAVY POST ELEVATION



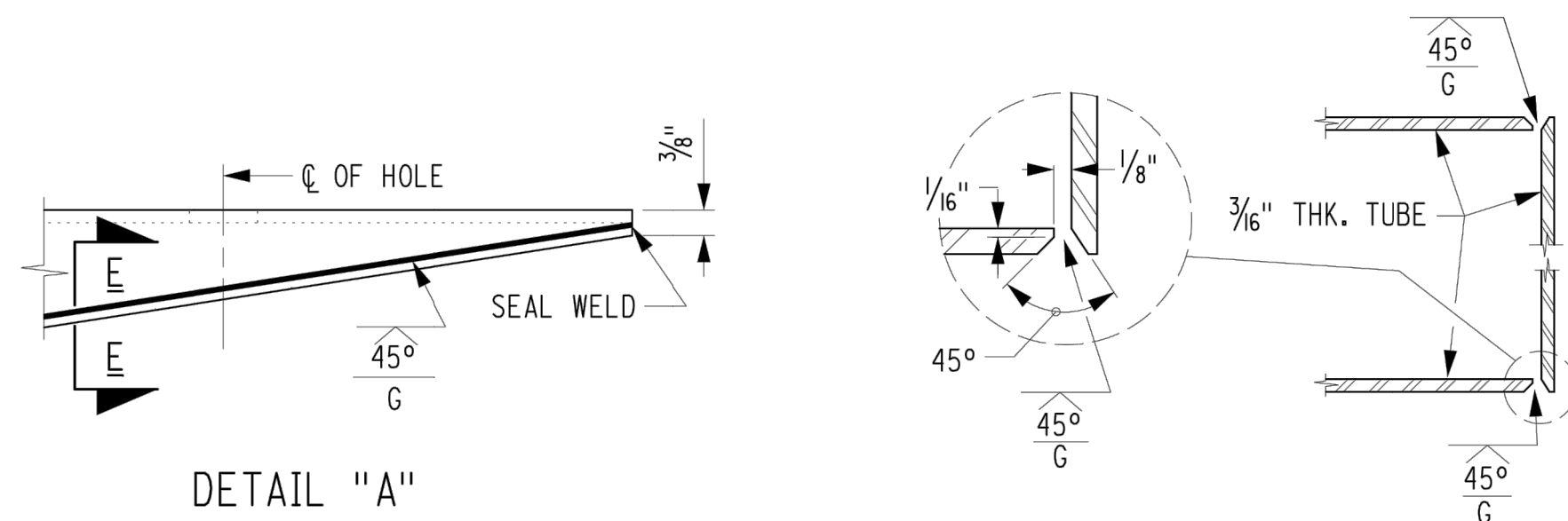
TRANSITION POST DETAIL



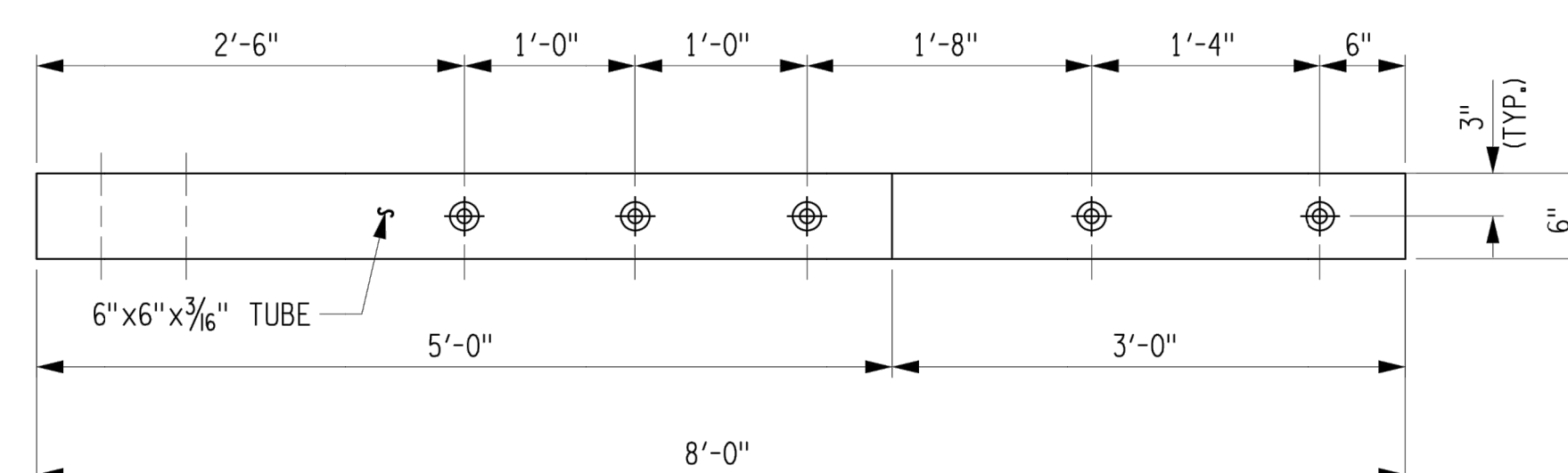
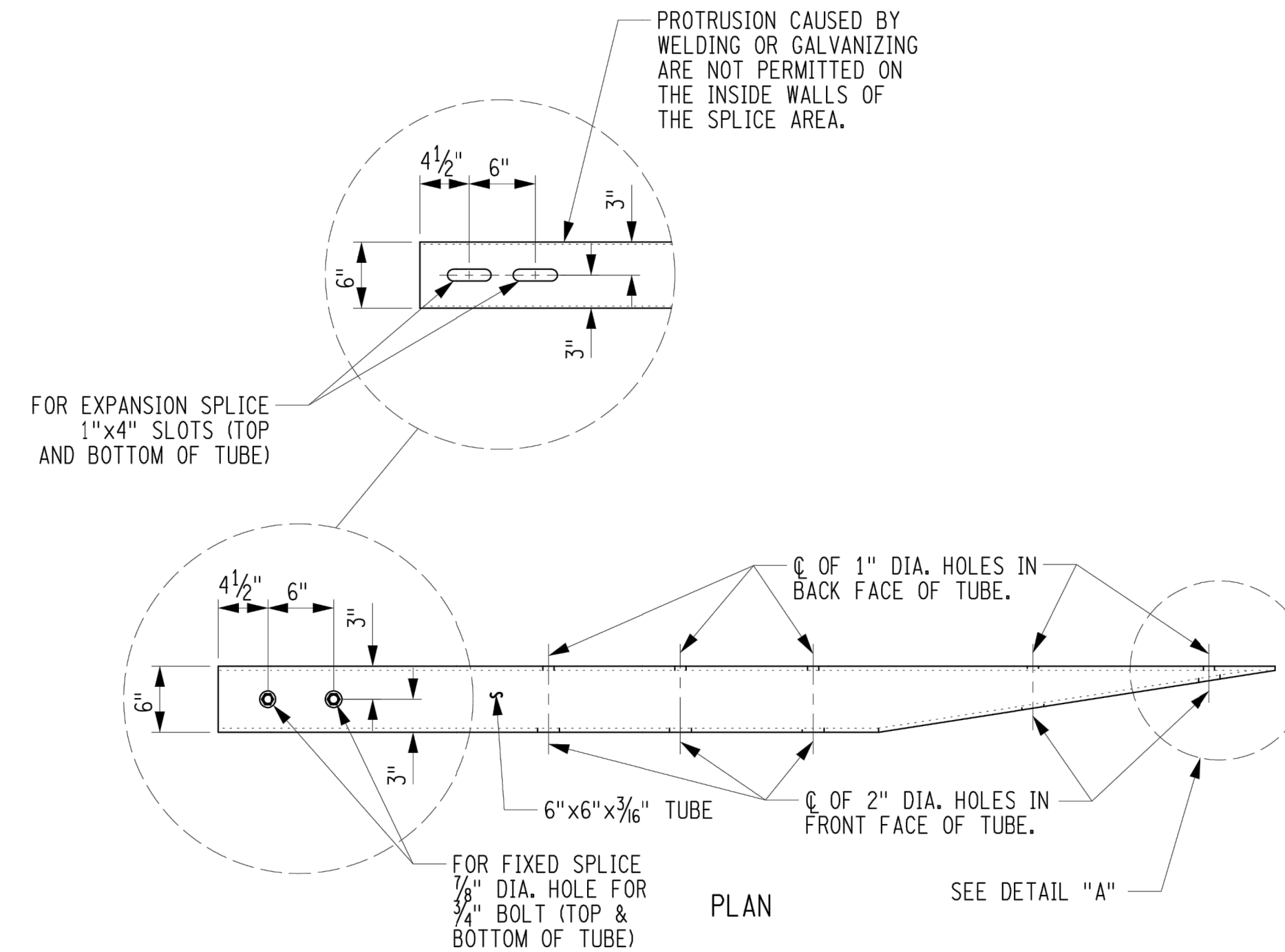
SPLICE DETAIL AT TURN BACK IN LOWER TRANSITION GUIDE RAIL



CUT DETAIL - END CONNECTION
NOT TO SCALE



SECTION E-E
NOT TO SCALE



END CONNECTION FOR BOX BEAM GUIDE RAIL

NOTE "D"

PROTRUSIONS CAUSED BY WELDING OR GALVANIZING ARE NOT PERMITTED ON THE ADJOINING SURFACES OF THE BOX BEAM RAILS, SPLICE TUBES AND FILL PLATES.

NOTE "E"

HOLES IN THE POST FOR THE LOWER RAIL MAY BE LOCATED AND DRILLED. IF SO, THE GALVANIZING SHALL BE REPAIRED IN ACCORDANCE WITH SUBSECTION 719-01.



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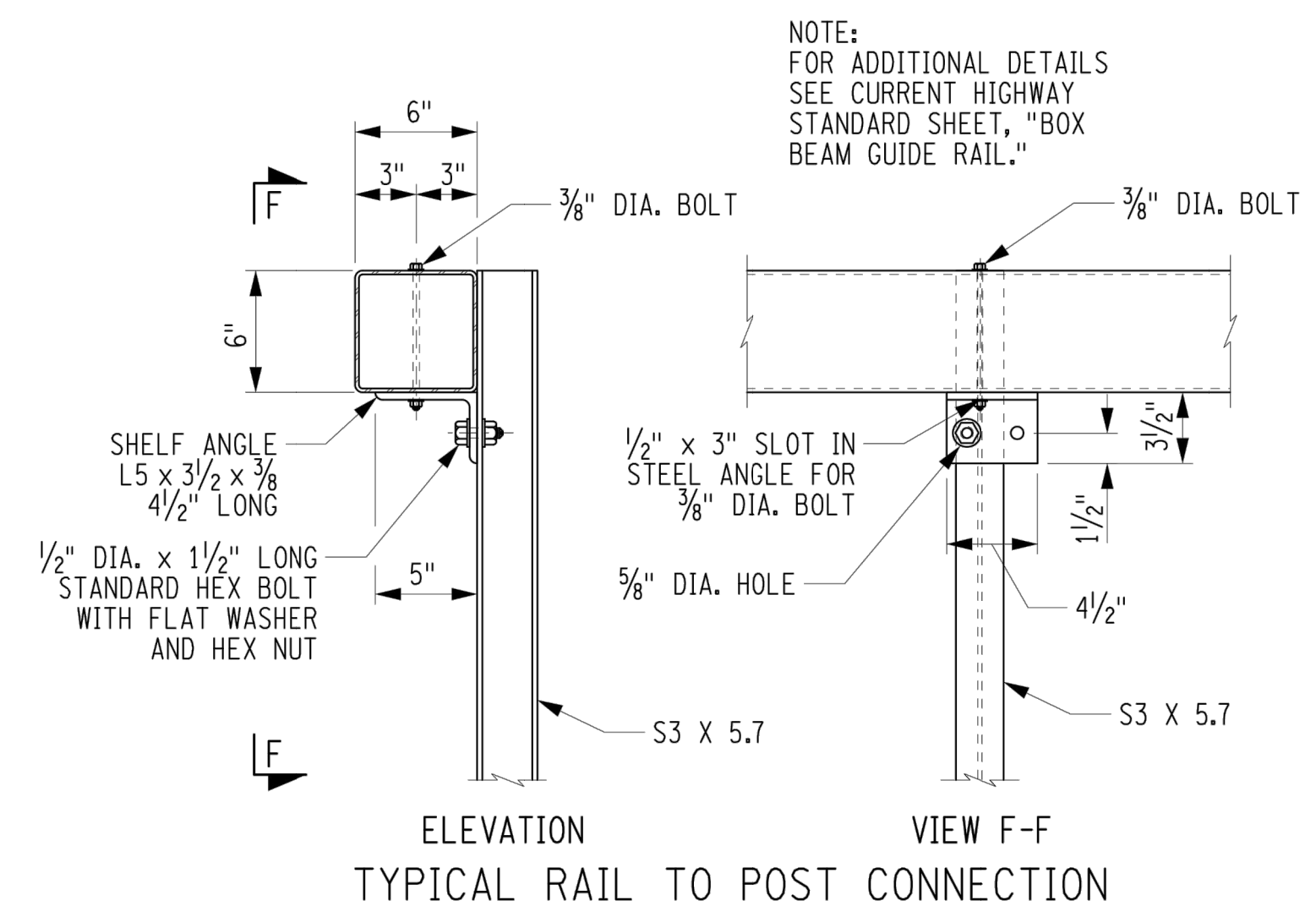
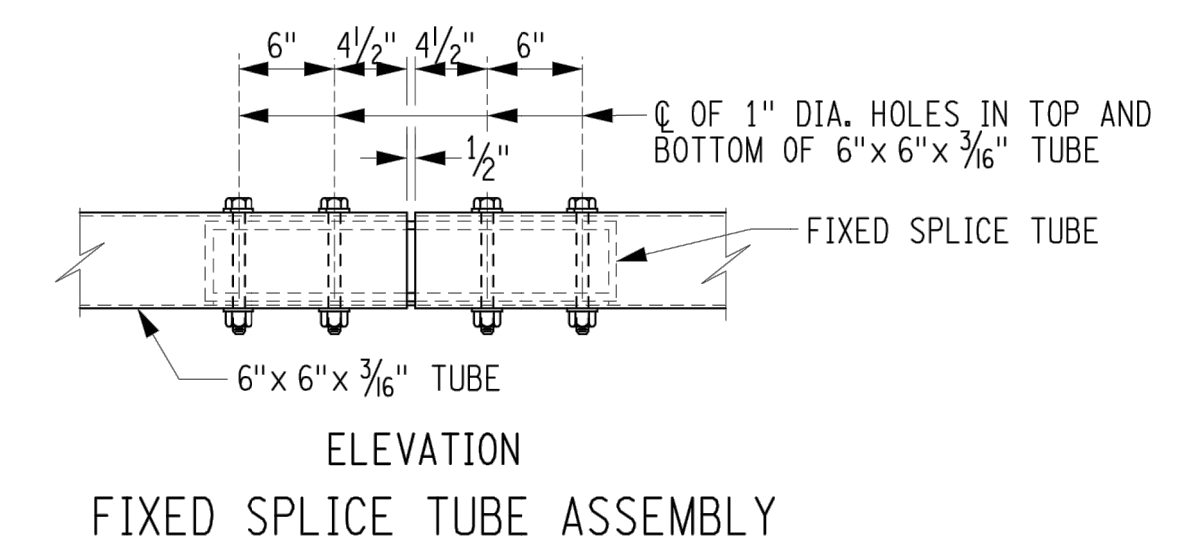
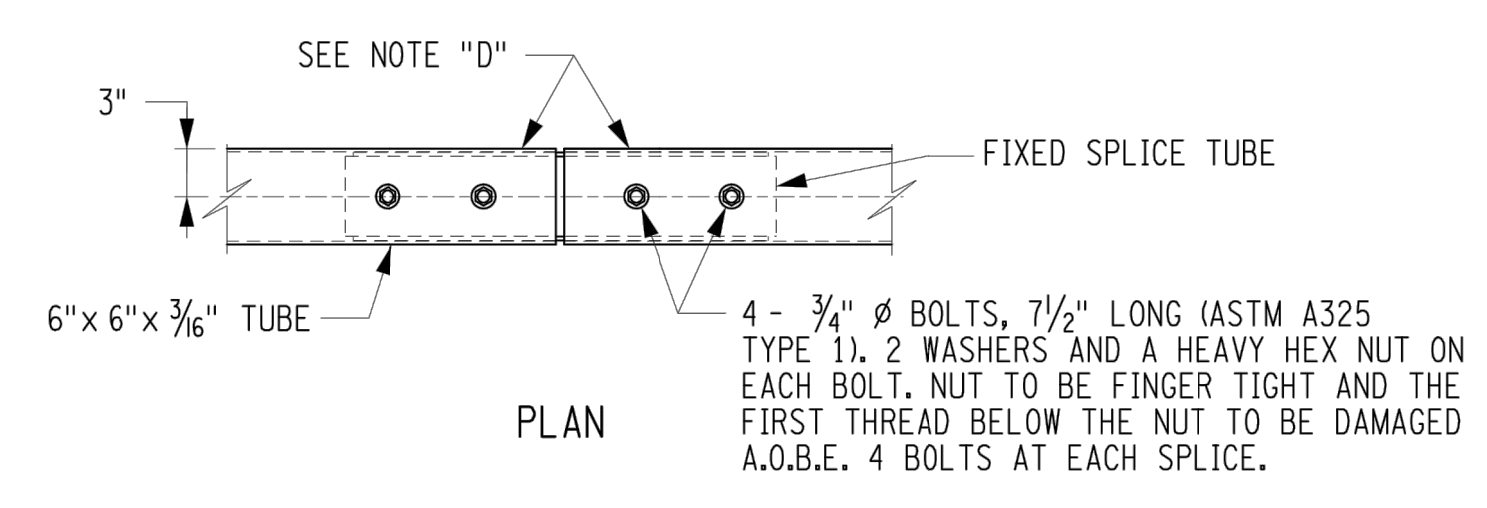
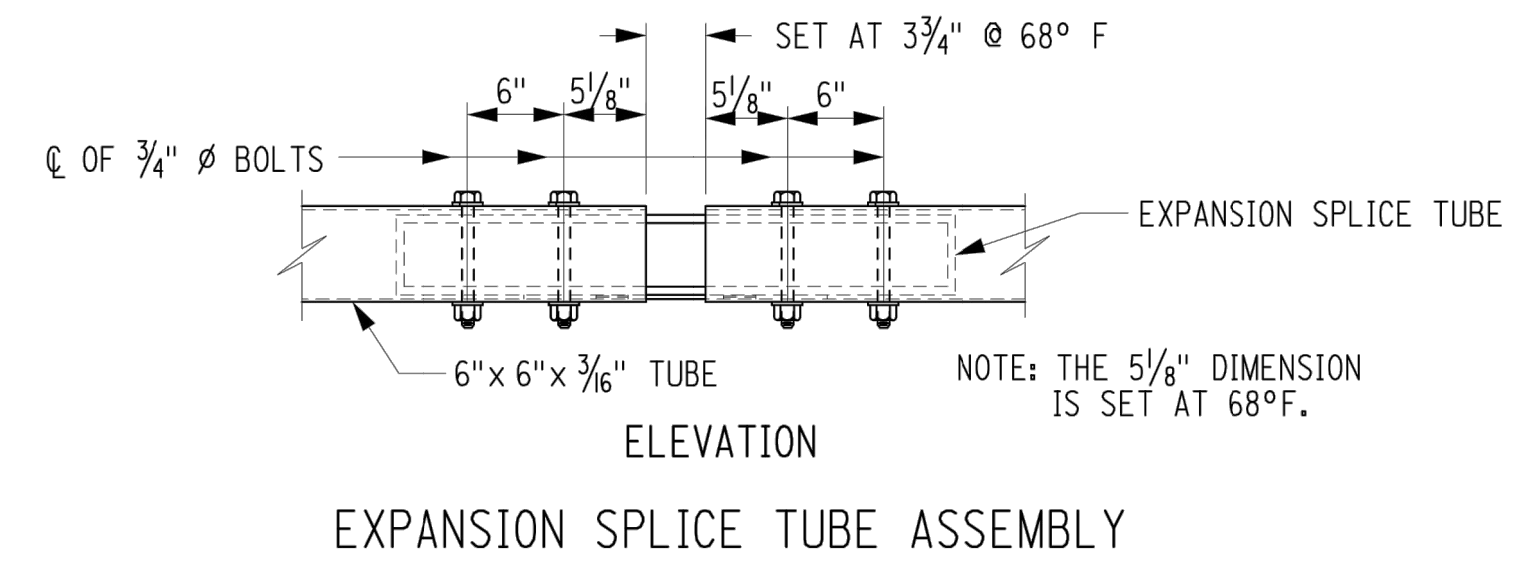
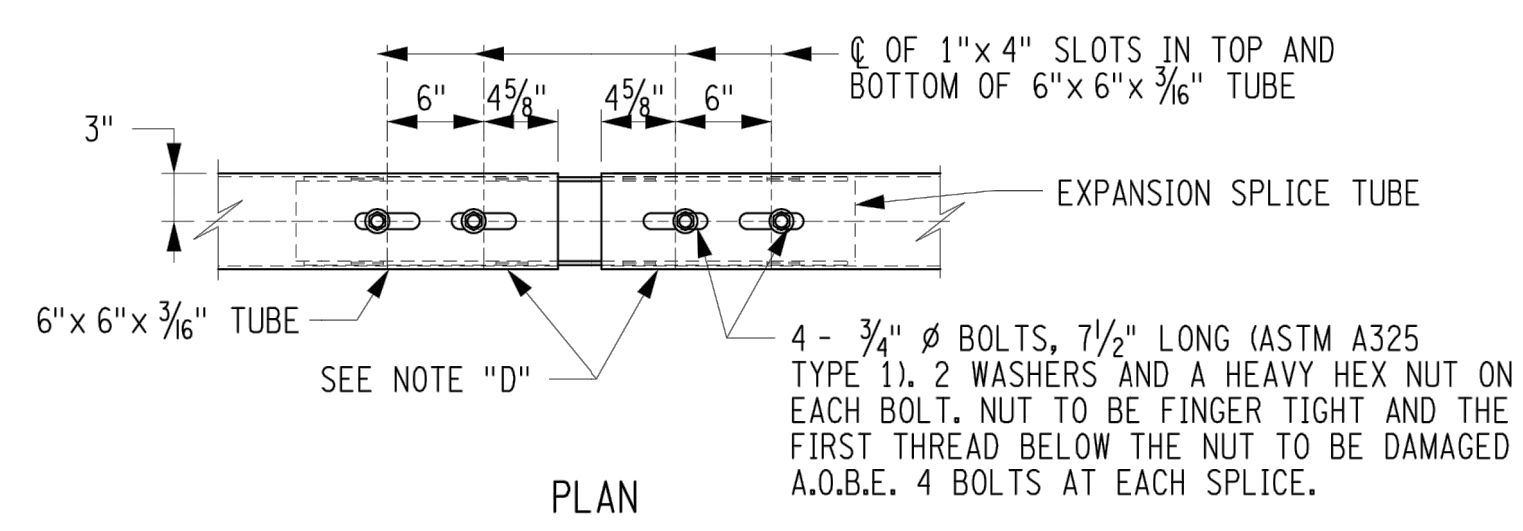
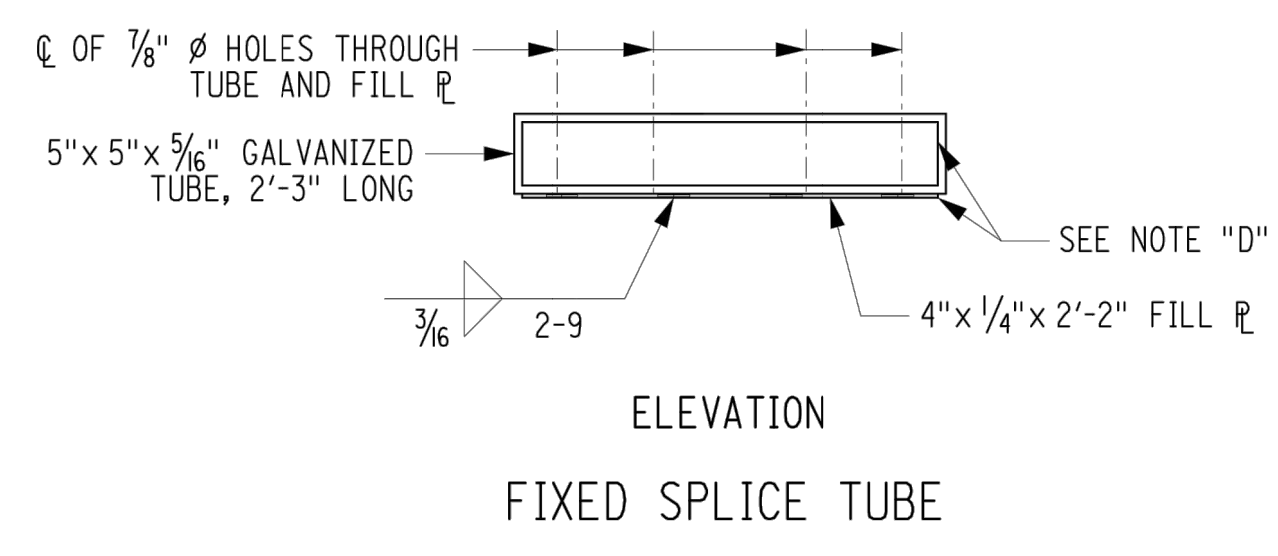
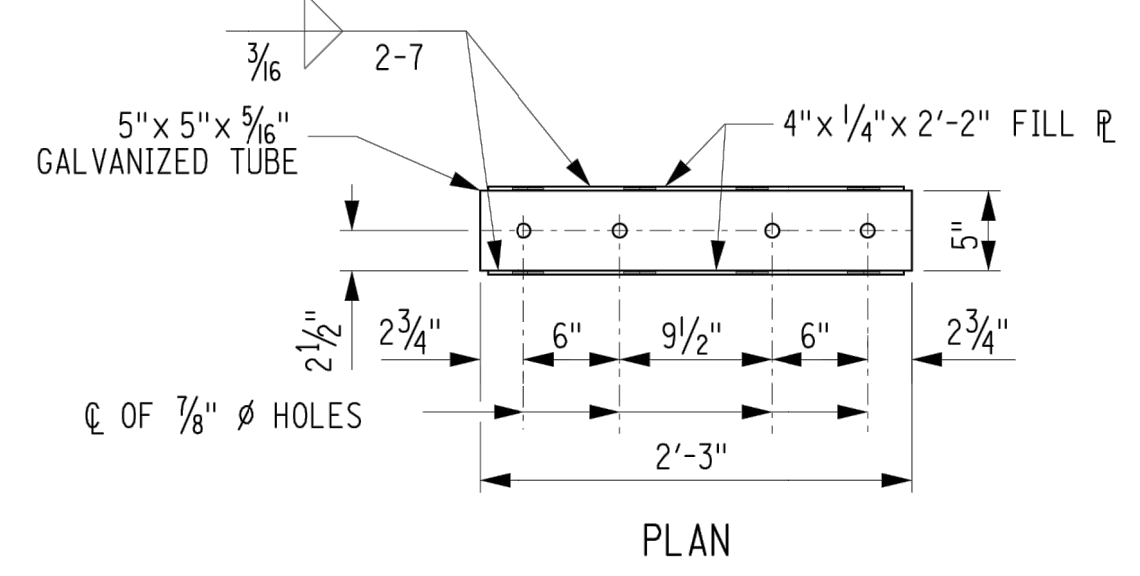
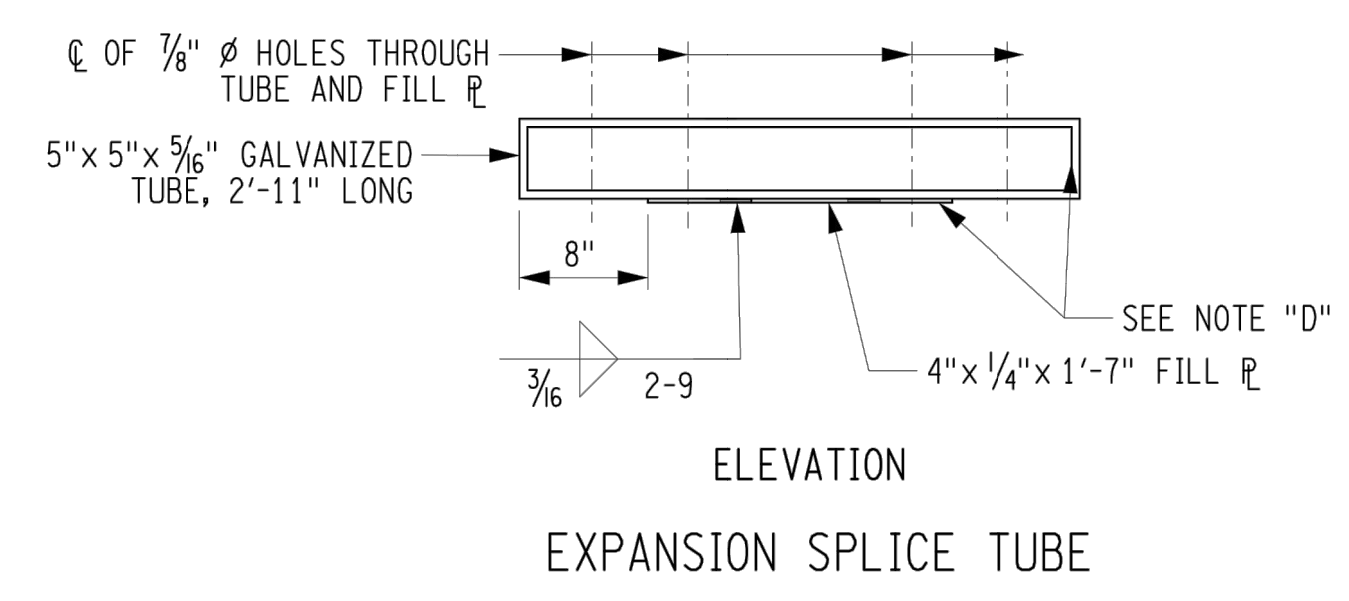
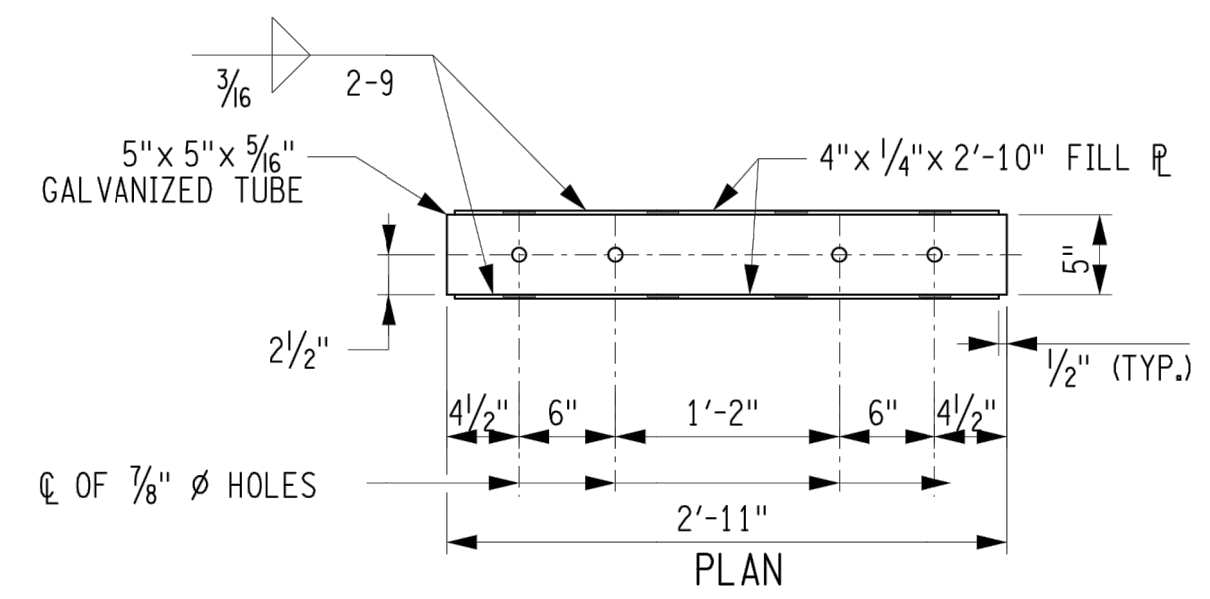
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DRAWING TITLE
RAILING DETAILS (3 OF 3)

DRAWING NUMBER
ST-35



NOTE "D": PROTRUSIONS CAUSED BY WELDING OR GALVANIZING ARE NOT PERMITTED ON THE ADJOINING SURFACES OF THE BOX BEAM RAILS, SPLICE TUBES AND FILL PLATES.

NUMBER OF ALBANY PORT EXPANSION BRACKETS SHEET FILES: BRIDGE PLAN PLANS (MID) (R) (L) (D) (S) (T) (U) (V) (W) (X) (Y) (Z) (AA) (AB) (AC) (AD) (AE) (AF) (AG) (AH) (AI) (AJ) (AK) (AL) (AM) (AN) (AO) (AP) (AQ) (AR) (AS) (AT) (AU) (AV) (AW) (AX) (AY) (AZ) (BA) (BB) (BC) (BD) (BE) (BF) (BG) (BH) (BI) (BJ) (BK) (BL) (BM) (BN) (BO) (BP) (BQ) (BR) (BS) (BT) (BU) (BV) (BW) (BX) (BY) (BZ) (CA) (CB) (CC) (CD) (CE) (CF) (CG) (CH) (CI) (CJ) (CK) (CL) (CM) (CN) (CO) (CP) (CQ) (CR) (CS) (CT) (CU) (CV) (CW) (CX) (CY) (CZ) (DA) (DB) (DC) (DD) (DE) (DF) (DG) (DH) (DI) (DJ) (DK) (DL) (DM) (DN) (DO) (DP) (DQ) (DR) (DS) (DT) (DU) (DV) (DW) (DX) (DY) (DZ) (EA) (EB) (EC) (ED) (EE) (EF) (EG) (EH) (EI) (EJ) (EK) (EL) (EM) (EN) (EO) (EP) (EQ) (ER) (ES) (ET) (EU) (EV) (EW) (EX) (EY) (EZ) (FA) (FB) (FC) (FD) (FE) (FF) (FG) (FH) (FI) (FJ) (FK) (FL) (FM) (FN) (FO) (FP) (FQ) (FR) (FS) (FT) (FU) (FV) (FW) (FX) (FY) (FZ) (GA) (GB) (GC) (GD) (GE) (GF) (GG) (GH) (GI) (GJ) (GK) (GL) (GM) (GN) (GO) (GP) (GQ) (GR) (GS) (GT) (GU) (GV) (GW) (GX) (GY) (GZ) (HA) (HB) (HC) (HD) (HE) (HF) (HG) (HH) (HI) (HJ) (HK) (HL) (HM) (HN) (HO) (HP) (HQ) (HR) (HS) (HT) (HU) (HV) (HW) (HX) (HY) (HZ) (IA) (IB) (IC) (ID) (IE) (IF) (IG) (IH) (II) (IJ) (IK) (IL) (IM) (IN) (IO) (IP) (IQ) (IR) (IS) (IT) (IU) (IV) (IW) (IX) (IY) (IZ) (JA) (JB) (JC) (JD) (JE) (JF) (JG) (JH) (JI) (JJ) (JK) (JL) (JM) (JN) (JO) (JP) (JQ) (JR) (JS) (JT) (JU) (JV) (JW) (JX) (JY) (JZ) (KA) (KB) (KC) (KD) (KE) (KF) (KG) (KH) (KI) (KJ) (KK) (KL) (KM) (KN) (KO) (KP) (KQ) (KR) (KS) (KT) (KU) (KV) (KW) (KX) (KY) (KZ) (LA) (LB) (LC) (LD) (LE) (LF) (LG) (LH) (LI) (LJ) (LK) (LL) (LM) (LN) (LO) (LP) (LQ) (LR) (LS) (LT) (LU) (LV) (LW) (LX) (LY) (LZ) (MA) (MB) (MC) (MD) (ME) (MF) (MG) (MH) (MI) (MJ) (MK) (ML) (MN) (MO) (MP) (MQ) (MR) (MS) (MT) (MU) (MV) (MW) (MX) (MY) (MZ) (NA) (NB) (NC) (ND) (NE) (NF) (NG) (NH) (NI) (NJ) (NK) (NL) (NM) (NO) (NP) (NQ) (NR) (NS) (NT) (NU) (NV) (NW) (NX) (NY) (NZ) (OA) (OB) (OC) (OD) (OE) (OF) (OG) (OH) (OI) (OJ) (OK) (OL) (OM) (ON) (OO) (OP) (OQ) (OR) (OS) (OT) (OU) (OV) (OW) (OX) (OY) (OZ) (PA) (PB) (PC) (PD) (PE) (PF) (PG) (PH) (PI) (PJ) (PK) (PL) (PM) (PN) (PO) (PP) (PQ) (PR) (PS) (PT) (PU) (PV) (PW) (PX) (PY) (PZ) (QA) (QB) (QC) (QD) (QE) (QF) (QG) (QH) (QI) (QJ) (QK) (QL) (QM) (QN) (QO) (QP) (QQ) (QR) (QS) (QT) (QU) (QV) (QW) (QX) (QY) (QZ) (RA) (RB) (RC) (RD) (RE) (RF) (RG) (RH) (RI) (RJ) (RK) (RL) (RM) (RN) (RO) (RP) (RQ) (RR) (RS) (RT) (RU) (RV) (RW) (RX) (RY) (RZ) (SA) (SB) (SC) (SD) (SE) (SF) (SG) (SH) (SI) (SJ) (SK) (SL) (SM) (SN) (SO) (SP) (SQ) (SR) (SS) (ST) (SU) (SV) (SW) (SX) (SY) (SZ) (TA) (TB) (TC) (TD) (TE) (TF) (TG) (TH) (TI) (TJ) (TK) (TL) (TM) (TN) (TO) (TP) (TQ) (TR) (TS) (TT) (TU) (TV) (TW) (TX) (TY) (TZ) (UA) (UB) (UC) (UD) (UE) (UF) (UG) (UH) (UI) (UJ) (UK) (UL) (UM) (UN) (UO) (UP) (UQ) (UR) (US) (UT) (UU) (UV) (UW) (UX) (UY) (UZ) (VA) (VB) (VC) (VD) (VE) (VF) (VG) (VH) (VI) (VJ) (VK) (VL) (VM) (VN) (VO) (VP) (VQ) (VR) (VS) (VT) (VU) (VV) (VW) (VX) (VY) (VZ) (WA) (WB) (WC) (WD) (WE) (WF) (WG) (WH) (WI) (WJ) (WK) (WL) (WM) (WN) (WO) (WP) (WQ) (WR) (WS) (WT) (WU) (WV) (WW) (WX) (WY) (WZ) (XA) (XB) (XC) (XD) (XE) (XF) (XG) (XH) (XI) (XJ) (XK) (XL) (XM) (XN) (XO) (XP) (XQ) (XR) (XS) (XT) (XU) (XV) (XW) (XX) (XY) (XZ) (YA) (YB) (YC) (YD) (YE) (YF) (YG) (YH) (YI) (YJ) (YK) (YL) (YM) (YN) (YO) (YP) (YQ) (YR) (YS) (YT) (YU) (YV) (YW) (YX) (YZ) (ZA) (ZB) (ZC) (ZD) (ZE) (ZF) (ZG) (ZH) (ZI) (ZJ) (ZK) (ZL) (ZM) (ZN) (ZO) (ZP) (ZQ) (ZR) (ZS) (ZT) (ZU) (ZV) (ZW) (ZX) (ZY) (ZZ)